

AGENDA
VALLEY CITY COUNCIL
Tuesday, June 16, 2026
City Hall
203 North Spruce
Valley, NE 68064
4:30 PM

1. **Call to Order**
2. **Roll Call**
3. **Pledge of Allegiance**
4. **Proof of Publication**
5. **Visitors/Correspondence**

Anyone desiring to speak on any item or issue not on the agenda or any item on the agenda that does not include a public hearing may do so, but shall be limited to three (3) minutes. Persons should identify themselves by name and address. Persons speaking should not expect the Planning Commission to engage in back-and-forth dialogue regarding their comments. Unless an agenda item includes a public hearing, no person may speak during the business portion of the meeting; provided, however, persons speaking during a public hearing are limited to between five (5) and twenty (20) minutes.

The public is advised that a copy of the Open Meetings Act is located on the north wall of the Council Chamber, and one copy of all reproducible written material to be discussed at this meeting is available for examination or copying.

6. **Approval of Agenda**
7. **Consent Agenda**

All agenda items on the consent agenda will be acted on in a single motion. Consent agenda items are being forwarded to the Commission Members. Any individual item may be removed by a commission member for special discussion and consideration.

7.A. Approve Minutes of March 17, 2026 Planning Commission meeting

8. **Public Hearings**

8.A. Melvin Sudbeck Homes, Inc. consideration and approval for application for a change of zoning and change of future land use map for the purpose of rezoning

The City Council reserves the right to enter into an executive session at any time during the meeting, in accordance with the Nebraska Open Meetings Act, even though the closed session may not be indicated on the agenda.

It is the intention of the City Council to take up the items on the agenda in sequential order. However, the City Council reserves the right to take up matters in a different order to accommodate the schedules of the City Council members, persons having items on the agenda, and the public.

property for a proposed development that will provide 2 multi-family and 328 single-family residential lots:

- Open Public Hearing
- Public comments: proponents and opponents
- Close Public Hearing
- Discussion and questions by Planning Commission Members
- Vote on recommendation to City Council

8.B. Melvin Sudbeck Homes, Inc. consideration and approval for application for a preliminary plat:

- Open Public Hearing
- Public comments: proponents and opponents
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9. **Adjourn**

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**NOTICE OF MEETING
PLANNING COMMISSION
CITY OF VALLEY, NEBRASKA**

Notice is hereby given that a meeting of the City of Valley Planning Commission will be held on **Tuesday, June 16, 2026**, at **4:30 p.m.** at **Valley City Hall**. PUBLIC HEARINGS will be held on the following:

1. Melvin Sudbeck Homes, Inc.:
 - Consideration and approval of application for change of zoning and change of future land use map for the purpose of rezoning the property for a proposed development that will provide 2 multi-family and 328 single-family residential lots.
2. Melvin Sudbeck Homes, Inc.:
 - Consideration and approval of Preliminary Plat.

An agenda kept continually current shall be available for public inspection at Valley City Hall.

Larry Bottger, Chair
City of Valley, Planning Commission

6/5 ZNEZ



The **Daily Record**

Proof of Publication

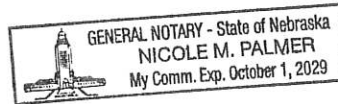
JASON W. HUFF, Publisher

UNITED STATES OF AMERICA,
 The State of Nebraska,
 District of Nebraska,
 County of Douglas,
 City of Omaha } ss.

JASON W. HUFF, being duly sworn, deposes and say that they are the PUBLISHER and/or MANAGING EDITOR of THE DAILY RECORD, of Omaha, a legal newspaper, printed and published daily in the English language, having a bona fide paid circulation in Douglas County in excess of 300 copies, and a general circulation in Sarpy, Lancaster, Cass and Dodge Counties, printed in Omaha, in said County of Douglas, Nebraska for more than fifty-two weeks last past; that the printed notice here-to attached was published in THE DAILY RECORD, of Omaha, for _____ 1 _____ consecutive weeks on:

6/5/26

That said Newspaper during that time was regularly published and in general circulation in the County of Douglas, and State of Nebraska.

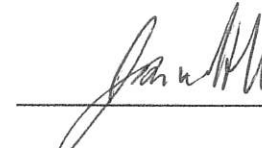


Publisher's Fee \$29.33

Additional Copies \$ _____

Filing Fee \$ _____

Total \$29.33



Subscribed in my presence and sworn to before
 me this JUNE 05 2026



Notary Public in and for
 Douglas County, State of Nebraska

AGENDA
PLANNING COMMISSION

Tuesday, June 16, 2026

City Hall

203 North Spruce

Valley, NE 68064

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CITY OF VALLEY
VALLEY PLANNING COMMISSION MINUTES
March 17, 2026

1 and 2. Roll Call and Call to Order: Larry Bottger, Chairman, Kyle Anderson, Scott Burke, Mark Conrey, Brian Foutch, Jim Tomanek. Absent: Danielle Lowry, Jeremy Mayer, and Greg Sunde. Also present City Administrator Mike Gorman, Building Inspector Marty Evans, City Attorney Matthew Munderloh, and City Engineer Greg Perry. Larry Bottger called the meeting to order at 4:30 pm.

3. Pledge of Allegiance: The Pledge of allegiance was recited.

4. Proof of Publication: The Proof of Publication was provided by Larry Bottger.

5. Visitors/Correspondence: No one spoke.

6. Approval of Agenda: Member Anderson moved to approve the agenda. Tomanek seconded. YES: Anderson, Bottger, Burke, Conrey, Foutch, Tomanek. NO: No one. ABSENT: Lowry, Mayer and Sunde. Motion carried.

7. Consent of Agenda: Member Conrey moved to approve the consent and member Anderson seconded the motion.

8A. Public Hearing: Chairman Bottger opened public hearing. No one was present. Chairman Bottger closed the public hearing.

City Engineer Greg Perry stated memory care facilities are becoming more popular. By changing the current matrix to conditional use in R1-R2-R-3 would better serve the city and property owners. Chairman Bottger commented this change would help alleviate problems in all building zones.

Member Conrey asked about building structure requirements for new and old homes.

City Engineer Perry stated most buildings can be converted to medical facilities per permits and codes with state and county inspections.

Member Anderson asked about parking areas for staff and visiting families.

Member Conrey talked about difficulties pertaining to staffing work force.

Building Inspector Evans talked about fencing property yearly state licensing and inspections. Facility resident complaints would go through Valley City Hall.

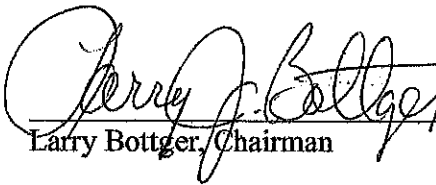
Member Anderson made a motion to approve an ordinance to amend Section 5.06 of the Valley zoning regs to permit residential/commercial institutions, group homes as a conditional use in zoning districts R1-R2-R3. Member Tomanek seconded the motion.

Voice vote: YES: Bottger, Anderson, Burke, Conrey, Foutch, Tomanek. NO: None.

8B. Withdrawn by applicant.

Further discussion: Inspector Marty Evans advised board that Douglas County will not be helping with inspections in the future and that all permits going forward must be done on time. Marty also introduced his assistant Lea Schmeling.

Chairman Bottger made a motion to adjoin, Member Fouch seconded.


Larry Bottger, Chairman

P.O. Box 682
Valley, NE 68064
Phone: 402-359-2251 Ext. 306
Fax-402-359-2610
www.valleyne.org
Business Hours 9 am – 5 pm M-F



Official Use Only

Planning Meeting Date _____

Check Cash Credit Card

Check # _____

APPLICATION FOR A CHANGE OF ZONING

Date: 05/04/2026

Form must be filled out completely before acceptance of this application for processing. Please print.

Applicant Name: Melvin Sudbeck Telephone No. 402-895-3288

Applicants Address: 16255 Woodland Dr, Omaha, NE 68136

Applicants Email: sudbeckhomes@gmail.com

Present Use of Subject Property: Agricultural Desired Use of Subject Property: Residential

Present Zoning: (TA) Transitional Agriculture Requested Zoning (R2) Medium/High Density Residential District

Legal Description of Property Requested to be Rezoned: _____

LANDS SEC-TWN-RGE 33-16-10 -EX IRREG W 40 & S 12 FT & IRREG E.309 W.359 N940.21 S980.21 FT- PT W OF KING LAKE NW 1/4 33-16-10

Area of Subject Property (Square Foot/Acres): 115

How are adjoining properties used (Actual Land Zoning)

North: TA South: TA East: R1 West: TA + CATALINA

If exhibits are furnished, please describe and enumerate. (Furnish Plot or Site Plan showing existing and proposed structures, easements, water courses, curb cutbacks, etc)

The Zoning Administrator, who may be accompanied by others in hereby authorized to enter upon the property during normal working hours for the purpose of becoming familiar with the proposed situation.

Justification


You must justify your request. Questions 1 through 4 must be answered completely. Use separate sheet if necessary.

1. What is the general character of the area? Describe.

2. What utilities provider will be used?

A. Sewer CITY OF VALLEY B. Water CITY OF VALLEY C. Electric OPPD D. Gas BLACK HILLS

3. How will the proposed Zoning District affect traffic in the area? Will streets or roads need to be updated for access to the area? If yes, what will the requirements be?

Signature of Owner  or Signature of Agent _____

Does this zoning change conform to the Current Future Land Use Plan? Y _____ N X If not, please fill out the following page.

I hereby state that that the information submitted on this application is accurate and correct. I recognize that the issuance of this application shall not grant approval to violate any of the provisions of the building codes or zoning ordinances enforced by this jurisdiction, state or federal law; and that this application shall not prevent the building official from requiring site to be in compliance with all applicable code provisions during field inspections.

APPLICATION FOR A CHANGE OF FUTURE LAND USE MAP

Set up a request for a pre-application conference with City Staff prior to submittal. Email (buildinginspector@valleyne.org)


Date: 02/20/2025 Time: 10:00 AM

Additional Material Required

1. Site Plan with all pertinent details to amend the Future Land Use Map
 - A. Schematic lay out, proposed lots, roadway layout, etc.
2. Does the proposed map change result in an excessive or burdensome use of public facilities such as streets, schools, transportation facilities or utilities?
3. Additional information deemed necessary by City Staff to conduct a thorough analysis of the application, which may include, but not limited to stormwater analysis, sanitary sewer analysis, traffic impact study, or environmental study.
4. Ownership record.

Project Narrative: The proposed development will provide 2 multi-family and 328 single-family residential lots.

The proposed Future Land Use Map amendment seeks to align the 115-acre site with the established development patterns immediately to its east and west, which already feature medium- to high-density residential uses. By incorporating 13 acres of multi-family housing alongside 328 single-family lots, the project creates a mix of densities supports lifecycle housing.

Signature of Owner  or Signature of Agent _____

I hereby state that that the information submitted on this application is accurate and correct. I recognize that the issuance of this application shall not grant approval to violate any of the provisions of the building codes or zoning ordinances enforced by this jurisdiction, state or federal law; and that this application shall not prevent the building official from requiring site to be in compliance with all applicable code provisions during field inspections.



EAGLE ENGINEERING GROUP

12100 West Center Road, Suite 803 Omaha, Nebraska 68144
(402) 399-0227 | www.eagleengineeringgroup.com

June 10, 2026

Larry Bottger, Chairman
Planning Commission
CITY OF VALLEY
203 North Spruce Street
Valley, Nebraska 68064

Re: Lago Villaggio Preliminary Plat Submittal
252nd Street and King Lake Road
Valley, Nebraska
EEG #24-03-15

Dear Mr. Bottger:

We offer the following review comments related to the Developers' Preliminary Plat submittal dated May 12th for the Lago Villaggio Lake development located on northeast corner near the intersection of N. 252nd Street and King Lake Road:

Preliminary Plat

1. Note: No direct vehicular access to N. 252nd or King Lake Road will be allowed.
2. Street names shall be verified by Douglas County/City of Omaha Planning Department.
3. Lot 293 and 322 do not meet the minimum lot size for the R-2 zoning district (6,000 sq ft).

Rezoning

1. The future land use plan map was amended in 2018 for low density residential for this parcel and the lands east of 252nd Street. The land use would be compatible with no public sanitary and adjacent floodplain of the Elkhorn River. The Catalina development has since extended utilities nearby that would suppose a medium to high density residential (MHDR) land use.
2. The requested zoning of R-2 is contiguous with the Catalina development and adjacent to the R-1 zoning within King Lake.

Utility Plan

1. Lift station location shall be further review with City Engineer for access, elevated above the AO floodplain (1133) and downstream force main conveyance connection.
2. Water main along 252nd Street shall extend south to intersection.

Grading and Drainage

1. Increase of runoff is being conveyed into the proposed lake feature. Overflow outlet release to the adjacent King Lake will need to be further review for location and any downstream impacts.
2. Conveyance of Impact Point 1 needs to be provided. Drainage from the north boundary may need to be considered to flow easterly.
3. Major storm conveyance from 252nd Street on the west to the east towards the Elkhorn River need to be provided.
4. Provide lake slope of stability for dredging operations.

Paving

1. Typical Section: sidewalk shall be located 1' from property line.
2. Street F and Street G shall extend to the north boundary.



Centerline Profiles

1. Provide vertical curves for all vertical point of intersection (VPI) with an algebraic difference above 1%.
2. Provide drainage away from 252nd Street on Street 'B' (STA. 100+00).
3. Provide drainage away from King Lake Road on Street 'F' (STA. 500+00) and Street 'G' (STA. 600+00)

Source and Use of Funds

1. The development will be financed using a Sanitary and Improvement District (SID).
2. Capital Facility Fees for sanitary sewer and water main shall be specially assessed on the 50% portion paid at the time of platting.

Traffic Impact Study

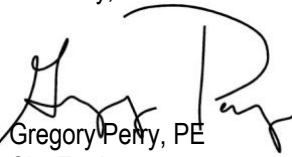
1. The analysis needs to provide a Phase 1 study year (i.e. 2028) build out and Phase 2 study year (i.e. 2035) buildout.
2. The analysis needs to include the Catalina development that is currently under construction for the 2035 short term and 2050 long term study years (Section 3.4).
3. 4.1 Trip Generation: Separate the traffic generation by development phase.
4. Appendix A: Update the conceptual site plan with the proposed preliminary plat exhibit.

General Items

1. Waivers: 1) 600' block Length – Provide the 10' pedestrian easements (Section 5.22)
2. Waivers: 2) 25' front yard setback: City Engineer does not support the waiver of the front yard setback as it does not demonstrate a hardship or meet the subdivision regulations.
3. Existing 252nd Street or King Lake Road improvements shall be coordinated with the City and Douglas County. The Developer will be required to construct all off-site improvements as part of Phase 1.
4. The property is located within the Zone 'AO' special flood hazard area (SFHA) as delineated by FEMA and a floodplain development permit will be required.
5. The Development shall confirm with Douglas County Emergency Management Agency (DC EMA) for adequate outdoor warning siren coverage (#102).

I will be available to answer any questions that you may have prior to the Planning Commission meeting on June 16, 2026.

Sincerely,



Gregory Perry, PE
City Engineer

ec: Mike Gorman, City Administrator
Gabe Evans, Building Inspector/ Zoning Administrator
Josh Seil, PE FoleyShald

APPLICATION FOR PRELIMINARY PLAT

Valley, Nebraska

LEGAL DESCRIPTION AND GENERAL LOCATION

LANDS SEC-TWN-RGE 33-16-10 - EX IRREG W 40 & S 12 FT &
IRREG E, 309 W.359 N940.21 S980.21 FT-PT W OF KING LAKE
NW 1/4 33-16-10

SUBDIVIDER

Name: MELVIN SUDBECK HOMES, INC
Address: 16255 WOODLAND DR
OMAHA, NE 68136

Telephone: () _____

OWNER

Name: LEWIS TERRY C ETAL TR
Address: 1111 STERLING RIDGE DR #264
OMAHA, NE 68144

Telephone: () _____

CITY USE ONLY

RECEIPT NO: _____

DATE: _____

PRELIMINARY PLAT #: _____

FEE PAID: \$

AGENT (Authorized to act on Subdivider's behalf):

Name: _____
Address: _____

Telephone: () _____

ANY OTHER ASSOCIATES:

Name: _____
Address: _____

Telephone: () _____

NAME OF PRELIMINARY PLAT: LAGO VILLAGGIO

NUMBER OF LOTS: 336

- Letter or transmittal and action(s) requested.
- Does the subdivider have any interest in the land surrounding the preliminary plat? Yes No If yes, please describe the nature of such interest:

Will the preliminary plat require any zoning or other action (rezoning, conditional use, or vacations) to complete the development? Yes No . If yes, please describe the nature of the action:

Rezoning of land from TA to R2

- Does the preliminary plat deviate from the requirements of the Subdivision Regulations of the City of Valley or the City's Design Standards? Yes No . If yes, please state each deviation, how the proposal meets the intent of the subdivision ordinance and why the proposal should be accepted (additional sheets may be added):

Block length and front yard setback (explanation in transmittal letter)

- Is any part of the land within the preliminary plat within a flood plain? Yes No . If yes, please include the following information: Hydrological and grade information to determine frequency and extent of inundation of flood waters; location of proposed use and type of use; areas of habitation and employment to include location, size and floor elevation of any structures, location and elevation of parking areas, use, location and elevation of open space; all plans and other information conform to Development Standards; limits of the flood plain; amount of Fill Material brought into the flood plain; a certificate that grading will not result in any increase in the flood plain. (additional sheets may be added).

Drainage Report with floodplain information is included with submittal.

Applicant's Signature: *Melvin*

Date: 05-04-2026

PRELIMINARY PLAT APPLICANT'S TECHNICAL CHECKLIST
Valley, Nebraska

NAME OF PRELIMINARY PLAT: Lago Villaggio

LOCATION OF SUCH PLAT: N. 252nd Street and King Lake Road

REQUIREMENTS OF PRELIMINARY PLAT

- Letter or transmittal with action requested.
- Name of Subdivision on each sheet
- Names and addresses of owners/applicants
- Names of adjacent property owners
- Name of engineer or surveyor
- Names of utility companies to be involved
- North arrow and graphic scale (according to sub-division regulations' specs)
- Date prepared
- A vicinity sketch at a scale no greater than one (1) inch = four hundred (400) feet and no smaller than one (1) inch = two thousand (2000) feet
- Sheet number and total number of sheets
- Contour lines (within and adjacent to subdivision, not greater than 2 feet intervals)
- Boundary lines of area to be subdivided and their bearings and distances, lot lines and lot numbers
- Is proposed use of preliminary plat in accordance with permitted City's zoning? Is a zoning action required?
- Existing and proposed streets or access easements to include:
 - R.O.W. width (according to future land use and transportation plans)
 - Paving width (according to future land use and transportation plans)
 - Approximate grades
 - Tangent length
 - Curve data and interior angle
 - Angle of Intersection
 - Name or number
- Sketch of future street system outside of plat where subdivision owns abutting land
- Utilities on and adjacent to the tract showing proposed connections to existing utilities. Rear easements for utility poles and wires shall be required whenever possible. All easements shall meet regulations set forth in subdivision regulations.
- Lot lines and dimensions; curvilinears should be noted as arc or chord
- Lot, block and outlot designation and total acreage of land to be subdivided and square feet in each lot
- Public areas for schools, parks, playgrounds or any other public uses
- All existing and proposed easements with locations, widths and distances
- All existing and proposed sidewalks, pedestrian ways and bikeways
- Location of all existing buildings and structures, easements, roads and major natural features on or within one hundred (100) feet of the boundaries of, within/adjacent to on or within one hundred (100) feet of the proposed subdivision. Any other interest in land surrounding the preliminary plat?
- Minimum building set back lines
- Erosion and sediment control plan
- Certified accurate survey by engineer or professional land surveyor
- All plans and other information conform to the City's General Requirements and Minimum Standards of Design
- Sketch of proposed subdivision sized 8 ½ inches by 14 inches

PRELIMINARY PLAT APPLICANT'S TECHNICAL CHECKLIST (cont.)

Valley, Nebraska

ADDITIONAL INFORMATION (to be shown on Preliminary Plat or on accompanying sheets)

- Four (4) copies of Preliminary Plat and required supplementary material filed with City Clerk
- Submit plat thirty (30) days prior to the meeting of the Planning commission
- Copy of application, site plan, preliminary plat, etc to proper agencies and authorities for review
 - City Engineer
 - City Council
 - City Attorney
 - Fire Department
 - County Engineer
 - County Health Department
 - Adjacent jurisdictions
 - Papio-Missouri River NRD
 - NDOR
 - FEMA
 - Planning Commission
 - Traffic engineering
 - OPPD
 - Building and safety
 - School District
 - other
- Any required landscape screens
- Copies of proposed Deed Restrictions, if any
- Sketch plan of larger tract and future concepts of development, if part of a larger tract that will be subdivided in the future

IF PLAT IS LOCATED WITHIN THE FLOOD PLAIN

- Hydrological and grade information to determine frequency and extent of inundation of flood waters
- Location of proposed use and type of use
- Areas of habitation and employment to include:
 - Location size and floor elevation of any structures
 - Location and elevation of parking areas
 - Use, location and elevation of open space
- Limits of the flood plain
- Amount of Fill Material brought into the flood plain
- A certificate that grading will not result in any increase in the flood plain

ANY OTHER INFORMATION OR COMMENTS RELATED TO THE PRELIMINARY PLAT:

(Please use additional sheet if necessary)

Section 3.03 Preliminary Plat Specifications.

The Preliminary Plat shall be drawn to a scale of at least one inch to 100 feet with a sheet size not to exceed 42"x30" and shall be plainly marked "Preliminary Plat" and shall include, show, or be accompanied by the following information:

- 3.03.01 A location map showing the general location of the proposed subdivision in relation to surrounding developments with a north arrow, scale and legend.
- 3.03.02 Both existing and proposed grades shall be shown.
- 3.03.03 Phasing lines shall be delineated on the plat and a phasing schedule, if developed in phases.
- 3.03.04 The proposed name of the subdivision which must not be as similar to that of an existing subdivision as to cause confusion. The developer shall verify the name with Douglas County.
- 3.03.05 The proposed names and addresses of the owner and subdivider; the engineer, surveyor, or landscape architect responsible (all of which are licensed to practice in Nebraska) for the subdivision layout.
- 3.03.06 The legal description of the area being platted, and boundary line (accurate in scale) and dimensions, and the location of monuments found or set, section lines, existing and the approximate acreage of the proposed development. All work shall be certified by a Registered Land Surveyor in the State of Nebraska.

- 3.03.07 Width and location of platted streets and alleys within 200 feet of the property; physical features of the property, including location of water courses, ravines, bridges, culverts, present structures and other features affecting the subdivision; contours with intervals of five feet or less; the location of all existing utilities with their sizes indicated, as well as flow lines; elevations of existing sanitary and storm sewer, the outline of wooded areas (the location of important individual trees may be required).
- 3.03.08 Location and name(s) of adjoining subdivision(s) or undeveloped land and owners and persons having ownership interest within 300 feet of the subject property (not including streets and right-of-ways). This should be submitted in list form and as mailing labels.
- 3.03.09 The proposed lot layout, lot and block numbers and approximate lot dimensions and square footage and grounds proposed to be dedicated for public use, such as schools, parks, pathways, playgrounds and streets.
- 3.03.10 The location and width of proposed streets, all easements including buffer easements, building setback lines, Rights of Way, corner radii, pavement width, thickness and type, sidewalks, alleys, location of all proposed improvements including: sanitary sewers, water mains, storm water drainage and other features and improvements required by this ordinance.
- 3.03.11 Easements for public utility and rights-of-way purposes. The book and page number of existing easements shall be labeled on the plan and any private easements should be labeled as such.
- 3.03.12 Both existing and proposed grades shall be shown.
- 3.03.13 All established floodway, floodway-fringe, and flood plain overlay lines.
- 3.03.14 The existing zoning classification and proposed uses of land within the proposed subdivision shall also be designated.
- 3.03.15 Three copies of a draft erosion control plan. Grading activities shall not proceed until said plan has been approved by the State of Nebraska and the Papio-Missouri River Natural Resources District and a copy of said approvals have been submitted to the City.
- 3.03.16 Three copies of a draft erosion control plan. Grading activities shall not proceed until said plan has been approved by the State of Nebraska and the Papio-Missouri River Natural Resources District and a copy of said approval has been submitted to the City.
- 3.03.17 Signature block indicating approval of the Planning Commission per Section 10.03
- 3.03.18 Signature block indicating approval of the City Engineer per Section 10.07
- 3.03.19 Requests for waivers of design standards.
- 3.03.20 Traffic impact analysis study may be required by the City Engineer.
- 3.03.21 Four copies of the following to the City for review at the time of pre-plat submittal :
 - 1. A preliminary sanitary sewer plan.
 - 2. A preliminary drainage study, within the subdivision.
 - 3. A preliminary street profile plan with a statement of proposed street improvements.

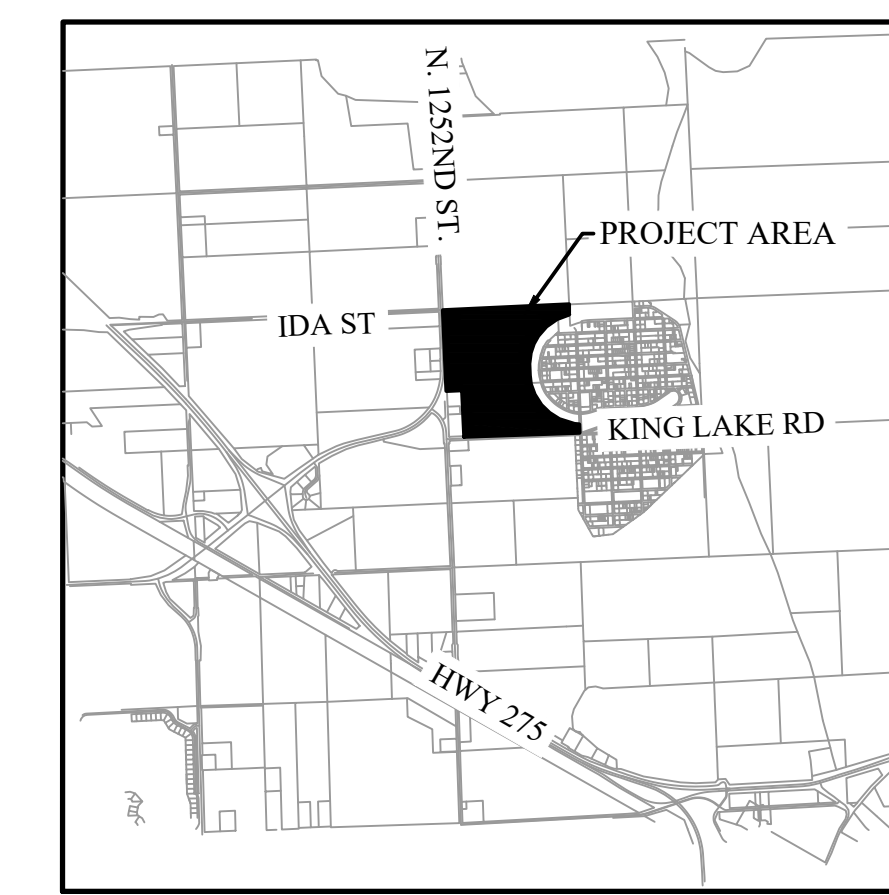
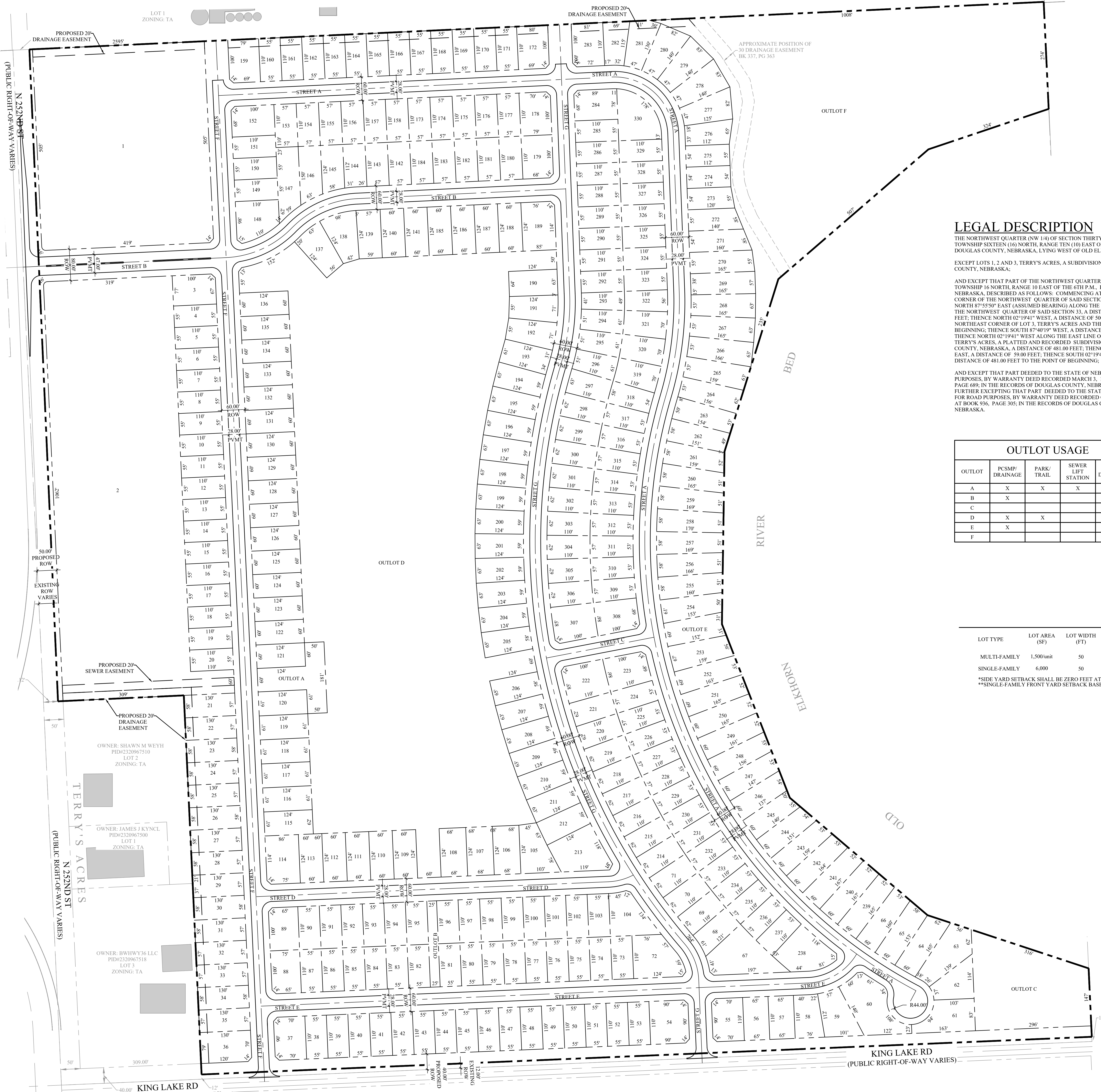
Applicant's Signature(s):

M. Williams

Date: 05-04-2026

LAGO VILLAGGIO

PRELIMINARY PLAT
LOTS 1-330 AND OUTLOTS A-F



VICINITY MAP
NOT TO SCALE

LEGAL DESCRIPTION

THE NORTHWEST QUARTER (NW 1/4) OF SECTION THIRTY-THREE (33), TOWNSHIP SIXTEEN (16) NORTH, RANGE TEN (10) EAST OF THE 6TH P.M., DOUGLAS COUNTY, NEBRASKA, DESCRIBED AS FOLLOWS: COMMENCING AT THE SOUTHWEST CORNER OF THE NORTHWEST QUARTER OF SAID SECTION 33, THENCE NORTH 87°55'59" EAST (ASSUMED BEARING) ALONG THE SOUTH LINE OF THE NORTHWEST QUARTER OF SAID SECTION 33, A DISTANCE OF 359.00 FEET; THENCE NORTH 02°19'41" WEST, A DISTANCE OF 500.60 FEET TO THE NORTHEAST CORNER OF LOT 3, TERRY'S ACRES AND THE POINT OF BEGINNING; THENCE SOUTH 87°40'19" WEST, A DISTANCE OF 59.00 FEET; THENCE NORTH 02°19'41" WEST ALONG THE EAST LINE OF LOTS 1 AND 2, TERRY'S ACRES, A PLATTED AND RECORDED SUBDIVISION IN DOUGLAS COUNTY, NEBRASKA, A DISTANCE OF 481.00 FEET; THENCE NORTH 87°40'19" EAST, A DISTANCE OF 59.00 FEET; THENCE SOUTH 02°19'41" EAST, A DISTANCE OF 481.00 FEET TO THE POINT OF BEGINNING;

AND EXCEPT THAT PART DEEDED TO THE STATE OF NEBRASKA FOR ROAD PURPOSES, BY WARRANTY DEED RECORDED MARCH 3, 1952 AT BOOK 906, PAGE 689 IN THE RECORDS OF DOUGLAS COUNTY, NEBRASKA, AND FURTHER EXCEPTING THAT PART DEEDED TO THE STATE OF NEBRASKA FOR ROAD PURPOSES, BY WARRANTY DEED RECORDED OCTOBER 27, 1953 AT BOOK 936, PAGE 305, IN THE RECORDS OF DOUGLAS COUNTY, NEBRASKA.

OUTLOT USAGE				
OUTLOT	PCSPM/ DRAINAGE	PARK/ TRAIL	SEWER LIFT STATION	FUTURE DEVELOPMENT
A	X	X	X	
B	X			
C				X
D	X	X		
E	X			
F				X

OWNER

LEWIS TERRY & ETAL TR
1111 STERLING RIDGE DR #264
OMAHA, NE 68144

APPLICANT

MELVIN SUDBECK HOMES, INC.
1625 WOODLAND DR
OMAHA, NE 68136

ENGINEER

FOLEYSHALD ENGINEERING
3930 S 147TH STREET, SUITE 200
OMAHA, NE 68144
402-404-3993

SURVEYOR

ALPHA LAND SURVEYING
10619 BURR CIRCLE
OMAHA, NE 68114
402-217-5816

ZONING

EXISTING:
AG (AGRICULTURAL DISTRICT)
PROPOSED:
R2 - (MEDIUM/HIGH DENSITY RESIDENTIAL)

UTILITY COMPANIES

POWER - OMAHA PUBLIC POWER DISTRICT
GAS - BLACK HILLS ENERGY
WATER - CITY OF VALLEY
SEWER - CITY OF VALLEY

WAIVERS

- BLOCK LENGTH
- FRONT YARD SETBACK
- DECREASE FRONT YARD AND STREET SIDE YARD SETBACK FROM 30' TO 25' ON SINGLE-FAMILY LOTS

NOTES

- ALL DISTANCES SHOWN ARE IN DECIMAL FEET.
- ALL DISTANCES SHOWN ALONG CURVES ARE ARC DISTANCES, NOT CHORD DISTANCES.
- ALL EXISTING EASEMENTS ARE NOT BEING REDEDICATED AND ARE SHOWN FOR REFERENCE ONLY.
- PROPOSED STORM SEWER AND DRAINAGE EASEMENTS AND SANITARY SEWER EASEMENTS TO CITY OF VALLEY VIA SEPARATE RECORDED INSTRUMENTS.
- LOTS ARE CONCEPTUAL AND EXACT LOT LINES MAY VARY AT TIME OF FINAL PLAT.
- LOT REQUIREMENTS INCLUDING PROPOSED SETBACKS ARE SHOWN ON THE DEVELOPMENT PLAN, EXHIBIT A.

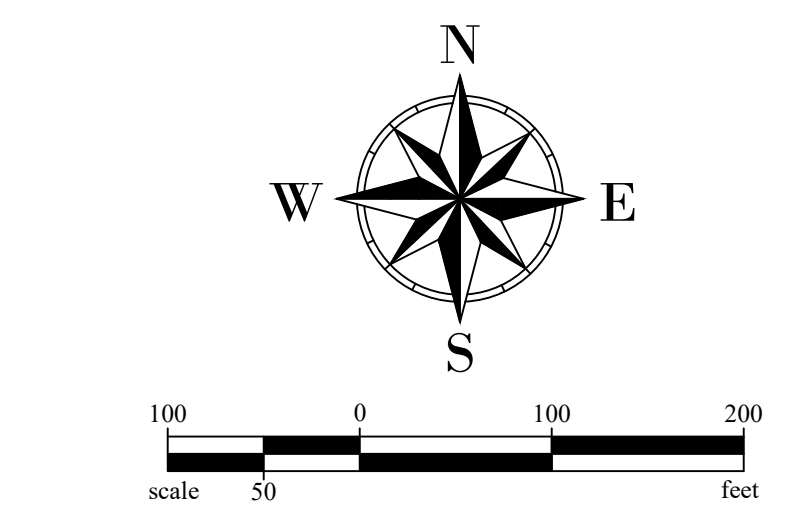
LOT REQUIREMENTS

LOT TYPE	LOT AREA (SF)	LOT WIDTH (FT)	FRONT YARD (FT)	SIDE YARD (FT)	REAR YARD (FT)	STREET SIDE YARD (FT)	MAX HEIGHT (FT)	MAX BUILDING AREA (%)	MAX IMPERVIOUS AREA (%)
MULTI-FAMILY	1,500/unit	50	30	10	25	30	45	35	50
SINGLE-FAMILY	6,000	50	25**	5*	25	25**	35	40	50

*SIDE YARD SETBACK SHALL BE ZERO FEET AT COMMON WALLS
**SINGLE-FAMILY FRONT YARD SETBACK BASED ON WAIVER REQUEST

LEGEND

- EXISTING PROPERTY LINE
- EXISTING SANITARY SEWER
- EXISTING GAS LINE
- EXISTING UNDERGROUND ELECTRIC
- EXISTING OVERHEAD ELECTRIC
- EXISTING WATER MAIN
- EXISTING STORM SEWER
- EXISTING COMMUNICATION LINES
- EXISTING FENCE
- PROPOSED PROPERTY LINE
- PROPOSED BACK OF CURB
- PROPOSED GUTTER
- PROPOSED RIGHT OF WAY
- PROPOSED ROAD CENTERLINE
- PROPOSED PROPERTY SETBACK
- LIMITS OF PRELIMINARY PLAT



FoleyShald
ENGINEERING
3930 S. 147TH STREET, SUITE 200 | OMAHA, NE 68144

SUDBECK
COMPANIES

SHEET INDEX

NO.	DESCRIPTION
1 OF 2	PRELIMINARY PLAT
2 OF 2	LOT AREA TABLE
EXHIBIT A	EXISTING CONDITIONS
EXHIBIT B	GRADING & DRAINAGE PLAN
EXHIBIT C	EROSION CONTROL PLAN
EXHIBIT D	UTILITY PLAN
EXHIBIT E	PAVING & STORM SEWER PLAN
EXHIBIT F	PHASING PLAN

REVISIONS

NUMBER	DATE	DESCRIPTION
	11/14/2025	ORIGINAL
	05/12/2026	RESUBMITTAL

Lago Villaggio
Valley, NE
FSE: CA-4197
FSE #: 104.001
May 12, 2026

Preliminary
NOT FOR CONSTRUCTION

LAGO VILLAGGIO
PRELIMINARY PLAT
LOTS 1-330 AND OUTLOTS A-F



PARCEL TABLE		
PARCEL #	AREA (SF)	
1	226148	5.19
2	359678	8.26
3	8452	0.19
4	6050	0.14
5	6050	0.14
6	6050	0.14
7	6050	0.14
8	6050	0.14
9	6050	0.14
10	6050	0.14
11	6050	0.14
12	6050	0.14
13	6050	0.14
14	6050	0.14

PARCEL TABLE		
PARCEL #	AREA (SF)	
15	6050	0.14
16	6050	0.14
17	6050	0.14
18	6050	0.14
19	6050	0.14
20	6050	0.14
21	7484	0.17
22	7483	0.17
23	7482	0.17
24	7481	0.17
25	7480	0.17
26	7478	0.17
27	7477	0.17
28	7476	0.17

PARCEL TABLE		
PARCEL #	AREA (SF)	
29	7475	0.17
30	7475	0.17
31	7475	0.17
32	7475	0.17
33	7475	0.17
34	7475	0.17
35	7475	0.17
36	10272	0.24
37	8726	0.20
38	6050	0.14
39	6050	0.14
40	6050	0.14
41	6050	0.14
42	6050	0.14

PARCEL TABLE		
PARCEL #	AREA (SF)	
43	6050	0.14
44	6050	0.14
45	6050	0.14
46	6050	0.14
47	6050	0.14
48	6050	0.14
49	6050	0.14
50	6050	0.14
51	6050	0.14
52	6050	0.14
53	6050	0.14
54	10857	0.25
55	8700	0.20
56	7150	0.16

PARCEL TABLE		
PARCEL #	AREA (SF)	
57	7150	0.16
58	7581	0.17
59	9463	0.22
60	15830	0.36
61	10643	0.24
62	8175	0.19
63	11837	0.27
64	9543	0.22
65	9210	0.21
66	9153	0.21
67	15331	0.35
68	6749	0.15
69	6530	0.15
70	6530	0.15

PARCEL TABLE		
PARCEL #	AREA (SF)	
71	6530	0.15
72	12315	0.28
73	6050	0.14
74	6050	0.14
75	6050	0.14
76	6050	0.14
77	6050	0.14
78	6050	0.14
79	6050	0.14
80	6050	0.14
81	6050	0.14
82	6050	0.14
83	6050	0.14
84	6050	0.14

PARCEL TABLE		
PARCEL #	AREA (SF)	
85	6050	0.14
86	6050	0.14
87	6050	0.14
88	8174	0.19
89	8226	0.19
90	6050	0.14
91	6050	0.14
92	6050	0.14
93	6050	0.14
94	6050	0.14
95	6050	0.14
96	6050	0.14
97	6050	0.14
98	6050	0.14

PARCEL TABLE		
PARCEL #	AREA (SF)	
99	6050	0.14
100	6050	0.14
101	6050	0.14
102	6050	0.14
103	6050	0.14
104	10314	0.24
105	9308	0.21
106	8432	0.19
107	8432	0.19
108	8432	0.19
109	7440	0.17
110	7440	0.17
111	7440	0.17
112	7440	0.17

PARCEL TABLE		
PARCEL #	AREA (SF)	
113	7440	0.17
114	10524	0.24
115	7647	0.18
116	7614	0.17
117	7614	0.17
118	7614	0.17
119	7614	0.17
120	7614	0.17
121	7440	0.17
122	7440	0.17
123	7440	0.17
124	7440	0.17
125	7440	0.17
126	7440	0.17

PARCEL TABLE		
PARCEL #	AREA (SF)	
127	7440	0.17
128	7440	0.17
129	7440	0.17
130	7440	0.17
131	7440	0.17
132	7440	0.17
133	7440	0.17
134	7440	0.17
135	7440	0.17
136	7440	0.17
137	8675	0.20
138	8774	0.20
139	7501	0.17
140	7440	0.17

PARCEL TABLE		
PARCEL #	AREA (SF)	
141	7440	0.17
142	6270	0.14
143	6270	0.14
144	6287	0.14
145	6658	0.15
146	7742	0.18
147	9739	0.22
148	10025	0.23
149	6050	0.14
150	6050	0.14
151	6050	0.14
152	8530	0.20
153	6269	0.14
154	6270	0.14

PARCEL TABLE		
PARCEL #	AREA (SF)	
155	6270	0.14
156	6270	0.14
157	6270	0.14
158	6270	0.14
159	8618	0.20
160	6050	0.14
161	6050	0.14
162	6050	0.14
163	6050	0.14
164	6050	0.14
165	6050	0.14
166	6050	0.14
167	6050	0.14
168	6050	0.14

PARCEL TABLE		
PARCEL #	AREA (SF)	
169	6050	0.14
170	6050	0.14
171	6050	0.14
172	8673	0.20
173	6270	0.14
174	6270	0.14
175	6270	0.14
176	6270	0.14
177	6270	0.14
178	8708	0.20
179	8603	0.20
180	6270	0.14
181	6270	0.14
182	6270	0.14

PARCEL TABLE		
PARCEL #	AREA (SF)	
183	6270	0.14
184	6270	0.14
185	7440	0.17
186	7440	0.17
187	7440	0.17
188	7440	0.17
189	10541	0.24
190	8047	0.18
191	7849	0.18
192	7849	0.18
193	7798	0.18
194	7573	0.17
195	7573	0.17
196	7573	0.17

PARCEL TABLE		
PARCEL #	AREA (SF)	
197	7573	0.17
198	7573	0.17
199	7573	0.17
200	7573	0.17
201	7573	0.17
202	7573	0.17
203	7573	0.17
204	7573	0.17
205	7644	0.18
206	7644	0.18
207	7573	0.17
208	7573	0.17
209	7573	0.17
210	7573	0.17

PARCEL TABLE		
PARCEL #	AREA (SF)	
211	7573	0.17
212	7573	0.17
213	13039	0.30
214	6530	0.15
215	6530	0.15
216	6530	0.15
217	6530	0.15
218	6530	0.15
219	6530	0.15
220	6530	0.15
221	6530	0.15
222	9949	0.23
223	8975	0.21
224	6049	0.14

PARCEL TABLE		
PARCEL #	AREA (SF)	
225	6049	0.14
226	6049	0.14
227	6049	0.14
228	6049	0.14
229	6049	0.14
230	6049	0.14
231	6049	0.14
232	6049	0.14
233	6049	0.14
234	6049	0.14
235	6049	0.14
236	6049	0.14
237	6049	0.14
238	11791	0.27

PARCEL TABLE		
PARCEL #	AREA (SF)	
239	9328	0.21
240	9373	0.22
241	9291	0.21
242	9077	0.21
243	8730	0.20
244	8243	0.19
245	7700	0.18
246	8060	0.19
247	8568	0.20
248	8933	0.21
249	9163	0.21
250	9263	0.21
251	9233	0.21
252	9073	0.21

PARCEL TABLE		
PARCEL #	AREA (SF)	
253	9527	0.22
254	8614	0.20
255	8933	0.21
256	9162	0.21
257	9271	0.21
258	9262	0.21
259	9135	0.21
260	8888	0.20
261	8516	0.20
262	8356	0.19
263	8568	0.20
264	9011	0.21
265	9336	0.21
266	9525	0.22

PARCEL TABLE		
PARCEL #	AREA (SF)	
267	9492	0.22
268	9492	0.22
269	9141	0.21
270	8856	0.20
271	8175	0.19
272	7085	0.16
273	6322	0.15
274	6104	0.14
275	6104	0.14
276	6777	0.16
277	8319	0.19
278	8935	0.21
279	8935	0.21
280	8524	0.20

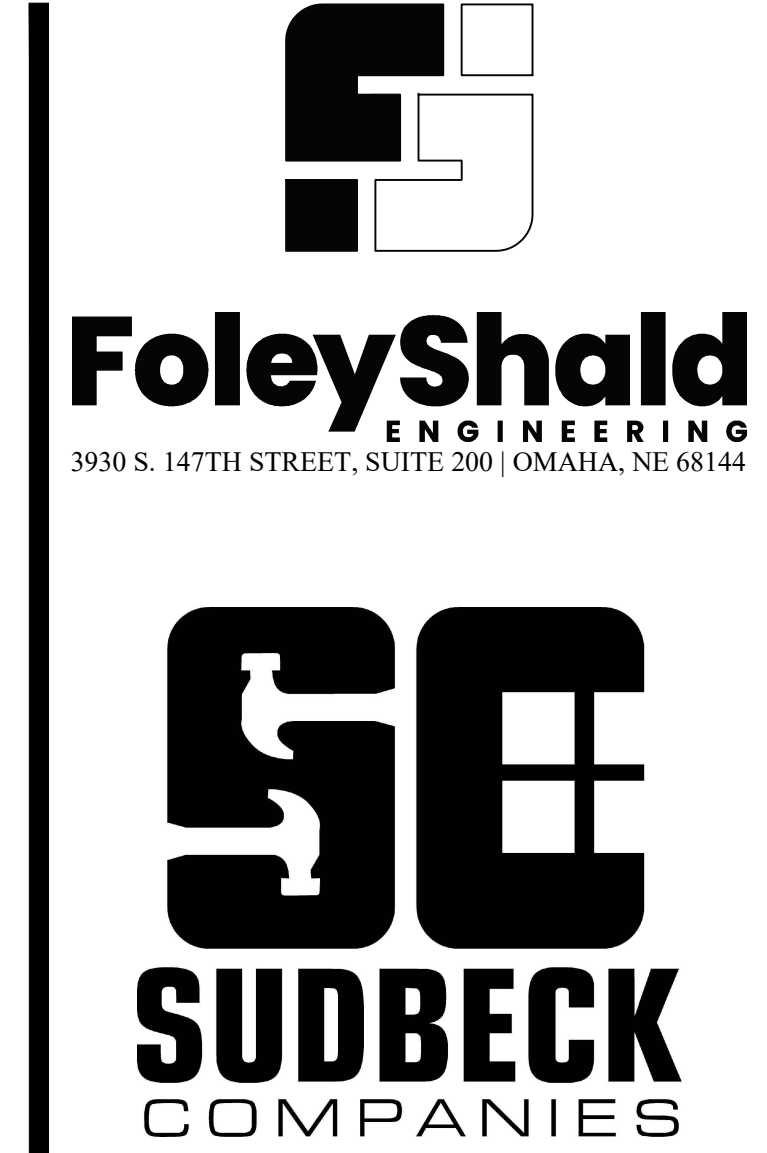
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281	7759	0.18
282	6571	0.15
283	8876	0.20
284	8540	0.20
285	6050	0.14
286	6050	0.14
287	6050	0.14
288	6050	0.14
289	6050	0.14
290	6050	0.14
291	6050	0.14
292	6050	0.14
293	4949	0.11
294	6176	0.14

PARCEL TABLE		
PARCEL #	AREA (SF)	
295	6176	0.14
296	6176	0.14
297	6669	0.15
298	6530	0.15
299	6530	0.15
300	6530	0.15
301	6530	0.15
302	6530	0.15
303	6530	0.15
304	6530	0.15
305	6530	0.15
306	6530	0.15
307	9910	0.23
308	8939	0.21

PARCEL TABLE		
PARCEL #	AREA (SF)	
309	6049	0.14
310	6049	0.14
311	6049	0.14
312	6049	0.14
313	6049	0.14
314	6049	0.14
315	6049	0.14
316	6049	0.14
317	6049	0.14
318	6178	0.14
319	7213	0.17
320		

LAGO VILLAGGIO

PRELIMINARY PLAT
LOTS 1-330 AND OUTLOTS A-F

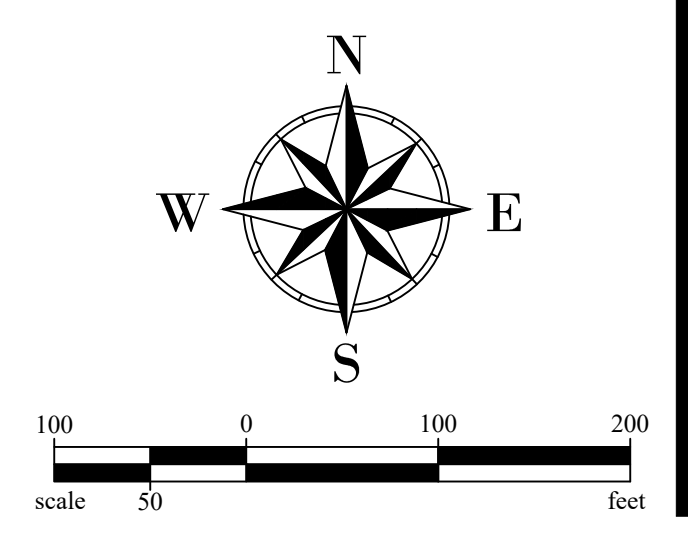
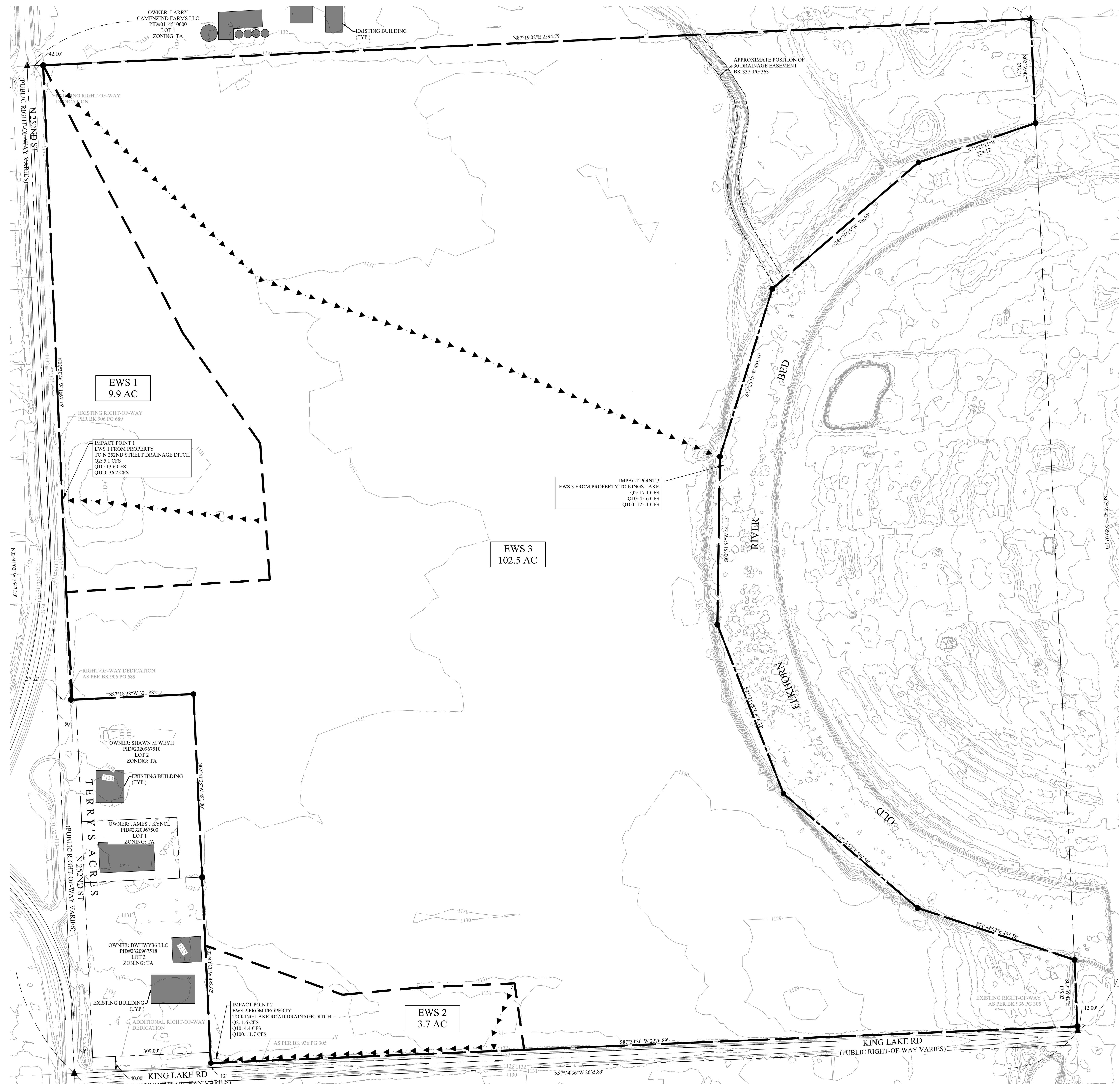


REVISIONS		
NUMBER	DATE	DESCRIPTION
	11/14/2025	ORIGINAL
	05/12/2026	RESUBMITTAL

Lago Villaggio
Valley, NE
FSE: CA-4197
FSE #: 104.001
May 12, 2026

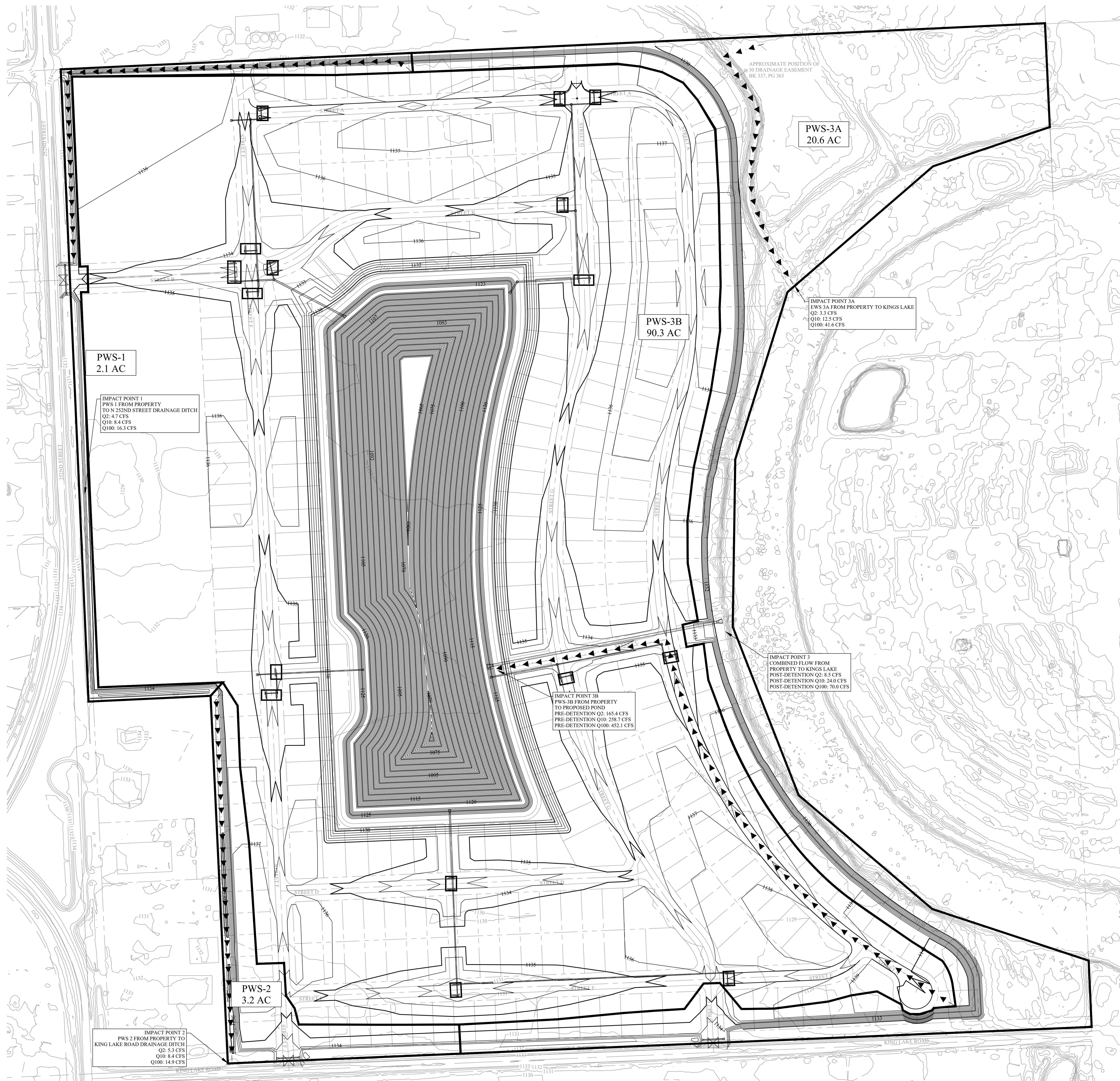
Preliminary
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Existing Conditions
Exhibit A



LAGO VILLAGGIO

PRELIMINARY FLAT
LOTS 1-330 AND OUTLOTS A-F



PWS-1
2.1 AC

IMPACT POINT 1
PWS 1 FROM PROPERTY
TO N 252ND STREET DRAINAGE DITCH
Q2: 4.7 CFS
Q10: 8.4 CFS
Q100: 16.3 CFS

PWS-3B
90.3 AC

IMPACT POINT 3B
PWS-3B FROM PROPERTY
TO PROPOSED POND
PRE-DETENTION Q2: 165.4 CFS
PRE-DETENTION Q10: 258.7 CFS
PRE-DETENTION Q100: 452.1 CFS

PWS-3A
20.6 AC

IMPACT POINT 3A
EWS 3A FROM PROPERTY TO KINGS LAKE
Q2: 3.3 CFS
Q10: 12.5 CFS
Q100: 41.6 CFS

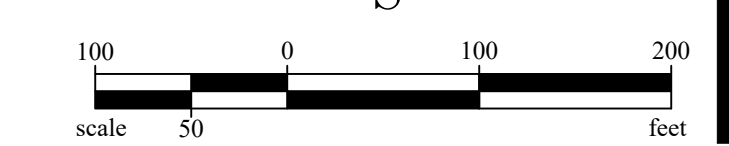
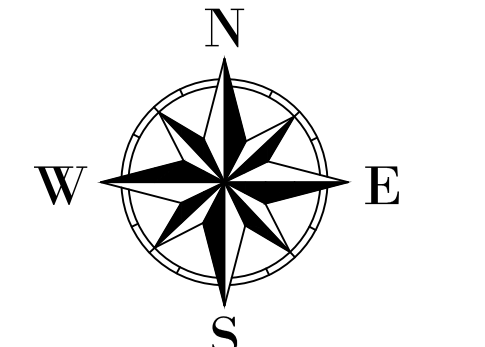
IMPACT POINT 3
COMBINED FLOW FROM
PROPERTY TO KINGS LAKE
POST-DETENTION Q2: 8.5 CFS
POST-DETENTION Q10: 24.0 CFS
POST-DETENTION Q100: 70.0 CFS

PWS-2
3.2 AC

IMPACT POINT 2
PWS 2 FROM PROPERTY TO
KING LAKE ROAD DRAINAGE DITCH
Q2: 5.3 CFS
Q10: 8.4 CFS
Q100: 14.9 CFS

LEGEND

- EXISTING MAJOR CONTOUR
- EXISTING MINOR CONTOUR
- - - EXISTING PROPERTY LINE
- SS --- EXISTING SANITARY SEWER
- GAS --- EXISTING GAS LINE
- UGE --- EXISTING UNDERGROUND ELECTRIC
- OHE --- EXISTING OVERHEAD ELECTRIC
- W --- W --- EXISTING WATER MAIN
- ST --- EXISTING STORM SEWER
- T --- T --- EXISTING TELEPHONE LINES
- EXISTING FENCE
- PROPOSED MAJOR CONTOUR
- PROPOSED MINOR CONTOUR
- PROPOSED PAVEMENT
- PROPOSED BACK OF CURB
- PROPOSED GUTTER
- PROPOSED DRAINAGE BASIN
- PROPOSED RIGHT OF WAY
- PROPOSED ROAD CENTERLINE
- PROPOSED PROPERTY SETBACK
- PROPOSED CURB INLET
- PROPOSED STORM SEWER
- PROPOSED STORM INLET
- PROPOSED WATERSHED



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3930 S. 147TH STREET, SUITE 200 | OMAHA, NE 68144

SE
SUDBECK
COMPANIES

REVISIONS		
NUMBER	DATE	DESCRIPTION
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	05/12/2026	RESUBMITTAL

Lago Villaggio

Valley, NE

FSE: CA-4197

FSE #: 104.001

May 12, 2026

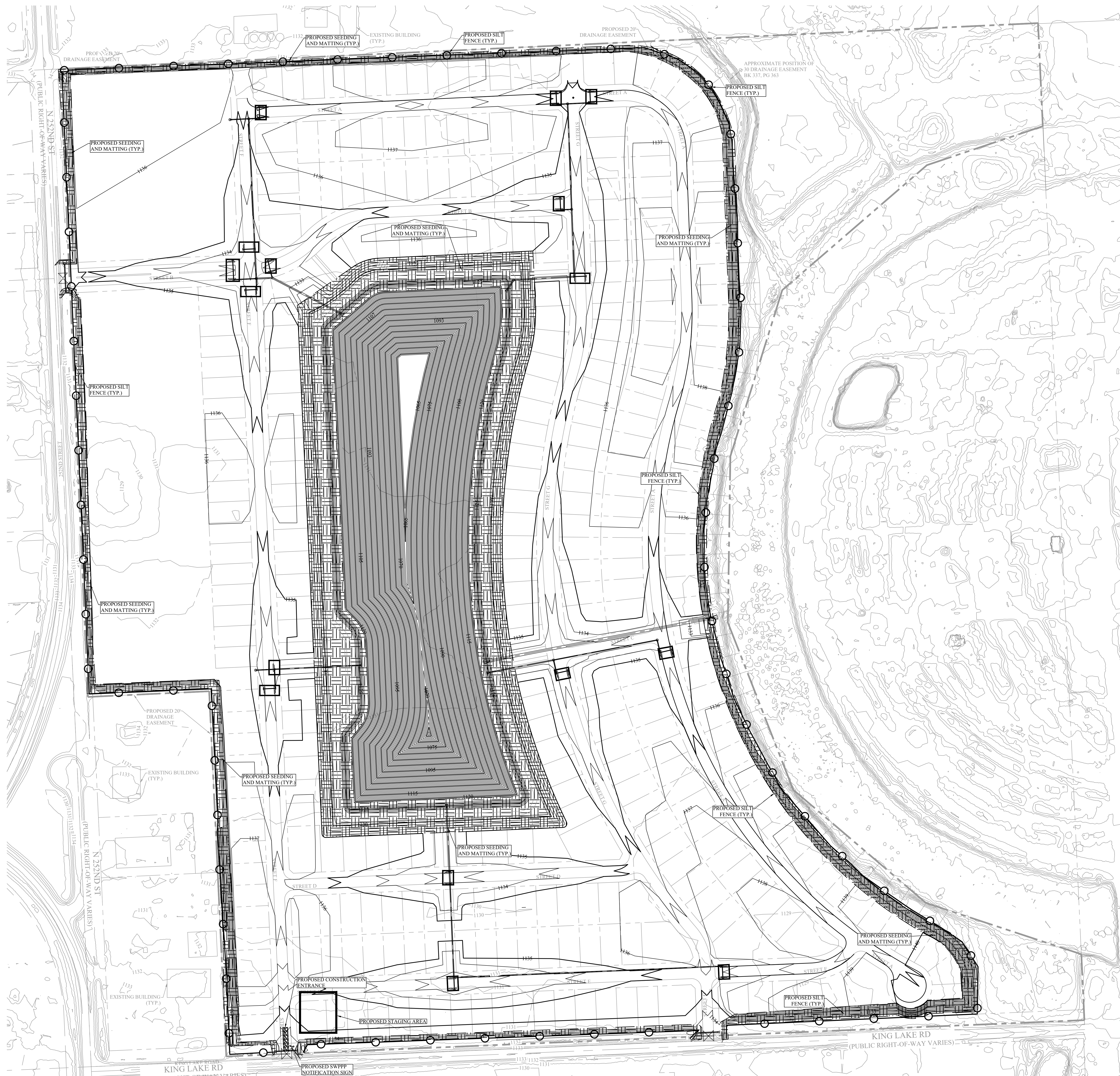
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Grading &
Drainage Plan

Exhibit B

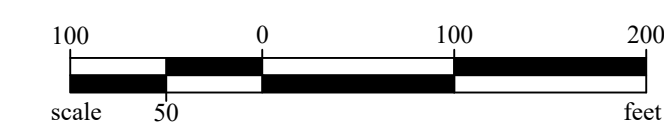
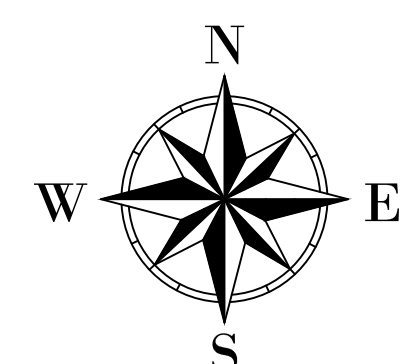
LAGO VILLAGGIO

PRELIMINARY FLAT
LOTS 1-330 AND OUTLOTS A-F



LEGEND

- EXISTING MAJOR CONTOUR
- EXISTING MINOR CONTOUR
- - - EXISTING PROPERTY LINE
- ==== PROPOSED MAJOR CONTOUR
- ==== PROPOSED MINOR CONTOUR
- ==== PROPOSED PAVEMENT
- PROPOSED SILT FENCE
- ▬ PROPOSED EROSION MATTING



REVISIONS

NUMBER	DATE	DESCRIPTION
	11/14/2025	ORIGINAL SUBMITTAL
	05/12/2026	

Lago Villaggio

Valley, NE

FSE: CA-4197

FSE #: 104.001

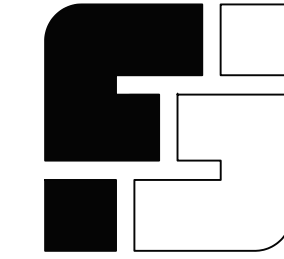
May 12, 2026

Preliminary

NOT FOR CONSTRUCTION

Erosion Control Plan

Exhibit C

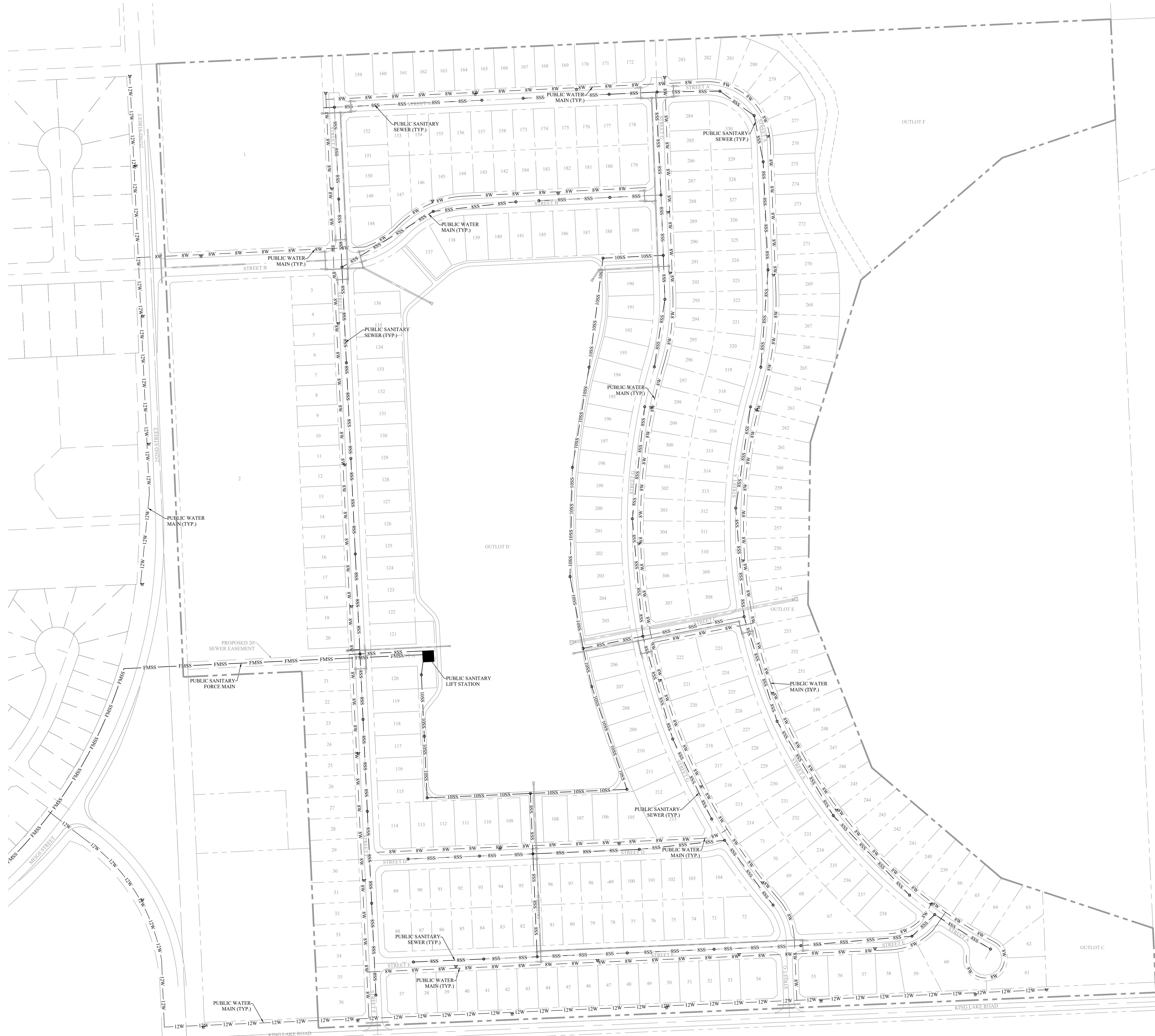


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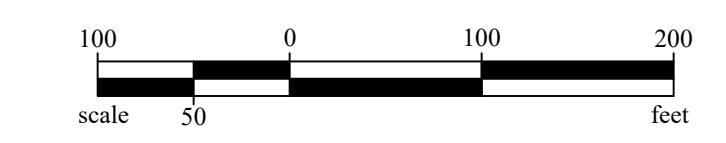
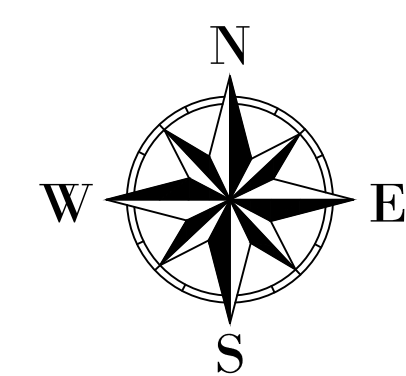


LAGO VILLAGGIO
PRELIMINARY PLAT
LOTS 1-330 AND OUTLOTS A-F



LEGEND

- EXISTING PROPERTY LINE
- EXISTING SANITARY SEWER
- EXISTING GAS LINE
- EXISTING UNDERGROUND ELECTRIC
- EXISTING OVERHEAD ELECTRIC
- EXISTING WATER MAIN
- EXISTING STORM SEWER
- EXISTING TELEPHONE LINES
- EXISTING FENCE
- PROPOSED PAVEMENT
- PROPOSED RIGHT OF WAY
- PROPOSED ROAD CENTERLINE
- PROPOSED WATER MAIN
- PROPOSED SANITARY SEWER
- PROPOSED STORM SEWER
- PROPOSED SANITARY MANHOLE
- PROPOSED FIRE HYDRANT
- PROPOSED CURB INLET
- PROPOSED PROPERTY SETBACK
- LIMITS OF PRELIMINARY PLAT



F

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ENGINEERING
3930 S. 147TH STREET, SUITE 200 | OMAHA, NE 68144

SE

SUDBECK
COMPANIES

REVISIONS

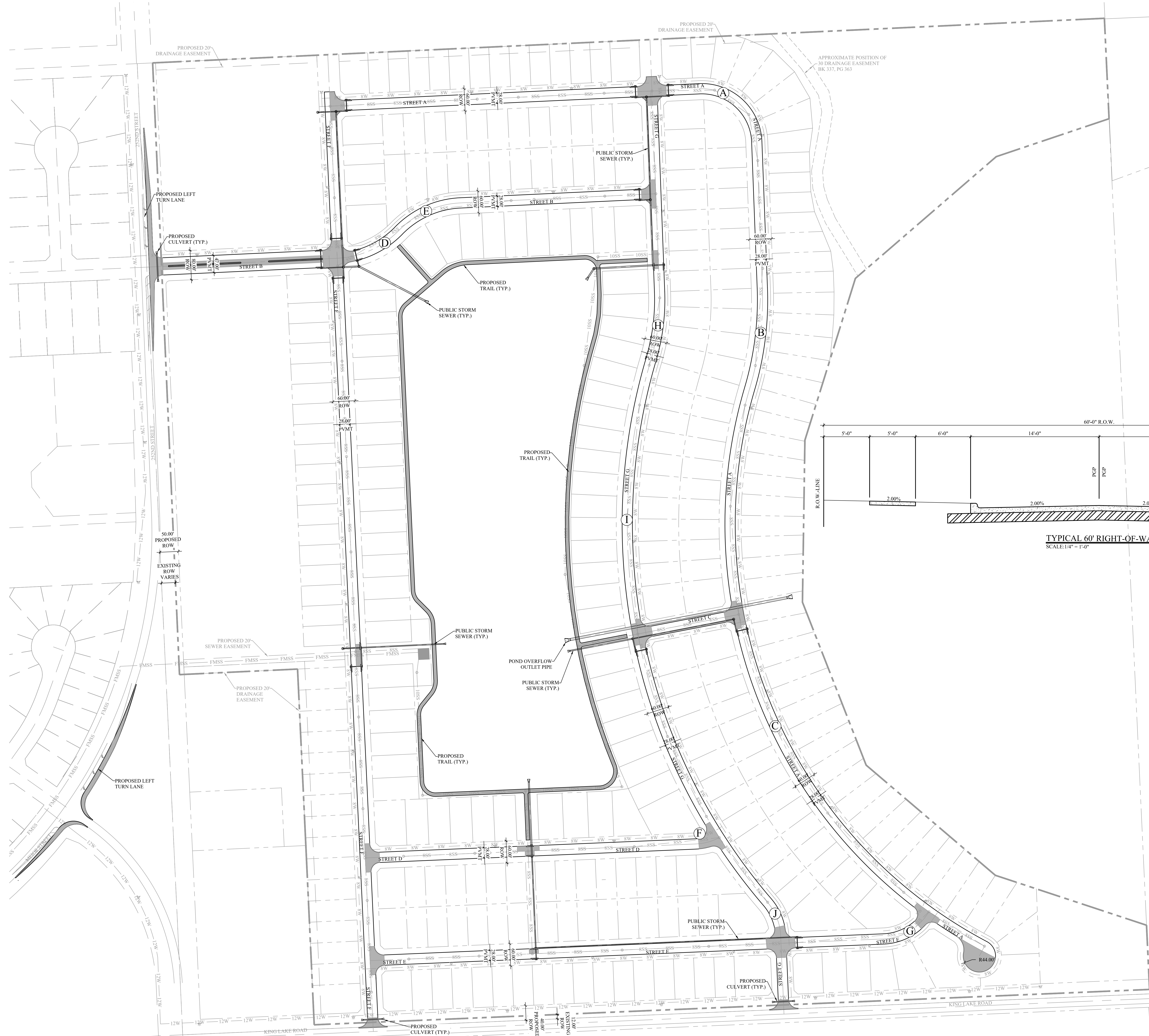
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2	05/12/2026	RESUBMITTAL

Lago Villaggio
Valley, NE
FSE: CA-4197
FSE #: 104.001
May 12, 2026

Preliminary
NOT FOR CONSTRUCTION

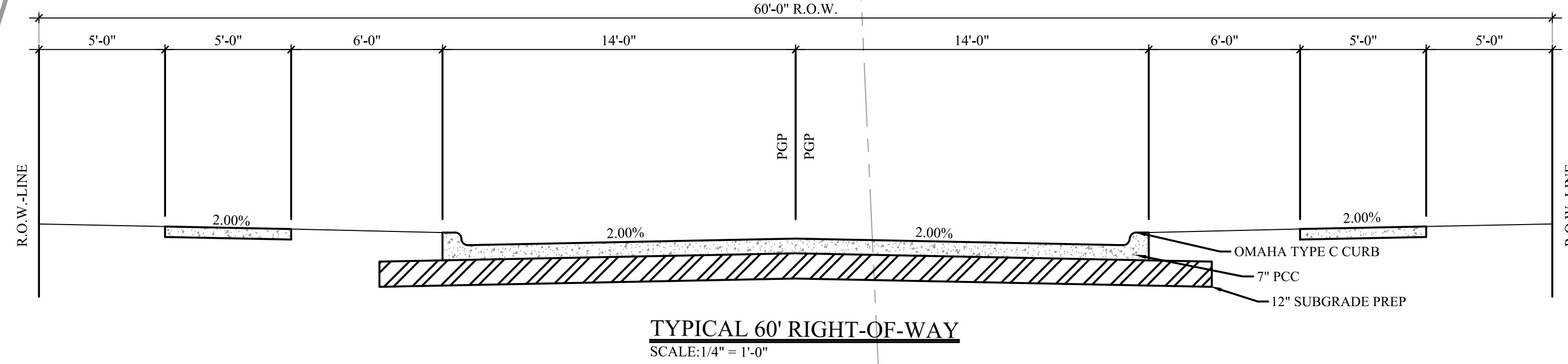
Utility Plan
Exhibit D

LAGO VILLAGGIO
PRELIMINARY PLAT
LOTS 1-330 AND OUTLOTS A-F



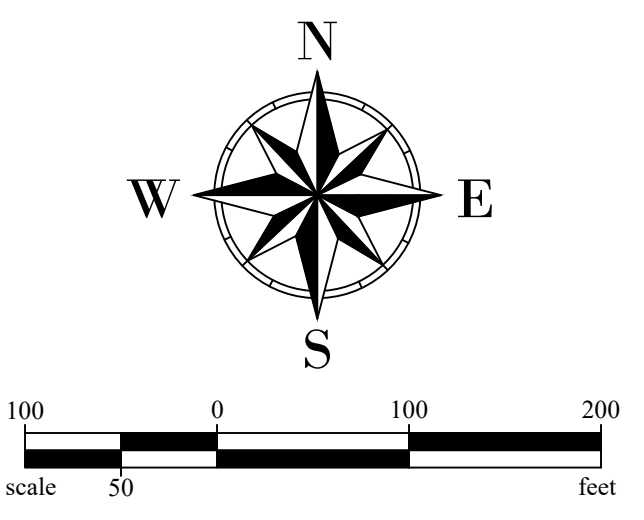
ROADWAY CENTERLINE CURVE DATA

CURVE NO.	LENGTH	RADIUS	CHORD LENGTH	DELTA
A	237.28'	150.00'	213.30'	90°38'05"
B	276.96'	850.00'	275.74'	18°40'08"
C	1784.60'	1300.00'	1647.73'	78°39'14"
D	188.04'	250.00'	183.64'	43°05'42"
E	188.00'	250.00'	183.60'	43°05'07"
F	66.63'	150.00'	66.09'	25°27'09"
G	149.90'	150.00'	143.74'	57°15'24"
H	185.73'	570.00'	184.91'	18°40'08"
I	1576.41'	1580.00'	1511.84'	57°09'56"
J	100.14'	150.00'	98.29'	38°15'08"



LEGEND

- SS — EXISTING PROPERTY LINE
- GAS — EXISTING SANITARY SEWER
- UGE — EXISTING GAS LINE
- OHE — EXISTING UNDERGROUND ELECTRIC
- W — W — EXISTING OVERHEAD ELECTRIC
- ST — EXISTING WATER MAIN
- T — T — EXISTING STORM SEWER
- — EXISTING COMMUNICATION LINES
- — EXISTING FENCE
- — PROPOSED PAVEMENT
- — PROPOSED RIGHT OF WAY
- W — W — PROPOSED ROAD CENTERLINE
- SS — PROPOSED WATER MAIN
- — PROPOSED SANITARY SEWER
- — PROPOSED STORM SEWER
- ⊕ — PROPOSED FAIRED END SECTION
- ⊕ — PROPOSED STORM MANHOLE
- ⊕ — PROPOSED SANITARY MANHOLE
- ⊕ — PROPOSED FIRE HYDRANT
- ⊕ — PROPOSED CURB INLET
- — PROPOSED PROPERTY SETBACK
- — LIMITS OF PRELIMINARY PLAT
- — GENERAL OBLIGATION
- — PCC PAVEMENT



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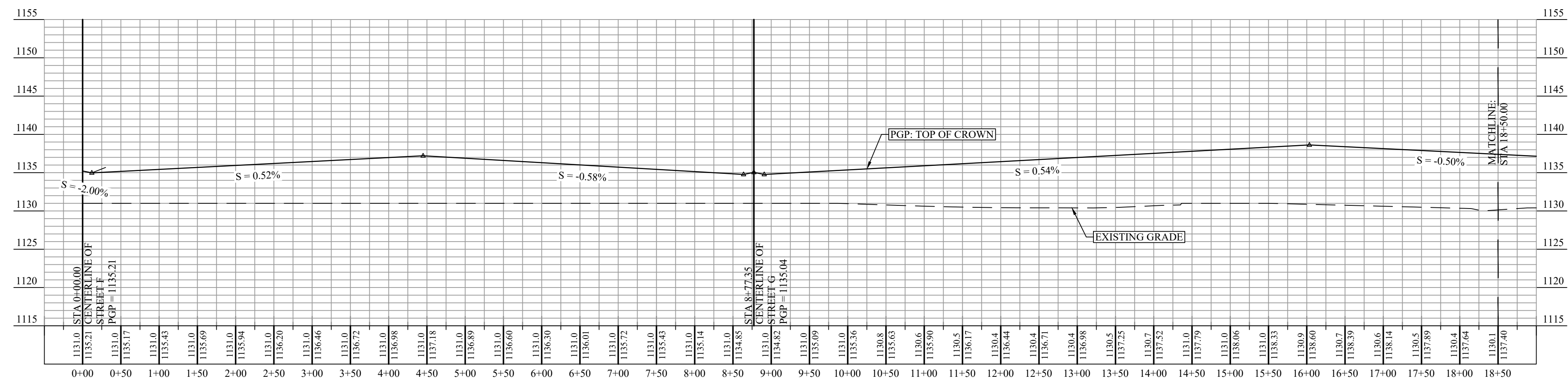
May 12, 2026

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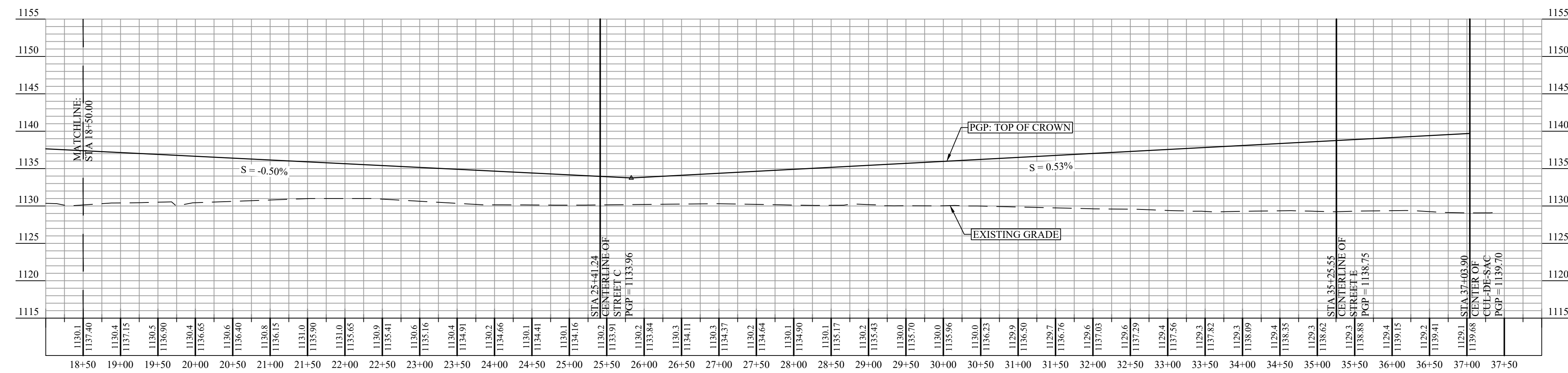
Paving &
Storm Sewer Plan

Exhibit E

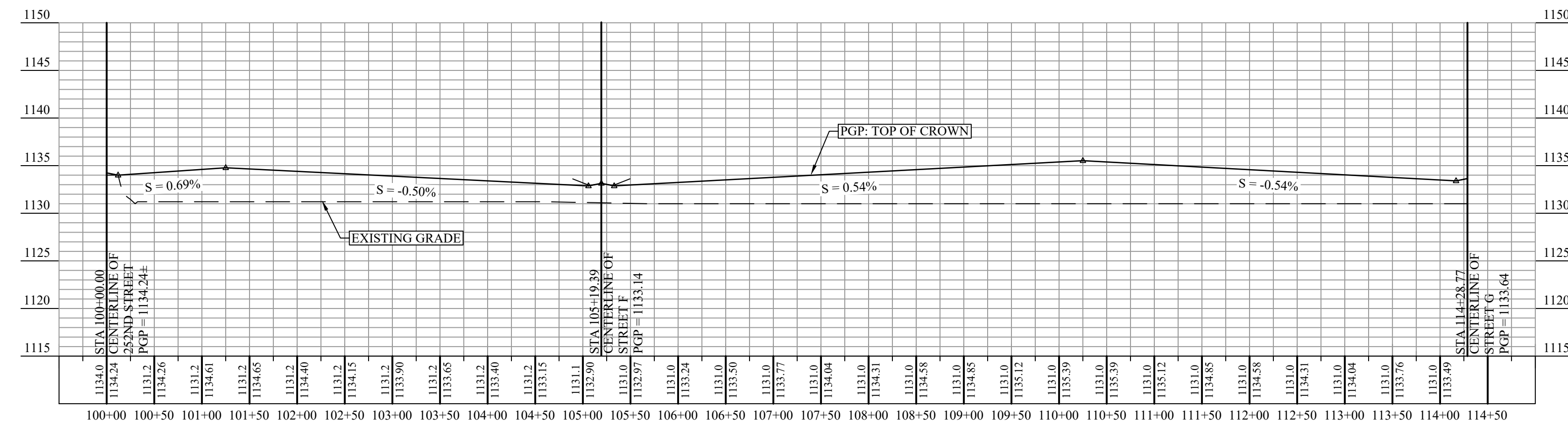
LAGO VILLAGGIO
PRELIMINARY PLAT
LOTS 1-330 AND OUTLOTS A-F



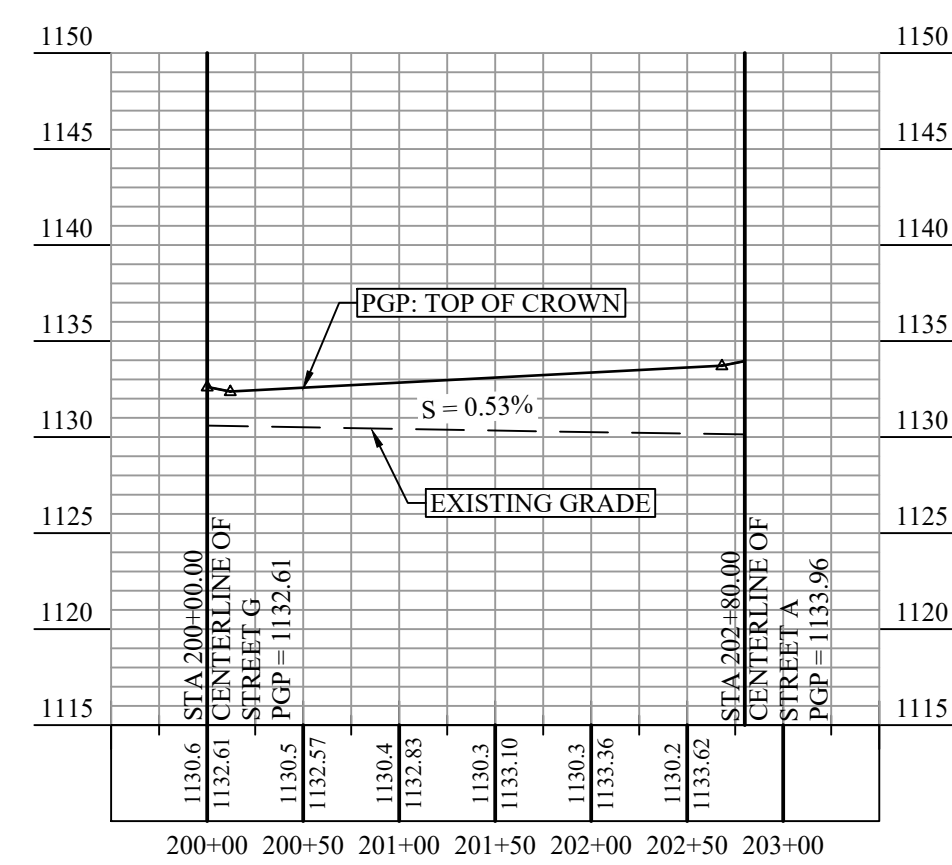
STREET A
H. SCALE: 1" = 100'-0"
V. SCALE: 1" = 10'-0"



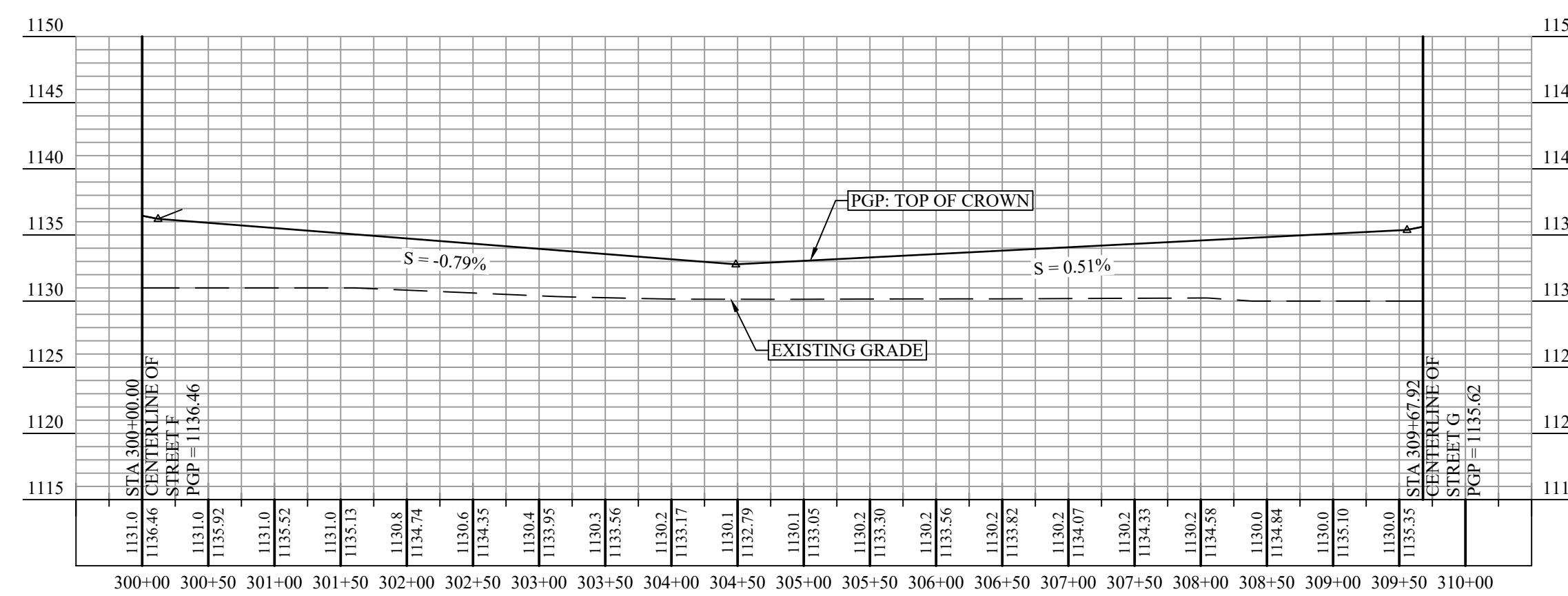
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STREET B
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V. SCALE: 1" = 10'-0"



STREET C
H. SCALE: 1" = 100'-0"
V. SCALE: 1" = 10'-0"



STREET D
H. SCALE: 1" = 100'-0"
V. SCALE: 1" = 10'-0"

REVISIONS

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Lago Villaggio

Valley, NE

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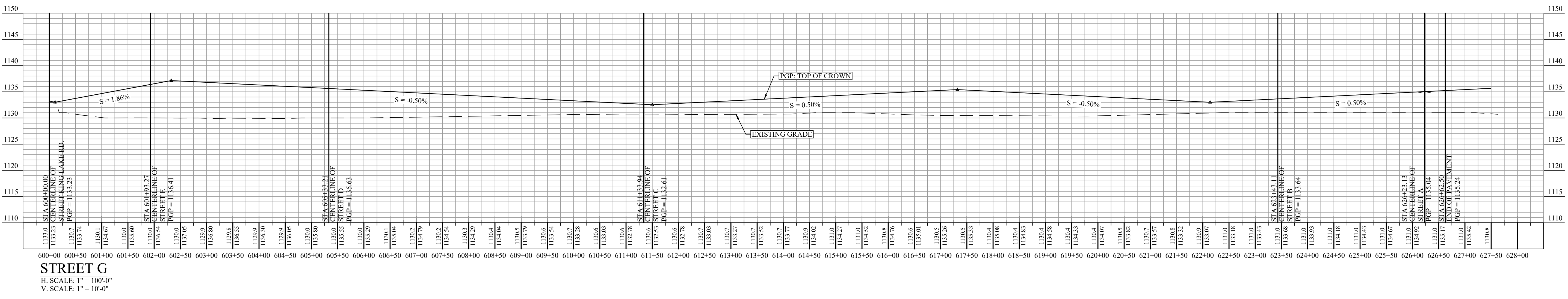
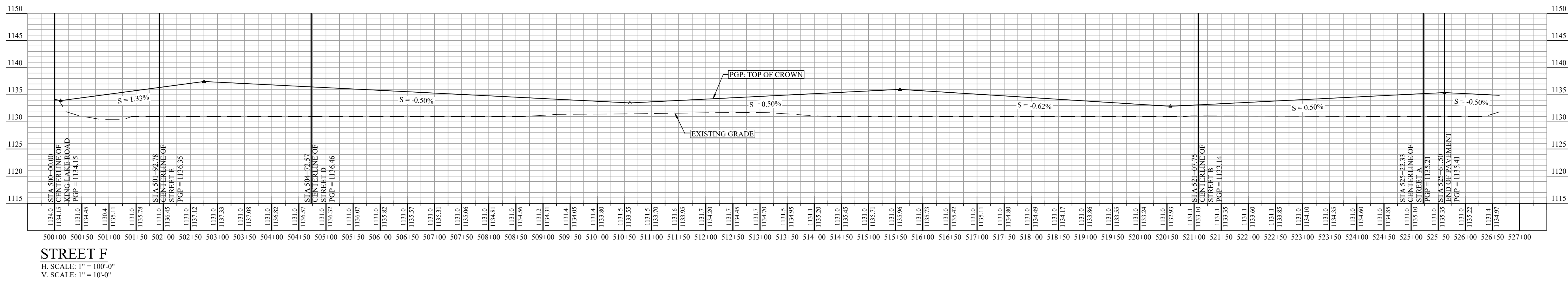
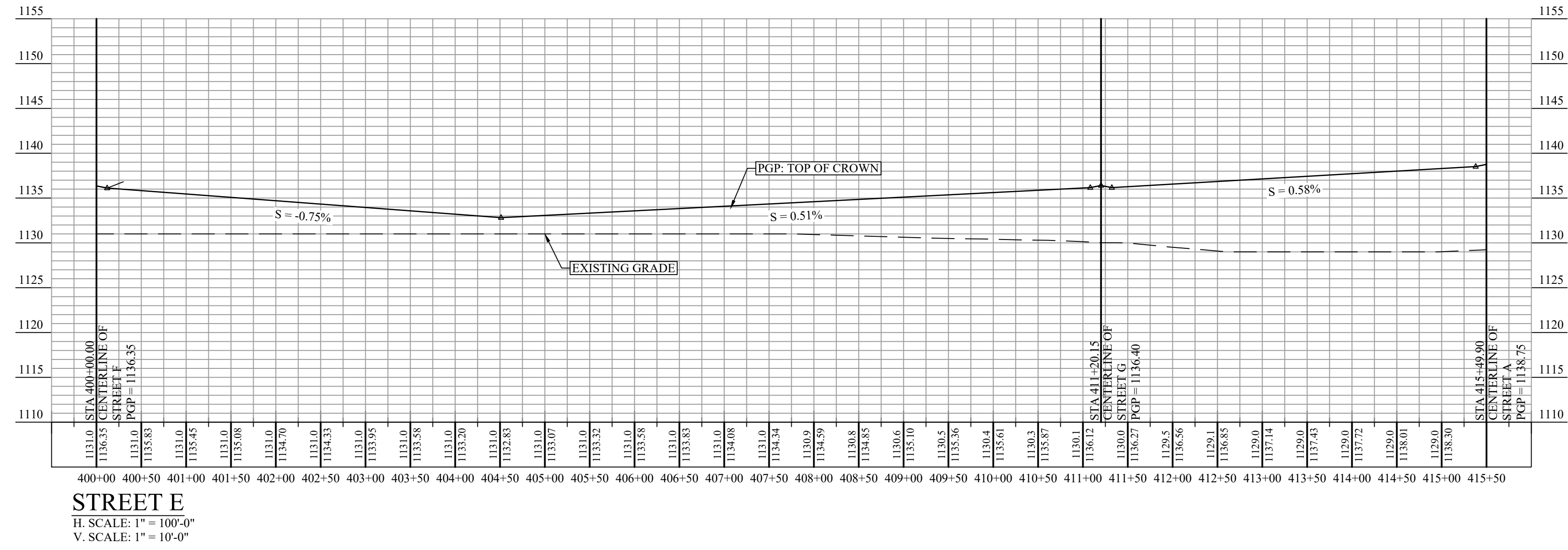
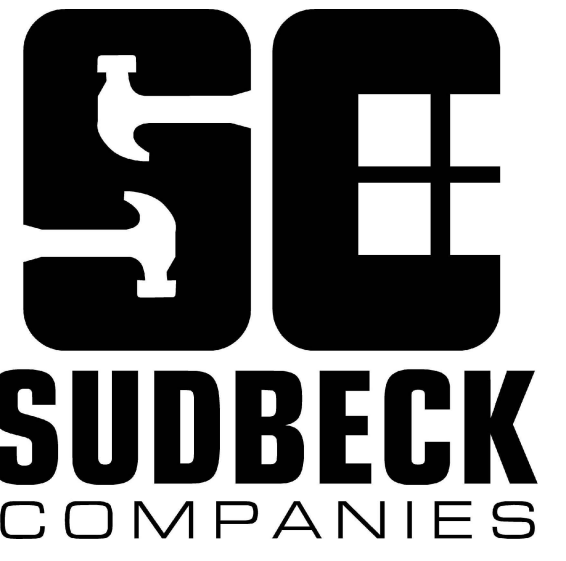
May 12, 2026

Preliminary
NOT FOR CONSTRUCTION

Street Profiles

Exhibit E1

LAGO VILLAGGIO
PRELIMINARY PLAT
LOTS 1-330 AND OUTLOTS A-F



REVISIONS

NUMBER	DATE	DESCRIPTION
	11/14/2025	ORIGINAL
	05/12/2026	RESUBMITTAL

Lago Villaggio

Valley, NE

FSE: CA-4197
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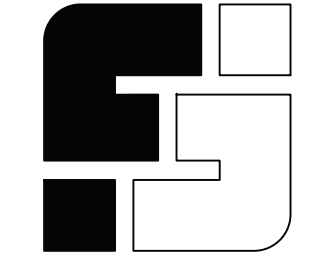
May 12, 2026

Preliminary
NOT FOR CONSTRUCTION


Street Profiles

Exhibit E2

LAGO VILLAGGIO
PRELIMINARY PLAT
LOTS 1-330 AND OUTLOTS A-F

FoleyShald
ENGINEERING
3930 S. 147TH STREET, SUITE 200 | OMAHA, NE 68144



SE
SUDBECK
COMPANIES

REVISIONS

NUMBER	DATE	DESCRIPTION
	11/14/2025	ORIGINAL
	05/12/2026	RESUBMITTAL

Lago Villaggio
Valley, NE










FSE: CA-4197
FSE #: 104.001

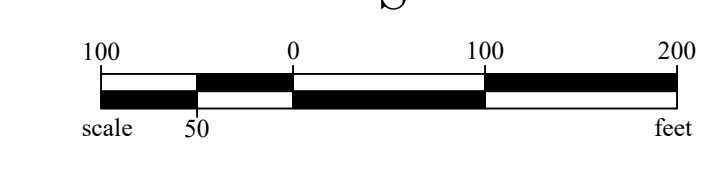
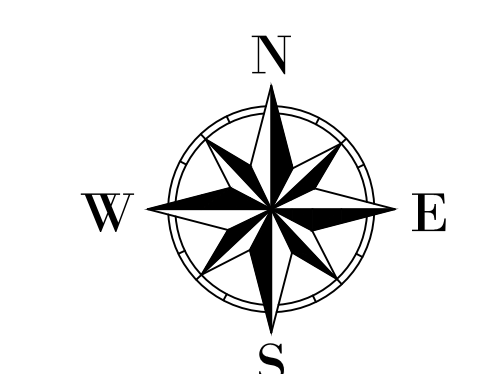
May 12, 2026

Preliminary
NOT FOR CONSTRUCTION

Phasing Plan
Exhibit F

LEGEND

-  EXISTING PROPERTY LINE
-  PROPOSED PROPERTY LINE
-  PHASE BOUNDARY
-  PROPOSED WATER MAIN
-  PROPOSED SANITARY SEWER
-  PROPOSED FIRE HYDRANT
-  PROPOSED STORM SEWER
-  PROPOSED SANITARY MANHOLE
-  PROPOSED CURB INLET





Drainage Report

Lago Villaggio

Valley, NE

FSE #104.001

May 12, 2026

Revised

Preliminary Plat Submittal
Preliminary Plat Resubmittal

11/14/2025
05/12/2026

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Lago Villaggio

1 Introduction

Lago Villaggio is a proposed residential development in Valley, Douglas County, Nebraska. The 115-acre property is located north of King Lake Road and east of N 252nd Street.

The proposed development will incorporate a mix of single family and multi-family lots. This will include new streets, drainage improvements, and associated utilities. One central pond will act as a stormwater retention basin and recreation area for the proposed site.

The majority the existing drainage flows to the east side of the site and into Kings Lake. A small area on the west side of the site drains to N 252nd Street and flows to the south, and the southwest corner drains south to King Lake Road before flowing west. All drainage areas eventually flow to the Elkhorn River.

The project is proposed to be completed in two phases, and will generally develop from west to east. A majority of the site will drain into the proposed pond in the central outlot to minimize runoff potential.

The information provided within this drainage report includes a summary of the existing and proposed development conditions of the site.

2 Existing Development Conditions

The existing site is primarily flat farmland. Along most of the western side of the site, N 252nd Street is a two-lane paved roadway with ditches and culverts running adjacent, and an existing overhead powerline on the west side of the road. The southern boundary of the site is King Lake Road, a two-lane paved roadway with ditches and culverts, and an overhead powerline on the north side of the road. To the east of the site lies Kings Lake where most of the existing runoff currently flows.

The NRCS Web Soil Survey Website identifies the site consists of two primary soil groups. Cass-Wann fine sandy loams and Gilliam-Eudora silt loams. Both are occasionally flooded with a hydrologic soil group (HSG) rating of A and comprise about 87% of the site. There are small pockets of Cass-Novina complex with HSG B on site. The soils along the old Elkhorn riverbed are recognized as frequently flooded Platte, Inglewood, and Barney soils. Appendix A includes the soils map and the results of the web soil survey.

Table 1 shows the existing land use for the site. The development site is considered primarily row crops with wooded areas along the old riverbed. A majority of the site currently flows to the east with only approximately 14 acres initially flowing to the west and south.

Table 2 summarizes the existing hydrologic data used in the model for existing conditions. As previously mentioned, the majority of the site currently drains to the east into Kings Lake, and two relatively small areas that currently drain to the west and south respectively. Impact Point 1 includes runoff going west to 252nd Street. Impact Point 2 includes runoff going south to King Lake Road from the site. Impact point 3 is the majority of the site and flows to Kings Lake. Both Impact point 1 and 2 flow through a combination of roadway ditches, farm swales, and channels before eventually reaching the Elkhorn River.

Appendix A includes exhibits of the existing drainage areas showing time of concentration paths and land use, along with existing site runoff calculations from HydroCAD.

Table 1 – Existing Land Use

Land Use	Existing Conditions (acres)	CN	Percent of Total Existing Area
Row Crops, Straight Row, Good (HSG A)	94.1	67	81.0%
Row Crops, Straight Row, Good (HSG B)	11.0	78	9.5%
Woods/Grass Combination	11.0	43	9.5%
Total	116.1	66	100.0%

Table 2 – Existing Condition Peak Flows

Basin (ID used in HydroCAD)	Area (acres)	CN	Tc (min)	2-Year Flow (cfs)	10-Year Flow (cfs)	100-Year Flow (cfs)
EWS 1 (Flow West to N 252 nd Street)	9.9	67	20.5	5.1	13.6	36.2
EWS 2 (Flow South to King Lake Road)	3.7	67	27.4	1.6	4.4	11.7
EWS 3 (Flow East to Kings Lake)	102.5	66	108.0	17.1	45.6	125.0

3 Proposed Development Conditions

The proposed development is a medium density residential project. There are 328 proposed single family lots and 13 acres of multi-family area. One retention pond is proposed and centrally located in the site. A proposed outlet pipe will discharge easterly from the proposed pond to Kings Lake.

Construction for the development consists of the construction of roads, utilities, mass grading, drainage improvements, and home building. The proposed grading of the site will significantly reduce the amount of runoff with the majority of the site flowing inward to the proposed pond.

The site has two smaller proposed areas in addition to its main drainage area. The first of which (PWS-1) is along the west side of the site at a size of about 2 acres. This includes the street connection to 252nd Street and small portion of the multi-family lots. The flow from this area will enter the drainage ditch along the N 252nd Street roadway. The proposed area has a similar flow rate for the 2 and 10-year events but the 100-year flow rate is significantly reduced.

The other relatively small area (PWS-2) is in the southwest corner of the site and drains through a proposed ditch along the back of proposed single-family lots towards the King Lake Road ditch. The proposed drainage area is about 3 acres. Flow to this impact point is slightly increased from existing condition. However, the projected flow increase is minimal in scale and not expected to cause any adverse impact.

The aforementioned drainage ditch will run along the length of existing acreage properties directly southwest of the development. This is designed to prevent runoff from going directly onto neighboring properties and acts as a conveyance system for stormwater that might normally flow from the existing homes towards the project area. A similar ditch is proposed along the northern boundary of the project.

The majority of the site (PWS-3B) will flow to the center of the site into the proposed retention pond. A proposed 36" culvert will discharge to the east to impact point 3 which is Kings Lake. The far east slopes of the site and undeveloped area (PWS-3A) is included as undetained flow to impact point 3 and is accounted for in allowable release from the retention pond.

Table 3 shows the proposed land uses. The single-family lots are modeled as 1/8 acre lots with HSG D. This is expected to give a conservative estimate of runoff as most lots are expected to be slightly larger than 1/8 acre. It should also be noted that the water surface area is based on water being at the outlet pipe elevation. During dry periods it's expected the water surface area may be considerably smaller.

Appendix B identifies the locations of the proposed watersheds, paths of the time of concentrations, and proposed site calculations from HydroCAD. Appendix C includes the calculations for the proposed retention pond and outlet pipe. Table 4 shows the drainage areas and flow rates for the proposed watersheds.

Table 3 – Proposed Land Use

Land Use	Proposed Conditions (acres)	CN	Percent of Total Proposed Area
1/8 Acre lots, 65% Impervious	68.7	92	59%
Woods/Grass Combination	13.3	43	12%
Multi-family Residential, 72% Impervious	13.3	93	11%
Water Surface	12.0	98	10%
>75% Grass Cover, Good	7.7	80	7%
Paved Roads (areas not included in MF or 1/8 acre)	1.1	98	1%
Total	116.1	87	100%

Table 4 – Proposed Condition Peak Flows

Basin	Area (acres)	CN	Tc (min)	2-Year Flow (cfs)	10-Year Flow (cfs)	100-Year Flow (cfs)
PWS-1 (Flow to N 252 nd ST)	2.1	83	10.0	4.7	8.4	16.3
PWS-2 (Flow to King Lake Road)	3.2	91	34.7	5.3	8.4	14.9
PWS-3A (Flow to Kings Lake)	20.6	60	36.6	3.3	12.5	41.6
PWS-3B (Flow to Pond)	90.3	92	34.9	155.7	243.5	425.6

Table 5 and 6 show the proposed pond routing results. The pond was modeled with both minimum and maximum tailwater conditions. The minimum tailwater conditions create the highest peak discharges. Whereas the maximum tailwater conditions create the highest pond water elevation. It was important to check both conditions, as the proposed trails and homes around the pond need to be designed based on the maximum pond elevation. The proposed drainage improvements need to be designed based on maximum discharge scenario.

Table 5 – Routing Results for Pond with Minimum Tailwater

Return Period	Inflow (cfs)	Outflow Total (cfs)	Primary, Pipe Outflow (cfs)	Overflow Outflow (cfs)	Water Surface Elevation (ft)
2-year	155.7	5.15	5.15	0.00	1125.0
10-year	243.5	11.5	11.5	0.00	1125.5
100-year	425.6	28.4	28.4	0.00	1126.6

Table 6 – Routing Results for Pond with Maximum Tailwater

Return Period	Inflow (cfs)	Outflow Total (cfs)	Primary, Pipe Outflow (cfs)	Overflow Outflow (cfs)	Water Surface Elevation (ft)
2-year	155.7	0.00	0.00	0.00	1125.3
10-year	243.5	0.00	0.00	0.00	1126.0
100-year	425.6	0.00	0.00	0.00	1127.5

4 Drainage Summary

4.1 Runoff Rate Control

HydroCAD Version 10.20-6a modeling software was utilized in the analysis, using the revised rainfall information from Atlas 14. Rainfall Storm Type is MSE 24-hr with storm curve 3. This is similar to the NRCS Type II distribution. This method was used to analyze the peak runoff rate control requirements for the existing and proposed conditions for the Lago Villagio project. Results of the model are included in Appendix A/B/C.

High density residential areas used a weighted CN of 93. This is based on the assumed impervious surface on each lot plus the street and sidewalk area was used to calculate a CN for this land use. Single family areas utilized the 1/8 acre average lot area, which includes ROW infrastructure, and is a weighted CN of 92.

The City of Valley requirement for the site is that post-development conditions are less than 10% above existing conditions for the 10-year storm. With the addition of the pond, the flow rates for the site as a whole are greatly reduced which will aid in limiting any future peak flow issue to the surrounding area.

4.2 HydroCAD Peak Flow Results Summary

Table 7 summarizes the modeling results which reflect the total peak flow (cfs) leaving the site for all impact points. The table summarizes the existing development conditions, and the proposed development conditions after stormwater detention.

The proposed design indicates higher peak flow values at Impact Point 2, while significantly reducing peak flow values at Impact Points 1 and 3. This redistribution of runoff is important because it decreases the overall drainage burden on the more critical downstream areas, helping to improve system performance and reduce the potential for localized flooding and erosion at the most sensitive impact points.

It should be noted that the majority of area flowing towards Impact Point 2 will be backyards. For consistent analysis with the rest of the site, this area was modeled as 1/8 acre lots with HSG D. However, theoretically this specific area should have a lower impervious percentage and higher infiltration value. Therefore, we don't anticipate any adverse impacts.

Table 7 – Peak Flow Comparison

Return Period	Existing Conditions (cfs)	Proposed Conditions (cfs)
Impact Point 1 - West to N 252nd Street		
2	5.1	4.7
10	13.6	8.4
100	36.2	16.3
Impact Point 2 - South To King Lake Road		
2	1.6	5.3
10	4.4	8.4
100	11.7	14.9
Impact Point 3 – East to Kings Lake (PWS-3A + Pond P1)		
2	20.4	8.5
10	51.2	24.0
100	133.7	70.0

5 Preliminary Floodplain Review

A preliminary review of the flood insurance maps for the area indicated that of the project would be constructed in a Zone AO (see attached Figure 1). Zone AO is defined by FEMA as a river or stream flood hazard areas, and areas with a 1% or greater chance of shallow flooding each year, usually in the form of sheet flow, with an average depth ranging from 1 to 3 feet. These areas have a 26% chance of flooding over the life of a 30-year mortgage. Average flood depths derived from detailed analyses are shown within these zones. The average depth has been determined to be 2 feet for this site.

The city floodplain administrator can request an official Base Flood Elevation (BFE) be determined for a section by the DNR. This was done for section 31-16-10E west of the project site (see Figure 2). A review of the information for this area indicated the DNR determined that the 2 feet of depth was acceptable for this zone AO. Due to the proximity of this section to the project site, it is anticipated the same determination would be made for the project location section (33-16-10E).

During the initial review for the project, it revealed several “red flags” which include:

- A portion of the property currently lies in the AE flood zone. Flooding events from the north are likely to drain through an existing drainage ditch on the northeast side of the property. Future development will have to take this into consideration.
- A DNR BFE had not been determined for the area and due to the size of the project it should be discussed if this would be something the floodplain administrator should request. This would ensure that the development is filled to an elevation that would allow it to be removed from the floodplain.
- A statement in a nearby section BFE completed by the DNR indicated that the BFE determination was unacceptable for the design of developments greater than 5 acres or 50 lots. The proposed development exceeds both of these and thus a BFE determination by the DNR may not be of any value.
- Development in an AO special flood hazard area could be changed in the future whether it is an adjustment to a depth of 3 feet in the Zone AO or be mapped a Zone AE as part of a more detailed hydraulic study of either the Elkhorn River or Platte River. Figure 3 shows the potential flood elevation if either one of the rivers cross-sections were extended into the site. Because of these concerns, it is critical that the BFE used in the final grading is a value that can be defended against any future litigation due to changes in the flood hazard elevation.

Due to similar concerns with a previous project adjacent to Lago Villagio and of similar size, Chuck Chase, Floodplain Outreach Coordinator with the Nebraska Department of Natural Resources, was contacted by Rocky Keehn on March 10, 2023. Chuck discussed potential issues with developments in a Zone AO. Below is a summary of his relevant comments:

- When the DNR determines a BFE for a Zone AO it is based on available data and does not include a detailed survey of the area. It is assumed that the proposed project will do a pre-fill survey of the site to establish the actual existing ground elevation. This works well for a single lot or small site. Since the DNR BFE is not based on a detailed survey, this is the main reason for the disclaimer of not using it for larger developments. For large development areas a field survey-based elevation determination of the site is required and thus the DNR will not complete a BFE.
- Large-scale developments have a much higher potential of significantly altering the flow patterns. For example, a row of homes created by fill could become a “levee” or “dam” and cause significant upstream damage or redirect the flow to adjacent properties as the flow path is altered. Chuck recommended a plan be developed that shows the existing drainage from the site and how these will be maintained.

- You must do the existing pre-fill survey, and have the survey approved by a qualified engineer or registered land surveyor. If you do not do a detailed and documented existing survey, then the top elevation of the fill becomes the new ground elevation. The BFE would be based off the fill elevation and not the existing ground elevation
- Suggested that an engineer with the proper credentials be on site prior to filling any area to confirm the existing ground elevation.
- Because of the size of the project a CLOMR-F should be submitted to assure that the project will be approved upon completion.

Based on the information collected and previous conversations with Chuck Chase (DNR) the following recommendations are made:

- The existing ground survey will require a detailed grid of surveyed elevations to eliminate any question of interpolation of elevations between survey points. It is expected that the site will have a LIDAR survey completed which provides nearly exact ground elevations at any location. Documentation of the accuracy of the LIDAR survey for existing conditions will be an integral part of the final CLOMR-F or LOMR-F submittal.
- Even if a LIDAR survey is completed, benchmarks should be pre-established that will be used by others to determine final grade elevations on each lot prior to construction of any structure. Although an overall LOMR-F will be completed, each lot may need to be surveyed to confirm the final elevation of the filled area and that the structure will meet the requirements of construction in a floodplain. The benchmarks will need to be tied into the existing LIDAR survey.
- Once the benchmarks are established, several ground elevations should be surveyed in the location of future lots and compared to the LIDAR based elevations. This will confirm the benchmarks can be used to compare existing and future ground elevations.
- As part of the final grading plan, complete a summary table for each lot that includes the highest existing ground prior to any filling, the BFE for that lot and minimum building elevation (which may include freeboard from the BFE). This information will be critical in submittal of a LOMR-F for the entire subdivision and provide guidelines for future development. It is not uncommon for a home to be built too low or fill removed. Having approved detailed information for each lot will help protect against any future litigation.
- All elevations should be based on the datum used on the FIRM for the site.
- A CLOMR-F should be completed for the project.

- The Lowest Adjacent Grade (LAG) will be what the LOMR-F final elevation will be based on. If the fill of the street and backyard areas are not above the BFE elevation, then these areas will remain in Zone AO. To maintain existing drainage patterns some areas such as streets and potential green space may not be removed from the floodplain.

Conditional Letter of Map Revision (CLOMR)

The site is currently in a Zone AO based on the current flood insurance rate map (FIRM). The proposed project will raise the current land elevations so all lots will be above the current flood elevation. Zone AO is defined by FEMA as a river or stream flood hazard areas, and areas with a 1% or greater chance of shallow flooding each year, usually in the form of sheet flow, with an average depth ranging from 1 to 3 feet. These areas have a 26% chance of flooding over the life of a 30-year mortgage. Average flood depths derived from detailed analyses are shown within these zones. The average depth has been determined to be 2 feet for this site.

A conditional letter of map revision (CLOMR) will be prepared and submitted to the Federal Emergency Management Agency to remove this area from the FIRM. Once the CLOMR is approved, the excavation of the lake and raising of the lots can proceed. The final minimum building elevation will be based on a base flood elevation of 2 feet above the current elevation of the site. If the grading is done prior to approval of the CLOMR, there is a risk the review agency would assume the existing grade is the new grade created by the fill. If the assumed pre-existing grade is the new filled grade, either more material would have to be added or the lot remains in the floodplain.

The proposed process to assure the lots are out of the current floodplain is to have a detailed existing survey of the area. This will be done with a drone survey of the entire site. Using this survey, an existing elevation of each of the four corners of the lots will be determined. The lowest elevation will be documented as the existing ground elevation. Fill will be added until the final lot elevation is at least 3 feet above the lowest existing ground elevation. The three feet is the 2 feet of elevation to establish the base flood elevation plus 1 foot of free board. In non-building areas, such as the streets, any area that is 2 feet above the existing ground elevation would be considered out of the current identified flood area.

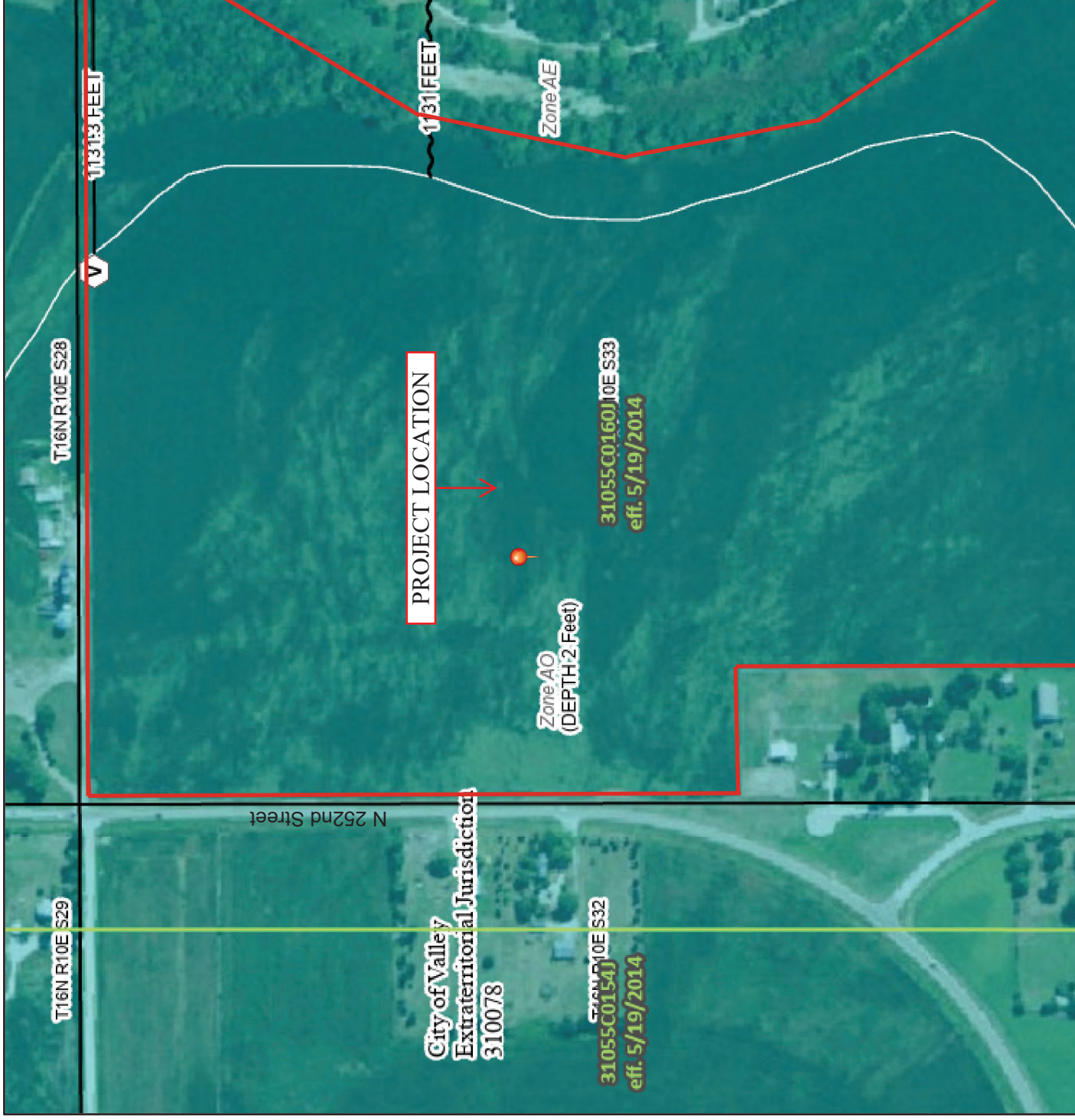
A summary of all the lots will be completed that includes the lowest ground elevation, base flood elevation and minimum building elevation. A revised flood insurance rate map of the area with all areas that are 2 feet above the existing ground elevation will be submitted as part of the CLOMR. This map once approved, will be the new flood insurance rate map for the area and should not include any of the new developed lots for this project.

National Flood Hazard Layer FIRMette - Figure 1

96°18'51"W 41°19'17"N



SEE FIS REPORT FOR DETAILED LEGEND AND INDEX MAP FOR FIRM PANEL LAYOUT



96°18'14"W 41°18'50"N

1:6,000

Basemap Imagery Source: USGS National Map 2023

Legend

SPECIAL FLOOD HAZARD AREAS

- Without Base Flood Elevation (BFE)
Zone A, V, A99
- With BFE or Depth
Zone AE, AO, AH, VE, AR
- Regulatory Floodway

OTHER AREAS OF FLOOD HAZARD

- 0.2% Annual Chance Flood Hazard, Areas of 1% annual chance flood with average depth less than one foot or with drainage areas of less than one square mile
Zone X
- Future Conditions 1% Annual Chance Flood Hazard
Zone X
- Area with Reduced Flood Risk due to Levee. See Notes.
Zone X
- Area with Flood Risk due to Levee
Zone D

OTHER AREAS

- Area of Minimal Flood Hazard
Zone X
- Effective LOMRMs
- Area of Undetermined Flood Hazard
Zone D

GENERAL STRUCTURES

- Channel, Culvert, or Storm Sewer
- Levee, Dike, or Floodwall

Cross Sections with 1% Annual Chance Water Surface Elevation

- 20.2
- 17.5
- 8

OTHER FEATURES

- Coastal Transect
- Base Flood Elevation Line (BFE)
- Limit of Study
- Coastal Transect Baseline
- Profile Baseline
- Hydrographic Feature

MAP PANELS

- Digital Data Available
- No Digital Data Available
- Unmapped

The pin displayed on the map is an approximate point selected by the user and does not represent an authoritative property location.

This map complies with FEMA's standards for the use of digital flood maps if it is not void as described below. The basemap shown complies with FEMA's basemap accuracy standards. The flood hazard information is derived directly from the authoritative NFHL web services provided by FEMA. This map was exported on 9/19/2025 at 1:18 PM and does not reflect changes or amendments subsequent to this date and time. The NFHL and effective information may change or become superseded by new data over time.

This map image is void if the one or more of the following map elements do not appear: basemap imagery, flood zone labels, legend, scale bar, map creation date, community identifiers, FIRM panel number, and FIRM effective date. Map images for unmapped and unmodernized areas cannot be used for regulatory purposes.

Figure 2. Nearby BFE and LOMFs

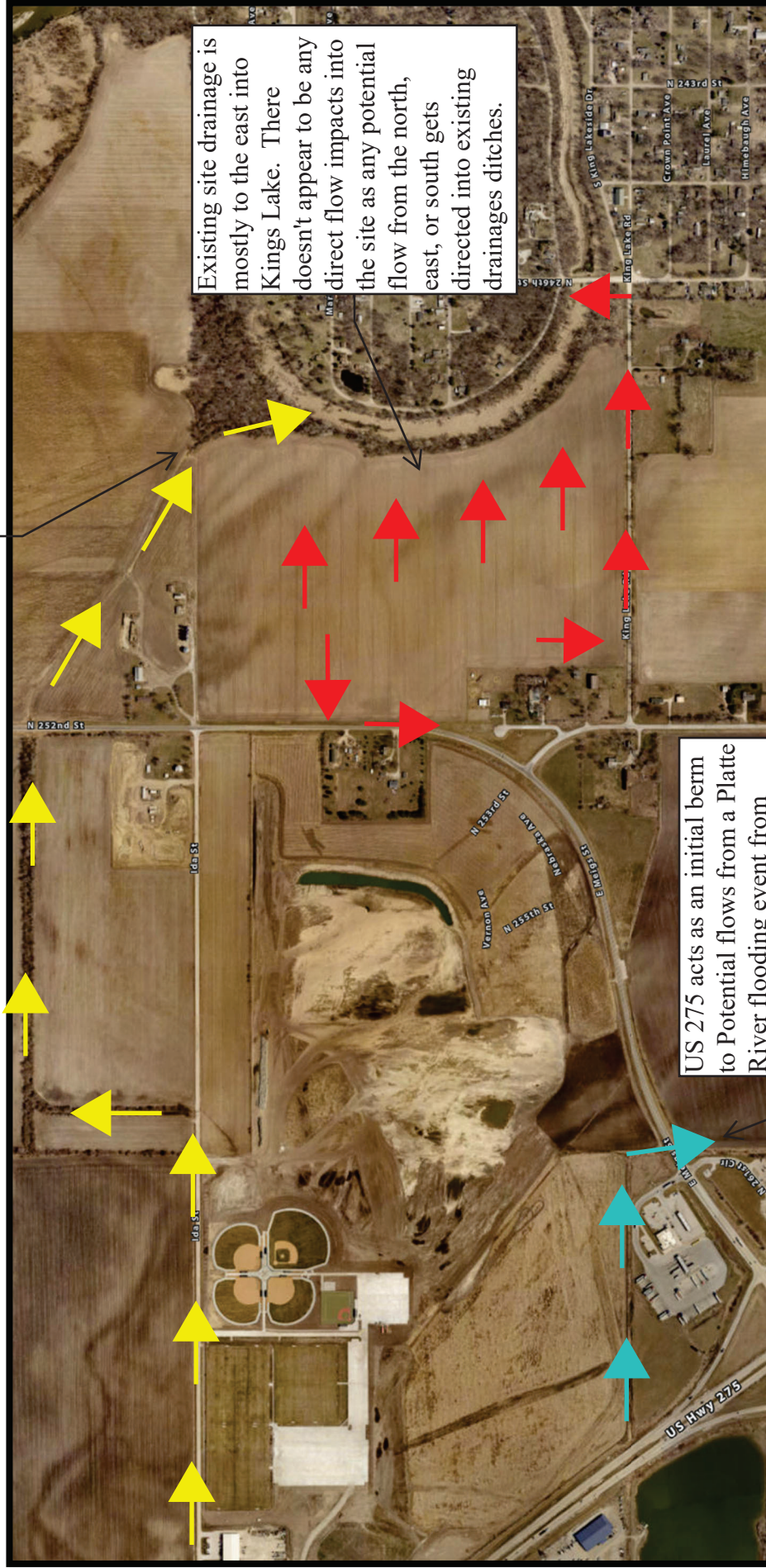


Figure 3 - Potential Impact of the Flood Elevations of Projected Platte and Elkhorn River Cross-Sections



Figure 4 - Existing Drainage Patterns

The area to the north doesn't seem to actively flow through the site. If there is a failure point of Platte River impoundments, the flood waters enter the project area but flow continue on through existing drainage ditches.

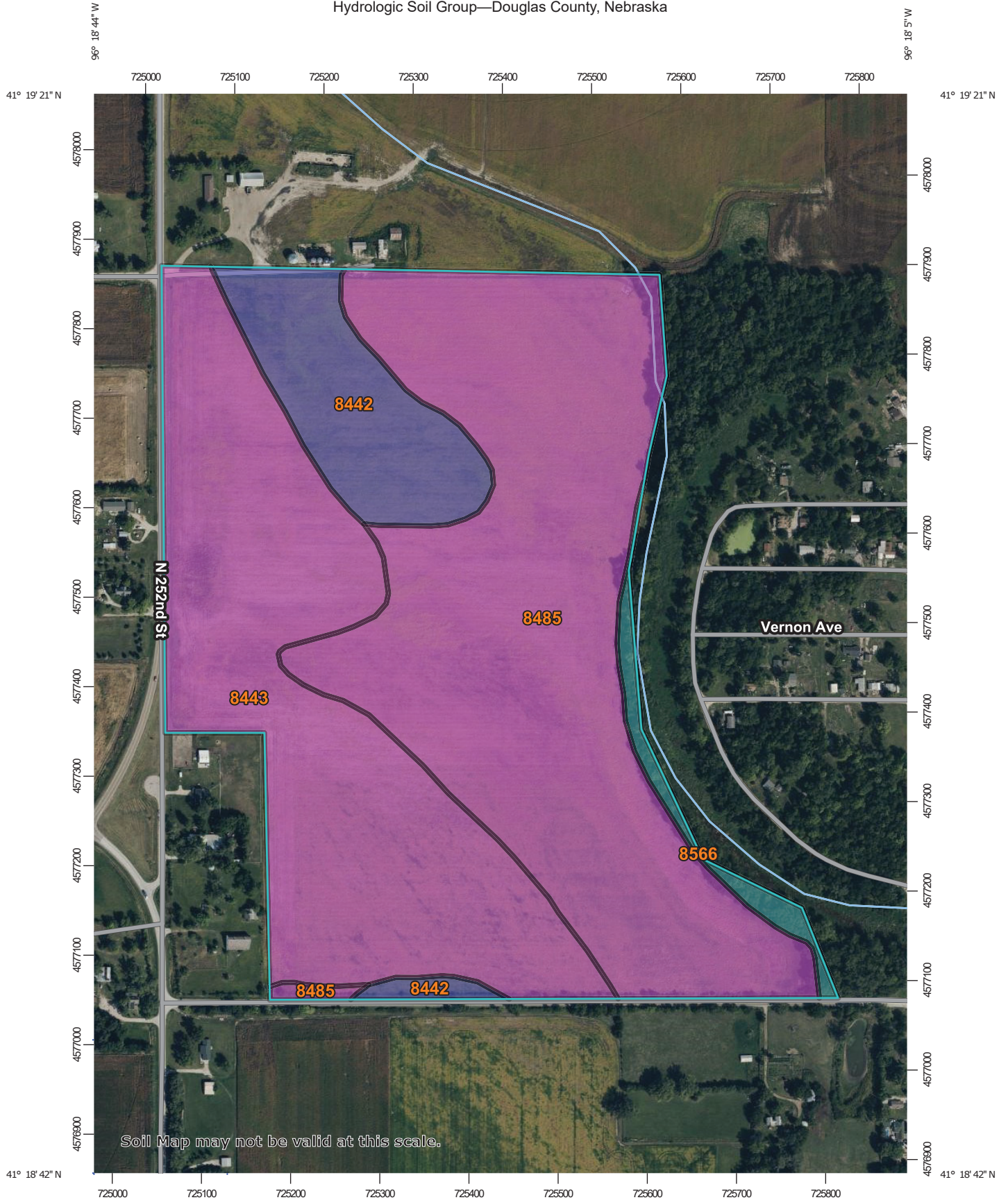


Existing site drainage is mostly to the east into Kings Lake. There doesn't appear to be any direct flow impacts into the site as any potential flow from the north, east, or south gets directed into existing drainages ditches.

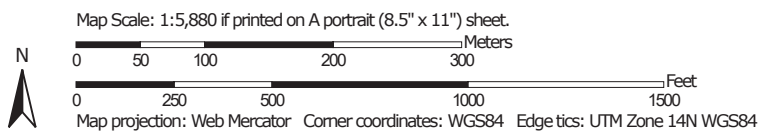
US 275 acts as an initial berm to Potential flows from a Platte River flooding event from west or south. Should there be a failure, flow would go southeast through drainage ditches to N 252nd Street before draining east.

APPENDIX A

































Hydrologic Soil Group—Douglas County, Nebraska



Soil Map may not be valid at this scale.



MAP LEGEND

Area of Interest (AOI)		 C
 Area of Interest (AOI)		 C/D
Soils		 D
Soil Rating Polygons		 Not rated or not available
 A		Water Features
 A/D		 Streams and Canals
 B		Transportation
 B/D		 Rails
 C		 Interstate Highways
 C/D		 US Routes
 D		 Major Roads
 Not rated or not available		 Local Roads
Soil Rating Lines		Background
 A		 Aerial Photography
 A/D		
 B		
 B/D		
 C		
 C/D		
 D		
 Not rated or not available		
Soil Rating Points		
 A		
 A/D		
 B		
 B/D		

MAP INFORMATION

The soil surveys that comprise your AOI were mapped at 1:12,000.

Warning: Soil Map may not be valid at this scale.

Enlargement of maps beyond the scale of mapping can cause misunderstanding of the detail of mapping and accuracy of soil line placement. The maps do not show the small areas of contrasting soils that could have been shown at a more detailed scale.

Please rely on the bar scale on each map sheet for map measurements.

Source of Map: Natural Resources Conservation Service
 Web Soil Survey URL:
 Coordinate System: Web Mercator (EPSG:3857)

Maps from the Web Soil Survey are based on the Web Mercator projection, which preserves direction and shape but distorts distance and area. A projection that preserves area, such as the Albers equal-area conic projection, should be used if more accurate calculations of distance or area are required.

This product is generated from the USDA-NRCS certified data as of the version date(s) listed below.

Soil Survey Area: Douglas County, Nebraska
 Survey Area Data: Version 19, Aug 28, 2024

Soil map units are labeled (as space allows) for map scales 1:50,000 or larger.

Date(s) aerial images were photographed: Data not available.

The orthophoto or other base map on which the soil lines were compiled and digitized probably differs from the background imagery displayed on these maps. As a result, some minor shifting of map unit boundaries may be evident.

Hydrologic Soil Group

Map unit symbol	Map unit name	Rating	Acres in AOI	Percent of AOI
8442	Cass-Novina complex, occasionally flooded	B	11.9	11.0%
8443	Cass-Wann fine sandy loams, occasionally flooded	A	40.6	37.4%
8485	Gilliam-Eudora silt loams, occasionally flooded	A	53.6	49.5%
8566	Platte, Inglewood, and Barney soils, frequently flooded	C	2.3	2.1%
Totals for Area of Interest			108.3	100.0%

Description

Hydrologic soil groups are based on estimates of runoff potential. Soils are assigned to one of four groups according to the rate of water infiltration when the soils are not protected by vegetation, are thoroughly wet, and receive precipitation from long-duration storms.

The soils in the United States are assigned to four groups (A, B, C, and D) and three dual classes (A/D, B/D, and C/D). The groups are defined as follows:

Group A. Soils having a high infiltration rate (low runoff potential) when thoroughly wet. These consist mainly of deep, well drained to excessively drained sands or gravelly sands. These soils have a high rate of water transmission.

Group B. Soils having a moderate infiltration rate when thoroughly wet. These consist chiefly of moderately deep or deep, moderately well drained or well drained soils that have moderately fine texture to moderately coarse texture. These soils have a moderate rate of water transmission.

Group C. Soils having a slow infiltration rate when thoroughly wet. These consist chiefly of soils having a layer that impedes the downward movement of water or soils of moderately fine texture or fine texture. These soils have a slow rate of water transmission.

Group D. Soils having a very slow infiltration rate (high runoff potential) when thoroughly wet. These consist chiefly of clays that have a high shrink-swell potential, soils that have a high water table, soils that have a claypan or clay layer at or near the surface, and soils that are shallow over nearly impervious material. These soils have a very slow rate of water transmission.

If a soil is assigned to a dual hydrologic group (A/D, B/D, or C/D), the first letter is for drained areas and the second is for undrained areas. Only the soils that in their natural condition are in group D are assigned to dual classes.

Rating Options

Aggregation Method: Dominant Condition

Component Percent Cutoff: None Specified

Tie-break Rule: Higher

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MSE 24-hr 3 2-Year Rainfall=2.97"

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Page 5

Time span=2.00-80.00 hrs, dt=0.01 hrs, 7801 points
Runoff by SCS TR-20 method, UH=SCS, Weighted-CN
Reach routing by Stor-Ind+Trans method - Pond routing by Stor-Ind method

Subcatchment7S: EWS 1

Runoff Area=9.870 ac 0.00% Impervious Runoff Depth=0.57"
Flow Length=550' Tc=20.5 min CN=67 Runoff=5.09 cfs 0.469 af

Subcatchment9S: EWS 2

Runoff Area=3.724 ac 0.00% Impervious Runoff Depth=0.57"
Flow Length=820' Slope=0.0050 '/' Tc=27.4 min CN=67 Runoff=1.64 cfs 0.177 af

Subcatchment10S: EWS 3

Runoff Area=102.508 ac 0.00% Impervious Runoff Depth=0.53"
Flow Length=2,950' Tc=108.0 min CN=66 Runoff=17.11 cfs 4.532 af

Total Runoff Area = 116.102 ac Runoff Volume = 5.178 af Average Runoff Depth = 0.54"
100.00% Pervious = 116.102 ac 0.00% Impervious = 0.000 ac

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Page 6

Time span=2.00-80.00 hrs, dt=0.01 hrs, 7801 points
Runoff by SCS TR-20 method, UH=SCS, Weighted-CN
Reach routing by Stor-Ind+Trans method - Pond routing by Stor-Ind method

Subcatchment7S: EWS 1

Runoff Area=9.870 ac 0.00% Impervious Runoff Depth=1.32"
Flow Length=550' Tc=20.5 min CN=67 Runoff=13.62 cfs 1.086 af

Subcatchment9S: EWS 2

Runoff Area=3.724 ac 0.00% Impervious Runoff Depth=1.32"
Flow Length=820' Slope=0.0050 '/' Tc=27.4 min CN=67 Runoff=4.36 cfs 0.410 af

Subcatchment10S: EWS 3

Runoff Area=102.508 ac 0.00% Impervious Runoff Depth=1.26"
Flow Length=2,950' Tc=108.0 min CN=66 Runoff=45.60 cfs 10.738 af

Total Runoff Area = 116.102 ac Runoff Volume = 12.234 af Average Runoff Depth = 1.26"
100.00% Pervious = 116.102 ac 0.00% Impervious = 0.000 ac

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MSE 24-hr 3 100-Year Rainfall=7.03"

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Time span=2.00-80.00 hrs, dt=0.01 hrs, 7801 points
Runoff by SCS TR-20 method, UH=SCS, Weighted-CN
Reach routing by Stor-Ind+Trans method - Pond routing by Stor-Ind method

Subcatchment7S: EWS 1

Runoff Area=9.870 ac 0.00% Impervious Runoff Depth=3.33"
Flow Length=550' Tc=20.5 min CN=67 Runoff=36.17 cfs 2.740 af

Subcatchment9S: EWS 2

Runoff Area=3.724 ac 0.00% Impervious Runoff Depth=3.33"
Flow Length=820' Slope=0.0050 '/ Tc=27.4 min CN=67 Runoff=11.67 cfs 1.034 af

Subcatchment10S: EWS 3

Runoff Area=102.508 ac 0.00% Impervious Runoff Depth=3.23"
Flow Length=2,950' Tc=108.0 min CN=66 Runoff=125.05 cfs 27.575 af

Total Runoff Area = 116.102 ac Runoff Volume = 31.348 af Average Runoff Depth = 3.24"
100.00% Pervious = 116.102 ac 0.00% Impervious = 0.000 ac

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Area Listing (selected nodes)

Area (acres)	CN	Description (subcatchment-numbers)
94.102	67	Row crops, straight row, Good, HSG A (7S, 9S, 10S)
11.000	78	Row crops, straight row, Good, HSG B (10S)
11.000	43	Woods/grass comb., Fair, HSG A (10S)
116.102	66	TOTAL AREA

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Summary for Subcatchment 7S: EWS 1

Runoff = 5.09 cfs @ 12.35 hrs, Volume= 0.469 af, Depth= 0.57"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 2.00-80.00 hrs, dt= 0.01 hrs
MSE 24-hr 3 2-Year Rainfall=2.97"

Area (ac)	CN	Description
9.870	67	Row crops, straight row, Good, HSG A
9.870		100.00% Pervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
12.2	100	0.0020	0.14		Sheet Flow, Cultivated: Residue<=20% n= 0.060 P2= 3.00"
8.3	450	0.0100	0.90		Shallow Concentrated Flow, Cultivated Straight Rows Kv= 9.0 fps
20.5	550	Total			

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Page 8

Summary for Subcatchment 9S: EWS 2

Runoff = 1.64 cfs @ 12.45 hrs, Volume= 0.177 af, Depth= 0.57"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 2.00-80.00 hrs, dt= 0.01 hrs
MSE 24-hr 3 2-Year Rainfall=2.97"

Area (ac)	CN	Description
3.724	67	Row crops, straight row, Good, HSG A
3.724		100.00% Pervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
8.5	100	0.0050	0.20		Sheet Flow, Cultivated: Residue<=20% n= 0.060 P2= 3.00"
18.9	720	0.0050	0.64		Shallow Concentrated Flow, Cultivated Straight Rows Kv= 9.0 fps
27.4	820	Total			

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Summary for Subcatchment 10S: EWS 3

Runoff = 17.11 cfs @ 13.68 hrs, Volume= 4.532 af, Depth= 0.53"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 2.00-80.00 hrs, dt= 0.01 hrs
MSE 24-hr 3 2-Year Rainfall=2.97"

Area (ac)	CN	Description
80.508	67	Row crops, straight row, Good, HSG A
11.000	78	Row crops, straight row, Good, HSG B
11.000	43	Woods/grass comb., Fair, HSG A
102.508	66	Weighted Average
102.508		100.00% Pervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
12.2	100	0.0020	0.14		Sheet Flow, Cultivated: Residue<=20% n= 0.060 P2= 3.00"
62.1	1,500	0.0020	0.40		Shallow Concentrated Flow, Cultivated Straight Rows Kv= 9.0 fps
14.8	400	0.0025	0.45		Shallow Concentrated Flow, Cultivated Straight Rows Kv= 9.0 fps
15.7	600	0.0050	0.64		Shallow Concentrated Flow, Cultivated Straight Rows Kv= 9.0 fps
3.2	350	0.0400	1.80		Shallow Concentrated Flow, Cultivated Straight Rows Kv= 9.0 fps
108.0	2,950	Total			

APPENDIX B

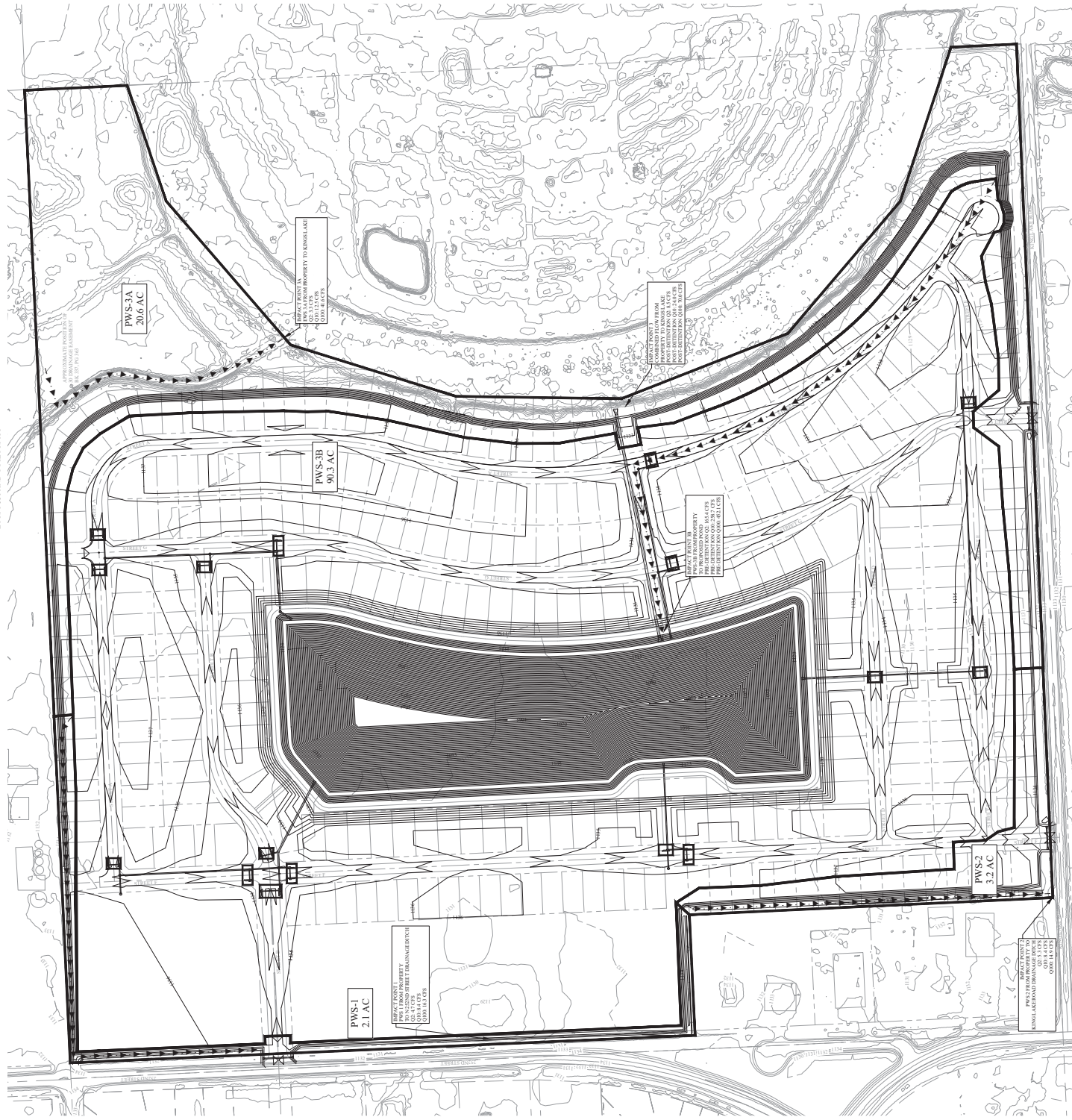
LAGO VILLAGGIO
PRELIMINARY PLAN
LOTS 1-30 AND OUTLOTS A-F

REVISIONS

NUMBER	DATE	DESCRIPTION
1	11/14/25	ISSUANCE PERMITS
2	01/12/26	REVISIONS

Lago Villaggio
Valley, NE
FSE: CA-4197
FSE #: 104.001
May 12, 2026

Preliminary
NOT FOR CONSTRUCTION



LEGEND

- EXISTING MAJOR CONTOUR
- EXISTING MINOR CONTOUR
- EXISTING PROPERTY LINE
- EXISTING SANITARY SEWER
- EXISTING GWA LINE
- EXISTING UNDERGROUND ELECTRICAL
- EXISTING UNDERGROUND TELEPHONE
- EXISTING UNDERGROUND GAS
- EXISTING WATER MAIN
- EXISTING STORM SEWER
- EXISTING CONCRETE CULVERT LINES
- EXISTING FENCE
- EXISTING MAJOR CONTOUR
- EXISTING MINOR CONTOUR
- PROPOSED PAVEMENT
- PROPOSED DRIVE
- PROPOSED DRIVE RAMP
- PROPOSED DRIVE RAMP BANK
- PROPOSED ROAD CENTERLINE
- PROPOSED PROPERTY SURFACE
- PROPOSED CURB INLET
- PROPOSED STORM INLET
- PROPOSED WATERWASH



PWS-3A
20.6 AC

PWS-3B
90.3 AC

PWS-1
2.1 AC

PWS-2
3.2 AC

BRIDGE POINT FACILITY
FOR STABILIZATION OF BRIDGE PILES
OF 14 LOTS
0.000 AC LOTS

PROPOSED DRIVEWAY
TO BE INSTALLED IN LOTS
1-10, 12-15, 17-20, 22-25, 27-30
AND OUTLOTS A-F
0.000 AC LOTS

PWS-2 FROM PROPERTY TO
KANSAS ROAD DRAINAGE
OF 14 LOTS
0.000 AC LOTS

PROPOSED DRIVEWAY
TO BE INSTALLED IN LOTS
1-10, 12-15, 17-20, 22-25, 27-30
AND OUTLOTS A-F
0.000 AC LOTS

PROPOSED DRIVEWAY
TO BE INSTALLED IN LOTS
1-10, 12-15, 17-20, 22-25, 27-30
AND OUTLOTS A-F
0.000 AC LOTS

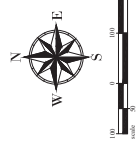
LAGO VILLAGGIO
PRELIMINARY PLAN
LOTS 1-330 AND OUTLOTS A-F

NUMBER	DATE	DESCRIPTION
1	10/1/2025	ISSUANCE OF RESUBMITTAL
2	01/2/2026	RESUBMITTAL

Lago Villaggio
Valley, NE
FSE: CA-4197
FSE #: 104.001
May 12, 2026

Preliminary
NOT FOR CONSTRUCTION

- LEGEND**
- EXISTING PROPERTY LINE
 - PROPOSED PROPERTY LINE
 - PHASE BOUNDARY LINE
 - PROPOSED SIDEWALK
 - PROPOSED SANITARY MAIN
 - PROPOSED SANITARY SUB MAIN
 - PROPOSED FIRE HYDRANT
 - PROPOSED FIRE HYDRANT
 - PROPOSED SANITARY MANHOLE
 - PROPOSED CURB/RAILLET



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MSE 24-hr 3 10-Year Rainfall=4.28"

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Page 2

Time span=2.00-80.00 hrs, dt=0.01 hrs, 7801 points
Runoff by SCS TR-20 method, UH=SCS, Weighted-CN
Reach routing by Stor-Ind+Trans method - Pond routing by Stor-Ind method

Subcatchment 1S: PWS-1 Runoff Area=2.132 ac 18.03% Impervious Runoff Depth=2.53"
Tc=10.0 min CN=83 Runoff=8.35 cfs 0.450 af

Subcatchment 2S: PWS-2 Runoff Area=3.184 ac 57.50% Impervious Runoff Depth=3.29"
Flow Length=980' Tc=34.7 min CN=91 Runoff=8.42 cfs 0.872 af

Subcatchment 3S: PWS-3A Runoff Area=20.584 ac 19.99% Impervious Runoff Depth=0.90"
Flow Length=765' Tc=36.6 min CN=60 Runoff=12.49 cfs 1.549 af

Subcatchment 4S: PWS-3B Runoff Area=90.253 ac 67.65% Impervious Runoff Depth=3.39"
Flow Length=1,950' Tc=34.9 min CN=92 Runoff=243.54 cfs 25.485 af

Total Runoff Area = 116.153 ac Runoff Volume = 28.356 af Average Runoff Depth = 2.93"
41.99% Pervious = 48.768 ac 58.01% Impervious = 67.385 ac

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MSE 24-hr 3 100-Year Rainfall=7.03"

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Page 3

Time span=2.00-80.00 hrs, dt=0.01 hrs, 7801 points
Runoff by SCS TR-20 method, UH=SCS, Weighted-CN
Reach routing by Stor-Ind+Trans method - Pond routing by Stor-Ind method

Subcatchment 1S: PWS-1 Runoff Area=2.132 ac 18.03% Impervious Runoff Depth=5.06"
Tc=10.0 min CN=83 Runoff=16.26 cfs 0.898 af

Subcatchment 2S: PWS-2 Runoff Area=3.184 ac 57.50% Impervious Runoff Depth=5.97"
Flow Length=980' Tc=34.7 min CN=91 Runoff=14.89 cfs 1.584 af

Subcatchment 3S: PWS-3A Runoff Area=20.584 ac 19.99% Impervious Runoff Depth=2.62"
Flow Length=765' Tc=36.6 min CN=60 Runoff=41.59 cfs 4.503 af

Subcatchment 4S: PWS-3B Runoff Area=90.253 ac 67.65% Impervious Runoff Depth=6.08"
Flow Length=1,950' Tc=34.9 min CN=92 Runoff=425.60 cfs 45.761 af

Total Runoff Area = 116.153 ac Runoff Volume = 52.746 af Average Runoff Depth = 5.45"
41.99% Pervious = 48.768 ac 58.01% Impervious = 67.385 ac

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Page 1

Area Listing (selected nodes)

Area (acres)	CN	Description (subcatchment-numbers)
68.684	92	1/8 acre lots, 65% imp, HSG D (1S, 2S, 3S, 4S)
7.683	80	>75% Grass cover, Good, HSG D (1S, 2S, 3S, 4S)
13.281	93	Multi--Family Residential, 72% imp, HSG D (4S)
1.103	98	Paved roads w/curbs & sewers, HSG D (1S, 2S, 4S)
12.075	98	Water Surface, HSG D (4S)
13.327	43	Woods/grass comb., Fair, HSG A (3S)
116.153	86	TOTAL AREA

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MSE 24-hr 3 2-Year Rainfall=2.97"

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Page 2

Summary for Subcatchment 1S: PWS-1

Runoff = 4.72 cfs @ 12.18 hrs, Volume= 0.253 af, Depth= 1.42"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 2.00-80.00 hrs, dt= 0.01 hrs
MSE 24-hr 3 2-Year Rainfall=2.97"

Area (ac)	CN	Description
0.073	98	Paved roads w/curbs & sewers, HSG D
1.580	80	>75% Grass cover, Good, HSG D
0.479	92	1/8 acre lots, 65% imp, HSG D
2.132	83	Weighted Average
1.748		81.97% Pervious Area
0.384		18.03% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
10.0					Direct Entry,

Summary for Subcatchment 2S: PWS-2

Runoff = 5.32 cfs @ 12.46 hrs, Volume= 0.542 af, Depth= 2.04"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 2.00-80.00 hrs, dt= 0.01 hrs
 MSE 24-hr 3 2-Year Rainfall=2.97"

Area (ac)	CN	Description
2.789	92	1/8 acre lots, 65% imp, HSG D
0.377	80	>75% Grass cover, Good, HSG D
0.018	98	Paved roads w/curbs & sewers, HSG D
3.184	91	Weighted Average
1.353		42.50% Pervious Area
1.831		57.50% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
1.3	30	0.3300	0.40		Sheet Flow, Grass: Short n= 0.150 P2= 3.00"
33.4	950	0.0010	0.47		Shallow Concentrated Flow, Grassed Waterway Kv= 15.0 fps
34.7	980	Total			

Summary for Subcatchment 4S: PWS-3B

Runoff = 155.72 cfs @ 12.46 hrs, Volume= 16.041 af, Depth= 2.13"
 Routed to Pond 5P : P1

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 2.00-80.00 hrs, dt= 0.01 hrs
 MSE 24-hr 3 2-Year Rainfall=2.97"

Area (ac)	CN	Description
* 13.281	93	Multi--Family Residential, 72% imp, HSG D
* 59.085	92	1/8 acre lots, 65% imp, HSG D
12.075	98	Water Surface, HSG D
4.800	80	>75% Grass cover, Good, HSG D
1.012	98	Paved roads w/curbs & sewers, HSG D
90.253	92	Weighted Average
29.198		32.35% Pervious Area
61.055		67.65% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
13.4	100	0.0100	0.12		Sheet Flow, Grass: Short n= 0.150 P2= 3.00"
21.5	1,850	0.0050	1.44		Shallow Concentrated Flow, Paved Kv= 20.3 fps
34.9	1,950	Total			

Lago Villaggio

Prepared by FoleyShald Engineering LLC

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MSE 24-hr 3 2-Year Rainfall=2.97"

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Page 4

Summary for Subcatchment 3S: PWS-3A

Runoff = 3.32 cfs @ 12.68 hrs, Volume= 0.553 af, Depth= 0.32"

Runoff by SCS TR-20 method, UH=SCS, Weighted-CN, Time Span= 2.00-80.00 hrs, dt= 0.01 hrs
MSE 24-hr 3 2-Year Rainfall=2.97"

Area (ac)	CN	Description
6.331	92	1/8 acre lots, 65% imp, HSG D
13.327	43	Woods/grass comb., Fair, HSG A
0.926	80	>75% Grass cover, Good, HSG D
20.584	60	Weighted Average
16.469		80.01% Pervious Area
4.115		19.99% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
17.1	85	0.0100	0.08		Sheet Flow, Grass: Dense n= 0.240 P2= 3.00"
19.5	680	0.0015	0.58		Shallow Concentrated Flow, Grassed Waterway Kv= 15.0 fps
36.6	765	Total			

APPENDIX C

Summary for Pond 5P: P1

Inflow Area = 90.253 ac, 67.65% Impervious, Inflow Depth = 2.13" for 2-Year event
 Inflow = 155.72 cfs @ 12.46 hrs, Volume= 16.041 af
 Outflow = 5.15 cfs @ 15.95 hrs, Volume= 11.750 af, Atten= 97%, Lag= 209.5 min
 Primary = 5.15 cfs @ 15.95 hrs, Volume= 11.750 af

Routing by Stor-Ind method, Time Span= 2.00-80.00 hrs, dt= 0.01 hrs
 Peak Elev= 1,125.01' @ 15.95 hrs Surf.Area= 559,172 sf Storage= 556,646 cf

Plug-Flow detention time= 1,269.5 min calculated for 11.748 af (73% of inflow)
 Center-of-Mass det. time= 1,202.4 min (2,013.6 - 811.3)

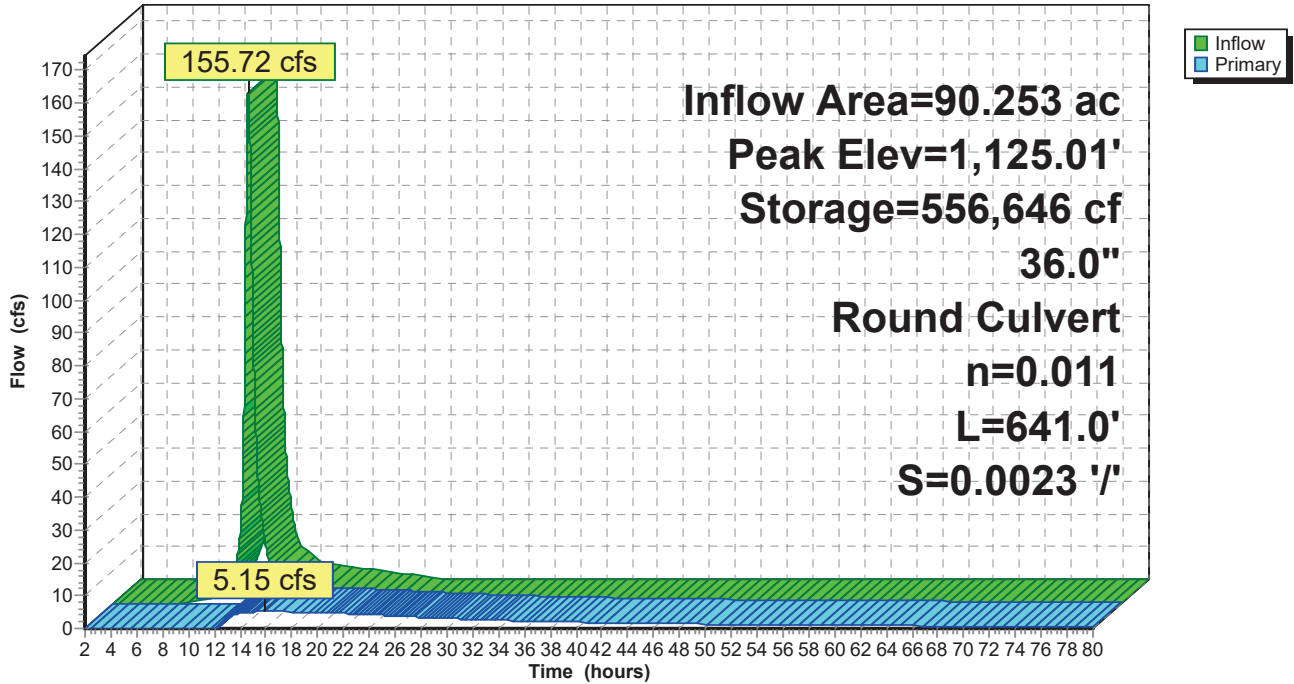
Volume	Invert	Avail.Storage	Storage Description		
#1	1,124.00'	6,886,455 cf	Custom Stage Data (Irregular) Listed below (Recalc)		
Elevation (feet)	Surf.Area (sq-ft)	Perim. (feet)	Inc.Store (cubic-feet)	Cum.Store (cubic-feet)	Wet.Area (sq-ft)
1,124.00	548,007	3,692.7	0	0	548,007
1,125.00	559,111	3,711.5	553,550	553,550	559,686
1,126.00	570,271	3,730.3	564,682	1,118,232	571,425
1,127.00	581,486	3,749.1	575,869	1,694,101	583,223
1,128.00	592,758	3,767.8	587,113	2,281,214	595,023
1,129.00	703,860	3,989.3	647,514	2,928,728	731,808
1,130.00	728,097	4,045.5	715,944	3,644,672	767,966
1,131.00	753,906	4,140.4	740,964	4,385,636	829,920
1,132.00	780,167	4,219.7	766,999	5,152,636	882,842
1,133.00	828,001	5,645.9	803,965	5,956,601	2,002,533
1,134.00	1,035,573	12,704.9	929,854	6,886,455	12,310,867

Device	Routing	Invert	Outlet Devices
#1	Primary	1,124.00'	36.0" Round Culvert L= 641.0' RCP, end-section conforming to fill, Ke= 0.500 Inlet / Outlet Invert= 1,124.00' / 1,122.50' S= 0.0023 '/' Cc= 0.900 n= 0.011 Concrete pipe, straight & clean, Flow Area= 7.07 sf

Primary OutFlow Max=5.15 cfs @ 15.95 hrs HW=1,125.01' (Free Discharge)
 ↑**1=Culvert** (Barrel Controls 5.15 cfs @ 3.70 fps)

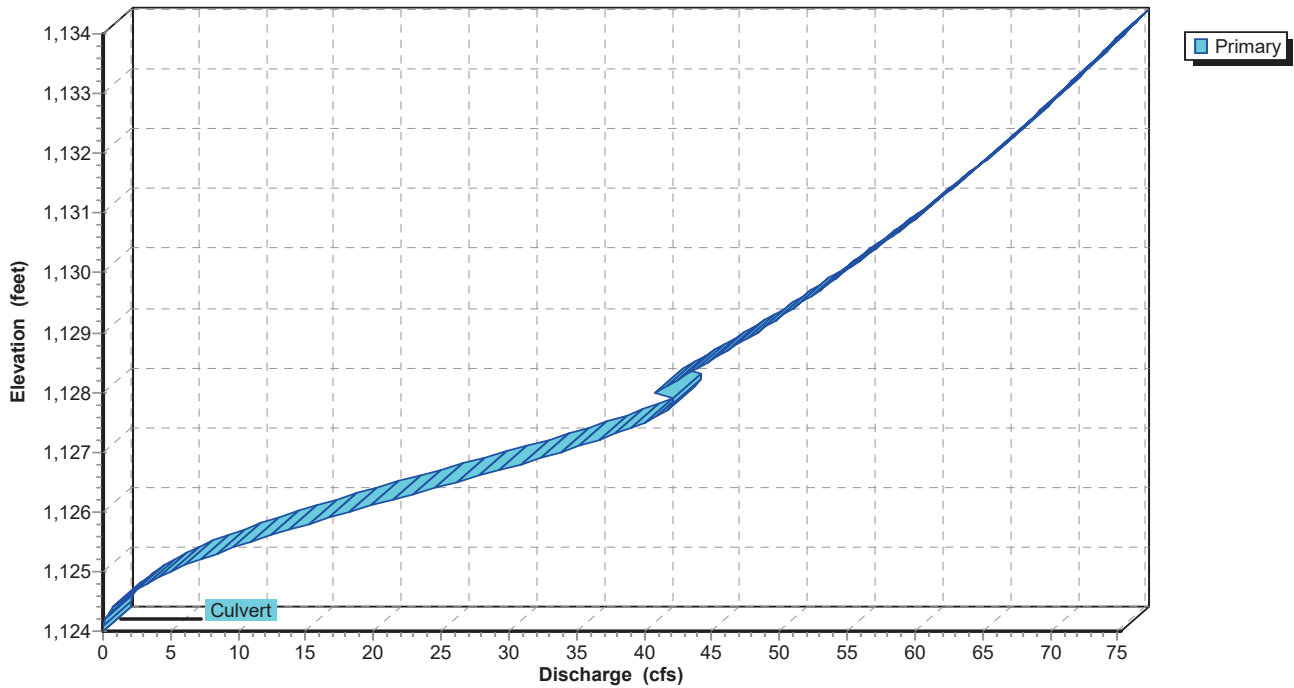
Pond 5P: P1

Hydrograph

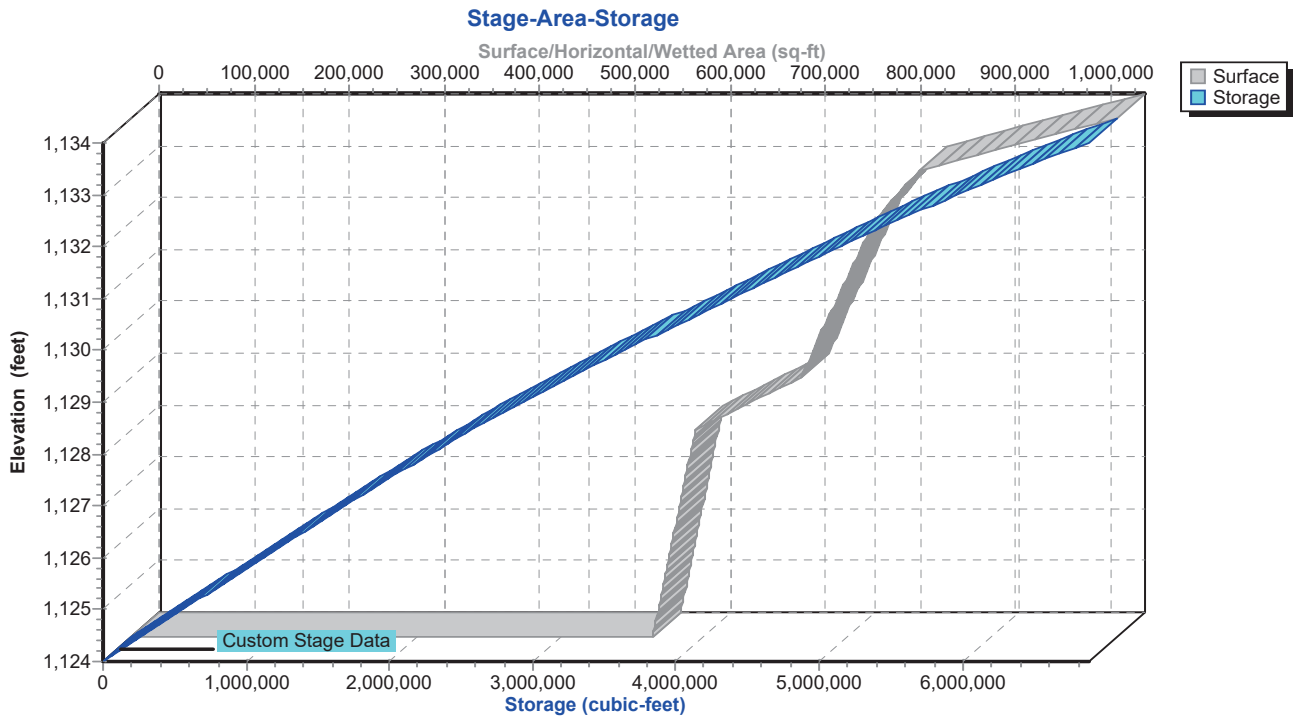


Pond 5P: P1

Stage-Discharge



Pond 5P: P1



Stage-Discharge for Pond 5P: P1

Elevation (feet)	Primary (cfs)	Elevation (feet)	Primary (cfs)	Elevation (feet)	Primary (cfs)	Elevation (feet)	Primary (cfs)
1,124.00	0.00	1,126.65	28.53	1,129.30	50.31	1,131.95	65.54
1,124.05	0.01	1,126.70	29.31	1,129.35	50.64	1,132.00	65.79
1,124.10	0.04	1,126.75	30.08	1,129.40	50.96	1,132.05	66.05
1,124.15	0.10	1,126.80	30.85	1,129.45	51.29	1,132.10	66.30
1,124.20	0.18	1,126.85	31.61	1,129.50	51.61	1,132.15	66.55
1,124.25	0.29	1,126.90	32.37	1,129.55	51.93	1,132.20	66.80
1,124.30	0.43	1,126.95	33.11	1,129.60	52.25	1,132.25	67.05
1,124.35	0.60	1,127.00	33.83	1,129.65	52.57	1,132.30	67.30
1,124.40	0.79	1,127.05	34.55	1,129.70	52.89	1,132.35	67.54
1,124.45	1.01	1,127.10	35.25	1,129.75	53.20	1,132.40	67.79
1,124.50	1.26	1,127.15	35.93	1,129.80	53.51	1,132.45	68.03
1,124.55	1.53	1,127.20	36.59	1,129.85	53.82	1,132.50	68.28
1,124.60	1.83	1,127.25	37.24	1,129.90	54.13	1,132.55	68.52
1,124.65	2.16	1,127.30	37.86	1,129.95	54.44	1,132.60	68.76
1,124.70	2.51	1,127.35	38.45	1,130.00	54.74	1,132.65	69.01
1,124.75	2.88	1,127.40	39.02	1,130.05	55.05	1,132.70	69.25
1,124.80	3.28	1,127.45	39.56	1,130.10	55.35	1,132.75	69.49
1,124.85	3.70	1,127.50	40.07	1,130.15	55.65	1,132.80	69.73
1,124.90	4.14	1,127.55	40.54	1,130.20	55.95	1,132.85	69.96
1,124.95	4.61	1,127.60	40.96	1,130.25	56.24	1,132.90	70.20
1,125.00	5.10	1,127.65	41.34	1,130.30	56.54	1,132.95	70.44
1,125.05	5.60	1,127.70	41.67	1,130.35	56.83	1,133.00	70.67
1,125.10	6.13	1,127.75	41.93	1,130.40	57.13	1,133.05	70.91
1,125.15	6.68	1,127.80	42.12	1,130.45	57.42	1,133.10	71.14
1,125.20	7.24	1,127.85	42.22	1,130.50	57.71	1,133.15	71.38
1,125.25	7.83	1,127.90	42.18	1,130.55	57.99	1,133.20	71.61
1,125.30	8.43	1,127.95	41.94	1,130.60	58.28	1,133.25	71.84
1,125.35	9.04	1,128.00	40.80	1,130.65	58.56	1,133.30	72.07
1,125.40	9.67	1,128.05	41.21	1,130.70	58.85	1,133.35	72.30
1,125.45	10.32	1,128.10	41.61	1,130.75	59.13	1,133.40	72.53
1,125.50	10.98	1,128.15	42.01	1,130.80	59.41	1,133.45	72.76
1,125.55	11.66	1,128.20	42.40	1,130.85	59.69	1,133.50	72.99
1,125.60	12.35	1,128.25	42.80	1,130.90	59.97	1,133.55	73.22
1,125.65	13.05	1,128.30	43.18	1,130.95	60.25	1,133.60	73.45
1,125.70	13.76	1,128.35	43.57	1,131.00	60.52	1,133.65	73.67
1,125.75	14.49	1,128.40	43.95	1,131.05	60.80	1,133.70	73.90
1,125.80	15.22	1,128.45	44.32	1,131.10	61.07	1,133.75	74.12
1,125.85	15.97	1,128.50	44.70	1,131.15	61.34	1,133.80	74.35
1,125.90	16.72	1,128.55	45.07	1,131.20	61.61	1,133.85	74.57
1,125.95	17.48	1,128.60	45.44	1,131.25	61.88	1,133.90	74.79
1,126.00	18.25	1,128.65	45.80	1,131.30	62.15	1,133.95	75.02
1,126.05	19.02	1,128.70	46.16	1,131.35	62.42	1,134.00	75.24
1,126.10	19.80	1,128.75	46.52	1,131.40	62.68		
1,126.15	20.59	1,128.80	46.88	1,131.45	62.95		
1,126.20	21.38	1,128.85	47.23	1,131.50	63.21		
1,126.25	22.17	1,128.90	47.59	1,131.55	63.48		
1,126.30	22.96	1,128.95	47.93	1,131.60	63.74		
1,126.35	23.76	1,129.00	48.28	1,131.65	64.00		
1,126.40	24.56	1,129.05	48.62	1,131.70	64.26		
1,126.45	25.36	1,129.10	48.96	1,131.75	64.52		
1,126.50	26.15	1,129.15	49.30	1,131.80	64.77		
1,126.55	26.95	1,129.20	49.64	1,131.85	65.03		
1,126.60	27.74	1,129.25	49.97	1,131.90	65.29		

Stage-Area-Storage for Pond 5P: P1

Elevation (feet)	Surface (sq-ft)	Storage (cubic-feet)	Elevation (feet)	Surface (sq-ft)	Storage (cubic-feet)
1,124.00	548,007	0	1,129.30	711,088	3,140,969
1,124.10	549,112	54,856	1,129.40	713,506	3,212,199
1,124.20	550,219	109,823	1,129.50	715,927	3,283,671
1,124.30	551,327	164,900	1,129.60	718,353	3,355,385
1,124.40	552,435	220,088	1,129.70	720,783	3,427,341
1,124.50	553,545	275,387	1,129.80	723,217	3,499,541
1,124.60	554,656	330,797	1,129.90	725,655	3,571,985
1,124.70	555,768	386,318	1,130.00	728,097	3,644,672
1,124.80	556,881	441,951	1,130.10	730,658	3,717,610
1,124.90	557,996	497,694	1,130.20	733,223	3,790,804
1,125.00	559,111	553,550	1,130.30	735,793	3,864,255
1,125.10	560,222	609,516	1,130.40	738,367	3,937,963
1,125.20	561,334	665,594	1,130.50	740,945	4,011,928
1,125.30	562,447	721,783	1,130.60	743,528	4,086,152
1,125.40	563,562	778,084	1,130.70	746,116	4,160,634
1,125.50	564,677	834,496	1,130.80	748,708	4,235,375
1,125.60	565,794	891,019	1,130.90	751,305	4,310,376
1,125.70	566,911	947,654	1,131.00	753,906	4,385,636
1,125.80	568,030	1,004,401	1,131.10	756,512	4,461,157
1,125.90	569,150	1,061,260	1,131.20	759,122	4,536,939
1,126.00	570,271	1,118,232	1,131.30	761,737	4,612,982
1,126.10	571,388	1,175,314	1,131.40	764,356	4,689,287
1,126.20	572,505	1,232,509	1,131.50	766,980	4,765,853
1,126.30	573,624	1,289,816	1,131.60	769,609	4,842,683
1,126.40	574,744	1,347,234	1,131.70	772,241	4,919,775
1,126.50	575,865	1,404,764	1,131.80	774,879	4,997,131
1,126.60	576,987	1,462,407	1,131.90	777,521	5,074,751
1,126.70	578,110	1,520,162	1,132.00	780,167	5,152,636
1,126.80	579,234	1,578,029	1,132.10	784,886	5,230,888
1,126.90	580,360	1,636,009	1,132.20	789,620	5,309,613
1,127.00	581,486	1,694,101	1,132.30	794,368	5,388,813
1,127.10	582,608	1,752,306	1,132.40	799,130	5,468,487
1,127.20	583,732	1,810,623	1,132.50	803,906	5,548,639
1,127.30	584,856	1,869,052	1,132.60	808,697	5,629,269
1,127.40	585,982	1,927,594	1,132.70	813,501	5,710,379
1,127.50	587,108	1,986,248	1,132.80	818,320	5,791,970
1,127.60	588,236	2,045,016	1,132.90	823,154	5,874,043
1,127.70	589,365	2,103,896	1,133.00	828,001	5,956,601
1,127.80	590,495	2,162,889	1,133.10	847,715	6,040,385
1,127.90	591,626	2,221,995	1,133.20	867,660	6,126,152
1,128.00	592,758	2,281,214	1,133.30	887,837	6,213,925
1,128.10	603,439	2,341,023	1,133.40	908,247	6,303,727
1,128.20	614,215	2,401,905	1,133.50	928,888	6,395,582
1,128.30	625,087	2,463,869	1,133.60	949,761	6,489,512
1,128.40	636,054	2,526,926	1,133.70	970,866	6,585,542
1,128.50	647,117	2,591,083	1,133.80	992,203	6,683,693
1,128.60	658,275	2,656,352	1,133.90	1,013,772	6,783,990
1,128.70	669,528	2,722,741	1,134.00	1,035,573	6,886,455
1,128.80	680,877	2,790,261			
1,128.90	692,321	2,858,920			
1,129.00	703,860	2,928,728			
1,129.10	706,265	2,999,234			
1,129.20	708,675	3,069,981			

Summary for Pond 5P: P1

Inflow Area = 90.253 ac, 67.65% Impervious, Inflow Depth = 3.39" for 10-Year event
 Inflow = 243.54 cfs @ 12.45 hrs, Volume= 25.485 af
 Outflow = 11.50 cfs @ 15.33 hrs, Volume= 20.702 af, Atten= 95%, Lag= 172.9 min
 Primary = 11.50 cfs @ 15.33 hrs, Volume= 20.702 af

Routing by Stor-Ind method, Time Span= 2.00-80.00 hrs, dt= 0.01 hrs
 Peak Elev= 1,125.54' @ 15.33 hrs Surf.Area= 565,098 sf Storage= 855,803 cf

Plug-Flow detention time= 1,093.5 min calculated for 20.702 af (81% of inflow)
 Center-of-Mass det. time= 1,035.6 min (1,837.7 - 802.1)

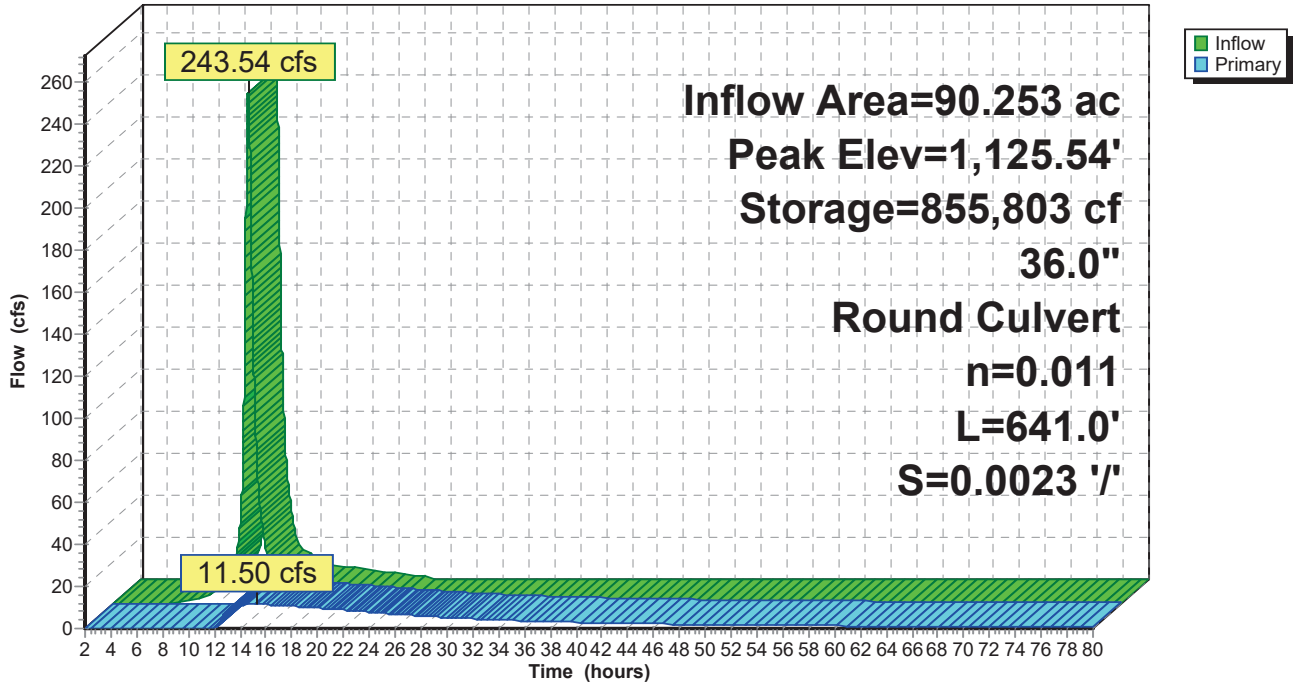
Volume	Invert	Avail.Storage	Storage Description		
#1	1,124.00'	6,886,455 cf	Custom Stage Data (Irregular) Listed below (Recalc)		
Elevation (feet)	Surf.Area (sq-ft)	Perim. (feet)	Inc.Store (cubic-feet)	Cum.Store (cubic-feet)	Wet.Area (sq-ft)
1,124.00	548,007	3,692.7	0	0	548,007
1,125.00	559,111	3,711.5	553,550	553,550	559,686
1,126.00	570,271	3,730.3	564,682	1,118,232	571,425
1,127.00	581,486	3,749.1	575,869	1,694,101	583,223
1,128.00	592,758	3,767.8	587,113	2,281,214	595,023
1,129.00	703,860	3,989.3	647,514	2,928,728	731,808
1,130.00	728,097	4,045.5	715,944	3,644,672	767,966
1,131.00	753,906	4,140.4	740,964	4,385,636	829,920
1,132.00	780,167	4,219.7	766,999	5,152,636	882,842
1,133.00	828,001	5,645.9	803,965	5,956,601	2,002,533
1,134.00	1,035,573	12,704.9	929,854	6,886,455	12,310,867

Device	Routing	Invert	Outlet Devices
#1	Primary	1,124.00'	36.0" Round Culvert L= 641.0' RCP, end-section conforming to fill, Ke= 0.500 Inlet / Outlet Invert= 1,124.00' / 1,122.50' S= 0.0023 '/' Cc= 0.900 n= 0.011 Concrete pipe, straight & clean, Flow Area= 7.07 sf

Primary OutFlow Max=11.49 cfs @ 15.33 hrs HW=1,125.54' (Free Discharge)
 ↑**1=Culvert** (Barrel Controls 11.49 cfs @ 4.59 fps)

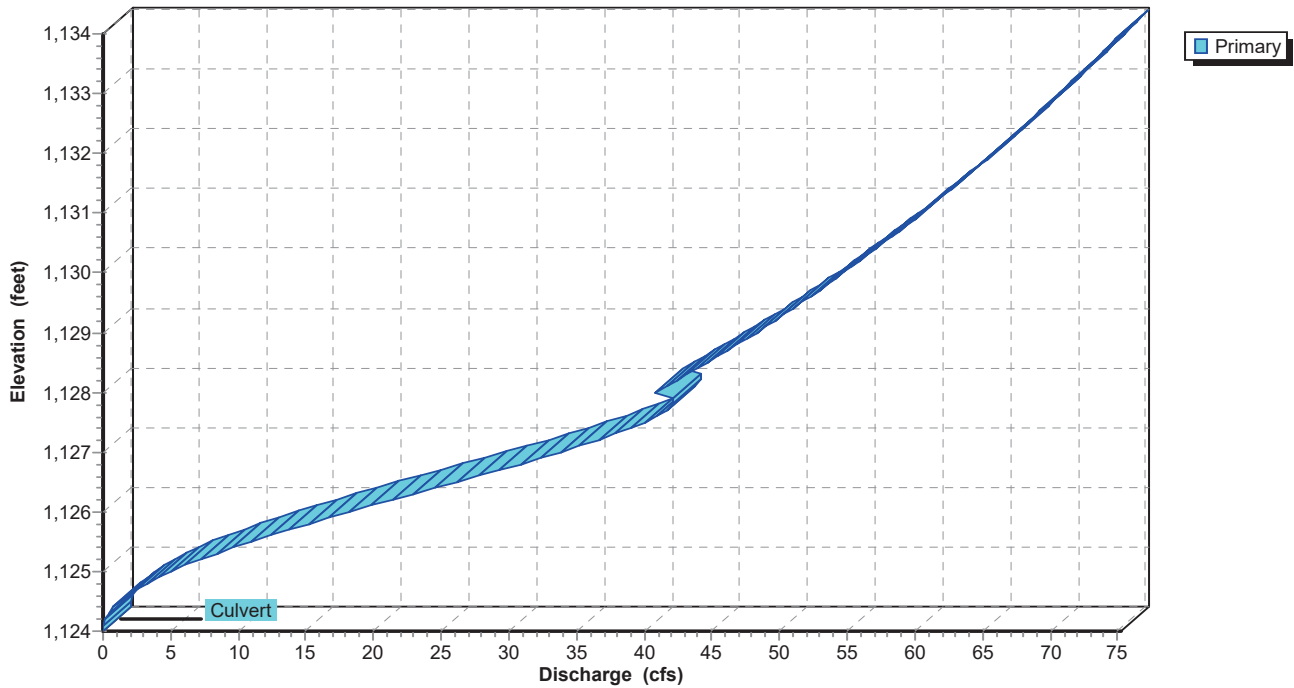
Pond 5P: P1

Hydrograph

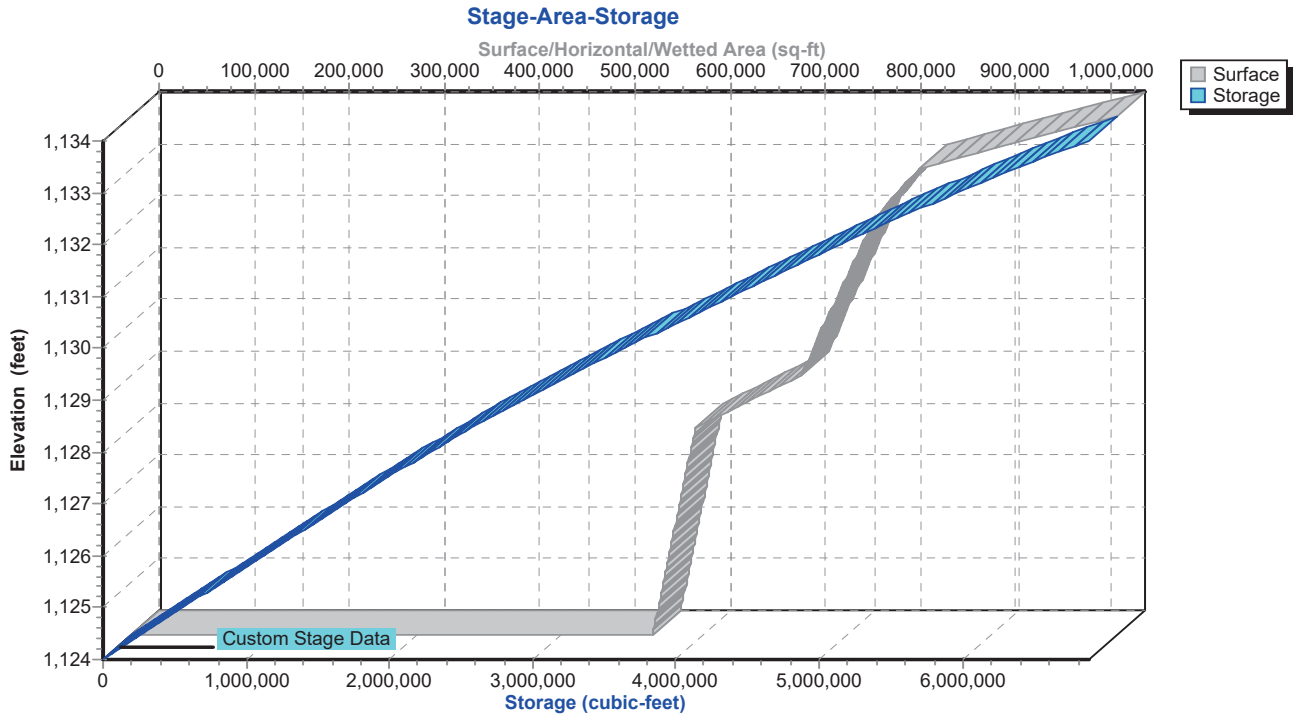


Pond 5P: P1

Stage-Discharge



Pond 5P: P1



Summary for Pond 5P: P1

Inflow Area = 90.253 ac, 67.65% Impervious, Inflow Depth = 6.08" for 100-Year event
 Inflow = 425.60 cfs @ 12.45 hrs, Volume= 45.761 af
 Outflow = 28.37 cfs @ 14.20 hrs, Volume= 40.436 af, Atten= 93%, Lag= 104.6 min
 Primary = 28.37 cfs @ 14.20 hrs, Volume= 40.436 af

Routing by Stor-Ind method, Time Span= 2.00-80.00 hrs, dt= 0.01 hrs
 Peak Elev= 1,126.64' @ 14.20 hrs Surf.Area= 577,437 sf Storage= 1,485,556 cf

Plug-Flow detention time= 907.6 min calculated for 40.436 af (88% of inflow)
 Center-of-Mass det. time= 862.3 min (1,653.3 - 791.0)

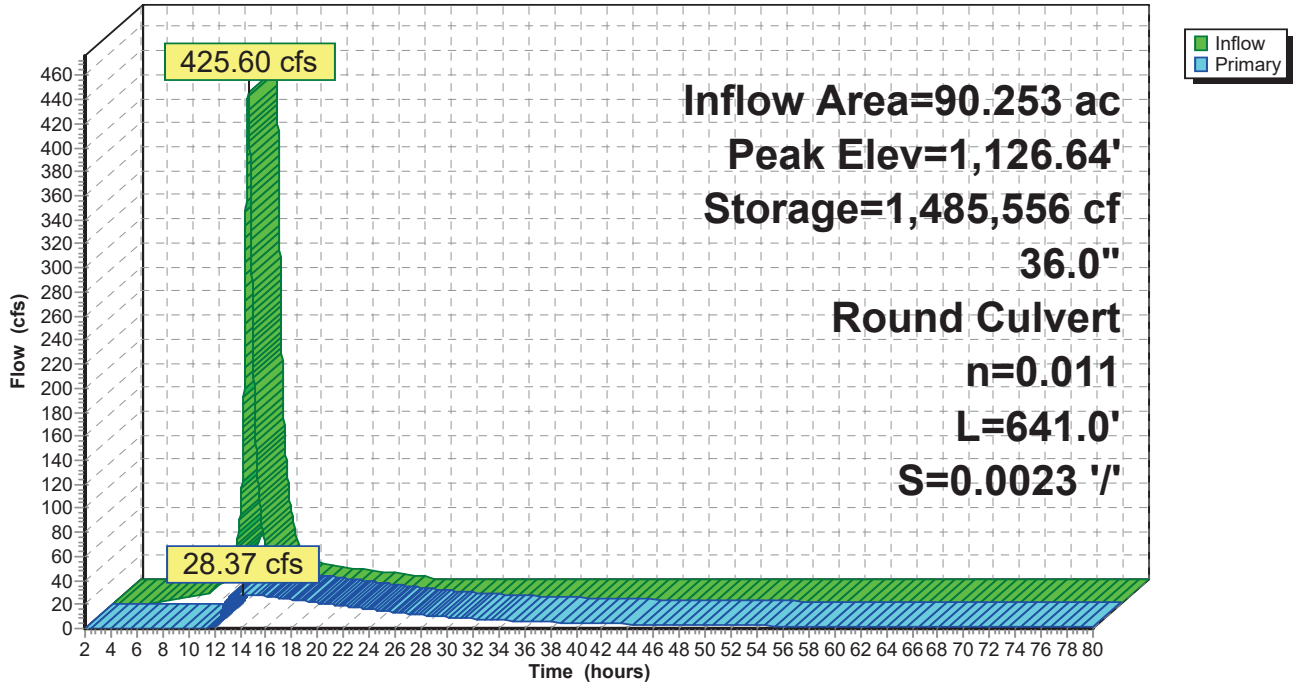
Volume	Invert	Avail.Storage	Storage Description		
#1	1,124.00'	6,886,455 cf	Custom Stage Data (Irregular) Listed below (Recalc)		
Elevation (feet)	Surf.Area (sq-ft)	Perim. (feet)	Inc.Store (cubic-feet)	Cum.Store (cubic-feet)	Wet.Area (sq-ft)
1,124.00	548,007	3,692.7	0	0	548,007
1,125.00	559,111	3,711.5	553,550	553,550	559,686
1,126.00	570,271	3,730.3	564,682	1,118,232	571,425
1,127.00	581,486	3,749.1	575,869	1,694,101	583,223
1,128.00	592,758	3,767.8	587,113	2,281,214	595,023
1,129.00	703,860	3,989.3	647,514	2,928,728	731,808
1,130.00	728,097	4,045.5	715,944	3,644,672	767,966
1,131.00	753,906	4,140.4	740,964	4,385,636	829,920
1,132.00	780,167	4,219.7	766,999	5,152,636	882,842
1,133.00	828,001	5,645.9	803,965	5,956,601	2,002,533
1,134.00	1,035,573	12,704.9	929,854	6,886,455	12,310,867

Device	Routing	Invert	Outlet Devices
#1	Primary	1,124.00'	36.0" Round Culvert L= 641.0' RCP, end-section conforming to fill, Ke= 0.500 Inlet / Outlet Invert= 1,124.00' / 1,122.50' S= 0.0023 ' /' Cc= 0.900 n= 0.011 Concrete pipe, straight & clean, Flow Area= 7.07 sf

Primary OutFlow Max=28.37 cfs @ 14.20 hrs HW=1,126.64' (Free Discharge)
 ↑**1=Culvert** (Barrel Controls 28.37 cfs @ 5.73 fps)

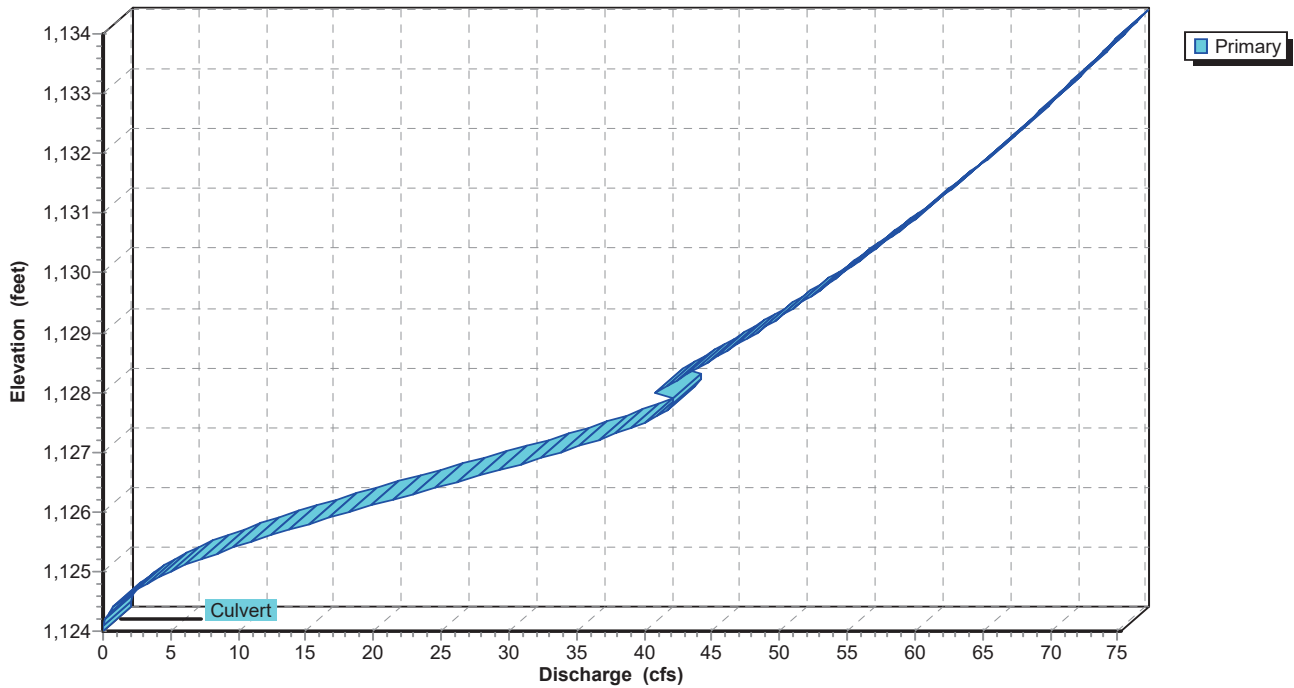
Pond 5P: P1

Hydrograph

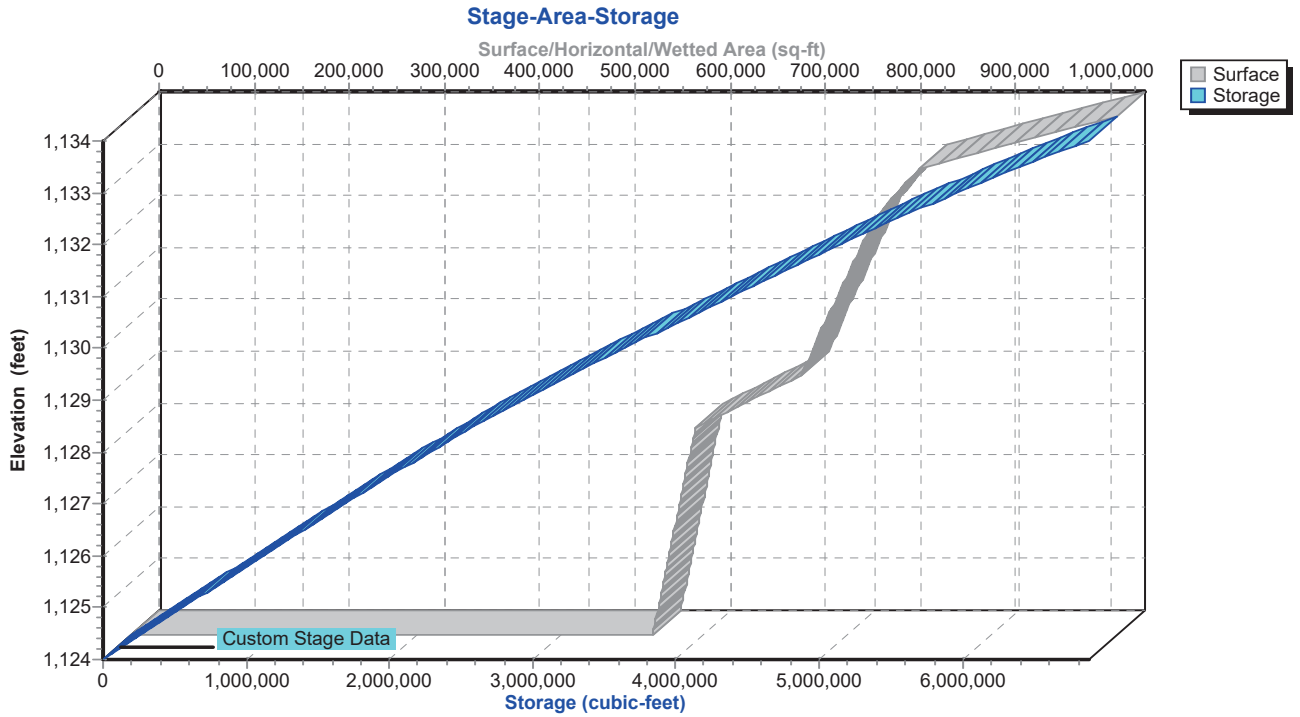


Pond 5P: P1

Stage-Discharge



Pond 5P: P1



TRAFFIC IMPACT STUDY

Lago Villaggio Valley, Nebraska

Prepared for:
Sudbeck Homes

Kimley»Horn



T R A F F I C I M P A C T S T U D Y

Lago Villaggio

Valley, Nebraska

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02/20/2026

February 2026

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1.0 EXECUTIVE SUMMARY

Kimley-Horn has prepared this report to document the results of a Traffic Impact Study for Lago Villaggio proposed to be located to the northeast of the King Lake Road and 252nd Street intersection in Valley, Nebraska. The project is proposed to include single-family detached, single-family attached, and multifamily housing totaling approximately 620 dwelling units. It is expected that the project will be completed over a period of several years with full buildout by approximately 2035; therefore, analysis was conducted for the 2035 short-term buildout horizon as well as the 2050 long-term twenty-year planning horizon.

Regional access to the site will be provided by US Highway 275 (US-275), W Maple Road (N-64), Pawnee Road (N-36), and US-6. Primary access will be provided by Meigs Street. Direct access will be provided by the three (3) proposed full movement accesses. There will be one access along 252nd Street, approximately 1,450 feet to the north of the intersection of 252nd Street and Meigs Street. Two accesses are proposed along King Lake Road approximately 525 feet and 1,625 feet to the east of the intersection of 252nd Street and King Lake Road.

Lago Villaggio is expected to generate approximately 4,684 weekday daily trips, with 337 of these trips occurring during the morning peak hour and 425 of these trips occurring during the afternoon peak hour.

Based on the analysis presented in this report, Kimley-Horn believes Lago Villaggio will be successfully incorporated into the existing and future roadway network. Analysis of the existing street network, the proposed project development, and expected traffic volumes resulted in the following recommendations:

2035 Near-Term Project Improvements

- With the completion of the project, one access is proposed on the east side of 252nd Street between Ida Street and Meigs Street. A southbound left turn lane is recommended at the 252nd Street access. A southbound left turn lane is also recommended at the 252nd Street & Meigs Street intersection.
- Two accesses are proposed along the north side of King Lake Road east of 252nd Street.

- All accesses are proposed to provide full turning movements and are recommended to have R1-1 “STOP” signs installed for control on the exiting approach. Each exiting approach is only anticipated to need a single lane for all exiting movements.
- A northbound right turn lane is recommended to be provided on the US-275 Northbound Off-Ramp approach to Meigs Street. While much of the traffic volume at this intersection is due to existing traffic volumes, the addition of project traffic to the northbound right turning movement also triggers this recommendation.

2050 Long-Term Improvements

With development of the Elkhorn Valley Youth Sports Complex, several surrounding area improvements should be constructed in association with that overall development. For purposes of this study, these additional recommended improvements are proposed in the 2050 horizon, but it is recommended the City, County, and NDOT (as appropriate) consider these improvements to be constructed alongside other roadway improvements in the study area planned as part of the Elkhorn Valley development to accommodate expected future growth.

- Both the US-275 Southbound (#1) and Northbound (#2) Ramps and Meigs Street intersections are anticipated to be signalized intersections. Likewise, Meigs Street may need an additional eastbound and westbound through lane to provide two through lanes in each direction through these two future signalized intersections.
- At the US-275 and Meigs Street Southbound Ramps (#1) intersection, dual westbound left turn lanes, an eastbound right turn lane, and a separate southbound left turn lane on the off-ramp may be needed at this intersection.
- The intersection of US-275 Northbound Ramps and Meigs Street (#2) is recommended to provide a westbound right turn lane. It is recommended that the previously proposed northbound right lane to be constructed in 2035 be improved to operate with channelized and operate with free right turning movements with an acceleration lane along eastbound Meigs Street constructed due to the high expected traffic volume performing this right turn with Elkhorn Valley development.
- The intersection of 252nd Street and Meigs Street (#3) will have a fourth leg constructed as part of Elkhorn Valley development. It is expected this intersection will meet warrants for signalization and need either a traffic signal or a roundabout. It is recommended the City consider installing a traffic signal or a single-lane roundabout at this intersection. If signalized,

northbound left, northbound right, southbound left, and westbound left turn lanes would be recommended.

- Similarly, the access intersection of 252nd Street and Street C (#5) is a proposed four-leg intersection with access to Elkhorn Valley provided on the west side of the roadway. This intersection is expected to operate acceptably with stop control on the minor street approaches, with the west leg providing access to Elkhorn Valley development and the east leg “Street C” serving as an access into the Lago Villaggio development. Northbound and southbound left turn lanes along 252nd Street are recommended at this intersection.

General Recommendations

- Any onsite or offsite improvements should be incorporated into the Civil Drawings and conform to standards of the City of Valley and Douglas County, as appropriate, as well as the Manual on Uniform Traffic Control Devices (MUTCD) – 11th Edition, 2023.

2.0 INTRODUCTION

Kimley-Horn has prepared this report to document the results of a Traffic Impact Study for Lago Villaggio proposed to be located to the northeast of the King Lake Road and 252nd Street intersection in Valley, Nebraska. A vicinity map illustrating the project development location is shown in **Figure 1**. The project is proposed to include single-family detached, single-family attached, and multifamily housing totaling approximately 620 dwelling units. A conceptual site plan is attached in **Appendix A**. It is expected that the project will be completed over a period of several years with full buildout by approximately 2035; therefore, analysis was conducted for the 2035 short-term buildout horizon as well as the 2050 long-term twenty-year planning horizon.

The purpose of this traffic study is to identify project traffic generation characteristics to determine potential project traffic related impacts on the local street system and to develop the necessary mitigation measures required for the identified traffic impacts. The following intersections were incorporated into this traffic study in accordance with the City of Valley standards and requirements:

- US-275 Southbound Ramps and Meigs Street (Intersection #1)
- US-275 Northbound Ramps and Meigs Street (#2)
- 252nd Street and Meigs Street (#3)
- 252nd Street and King Lake Road (#4)

In addition, the proposed full movement access along 252nd Street and the two proposed full movement accesses along King Lake Road were evaluated.

Regional access to the site will be provided by US Highway 275 (US-275), W Maple Road (N-64), Pawnee Road (N-36), and US-6. Primary access will be provided by Meigs Street. Direct access will be provided by the three (3) proposed full movement accesses. There will be one access, approximately 1,450 feet to the north of the intersection of 252nd Street and Meigs Street, which will be referred to as 252nd Street and Street C (#5). Two accesses are proposed approximately 525 feet and 1,625 feet to the east of the intersection of 252nd Street and King Lake Road; these accesses will be referred to as the intersection of Street G and King Lake Road (#6) as the western access and Street H and King Lake Road (#7) as the eastern access.



FIGURE 1
Lago Villaggio
Valley, Nebraska
Vicinity Map

3.0 EXISTING AND FUTURE CONDITIONS

3.1 Existing Study Area

The existing site is comprised of undeveloped land used for agriculture. To the east are single family residential homes. The Elkhorn Valley Youth Sports Complex is located to the west of the site and is partially built today, with full buildout of that site projected to occur over a period of the next several years. The remaining surrounding area is generally comprised of undeveloped farmland. The primary developed area within City of Valley limits is located to the southwest of this site.

3.2 Existing Roadway Network

US-275 is a four-lane highway which runs generally north/south in the vicinity of the site. The posted speed limit on this roadway is 65 miles per hour (mph). The Omaha Metropolitan Area Planning Agency (MAPA) classifies US-275 as a Freeway/Expressway in the area surrounding the project site.

Meigs Street is a two-lane roadway which runs east/west within the area adjacent to the site. Meigs Street is defined by MAPA as a Major Collector roadway and has a posted speed limit of 45 mph. The eastern end of Meigs Street alignment becomes 252nd Street to the north of the 252nd Street and Meigs Street intersection.

252nd Street is a two-lane roadway which runs north/south adjacent to the project site. It is defined by MAPA as a Major Collector roadway and has a posted speed limit of 45 mph.

King Lake Road is a two-lane roadway which runs east/west. King Lake Road is paved adjacent to the project site but becomes unpaved approximately half a mile to the east of the intersection of King Lake Road and 252nd Street. The posted speed limit of King Lake Road is 25 mph and it operates as a local roadway.

The unsignalized intersection of US-275 Southbound Ramps and Meigs Street (#1) operates under stop control for the southbound off-ramp approach. The southbound approach provides a single lane for all turning movements. The eastbound approach has one lane for both through and right turn movements. The westbound approach operates with a designated left turn lane and a separate through lane. An aerial photo of the existing intersection configuration is below.



US-275 Southbound Ramps and Meigs Street (#1)

The unsignalized intersection of US-275 Northbound Ramps Meigs Street (#2) operates under stop control for the northbound off-ramp approach. The northbound approach provides a single lane for all turning movements. The westbound approach has one lane for both through and right turn movements. The eastbound approach operates with a designated left turn lane and a separate through lane. An aerial photo of the existing intersection configuration is below.



US-275 Northbound Ramps and Meigs Street (#2)

The unsignalized intersection of 252nd Street and Meigs Street (#3) operates under stop control for the 252nd Street westbound approach. The westbound approach provides a single lane for shared left and right turning movements. The Meigs Street northbound approach provides a single lane for both through and right turning movements. The southbound 252nd Street approach has a single lane for left turn and through movements. An aerial photo of the existing intersection configuration is below.



252nd Street and Meigs Street (#3)

The unsignalized intersection of 252nd Street and King Lake Road (#4) operates under stop control for the westbound approach. All approaches to the intersection provide one lane for shared turning movements. Of note, for purposes of this study the west leg was identified as the farmhouse driveway that is slightly offset to the south of this intersection. An aerial photo of the existing intersection configuration is below.



252nd Street and King Lake Road (#4)

The intersection lane configuration and control for the study area intersections are shown in **Figure 2**.

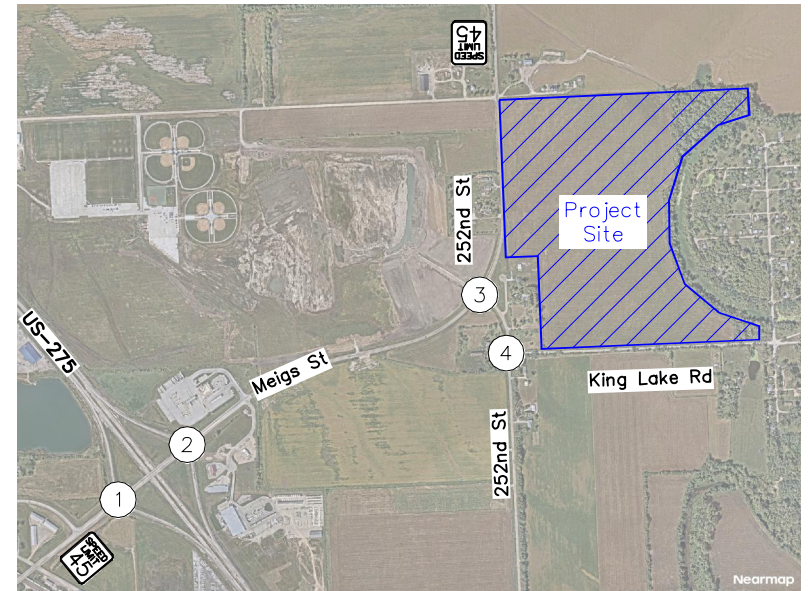
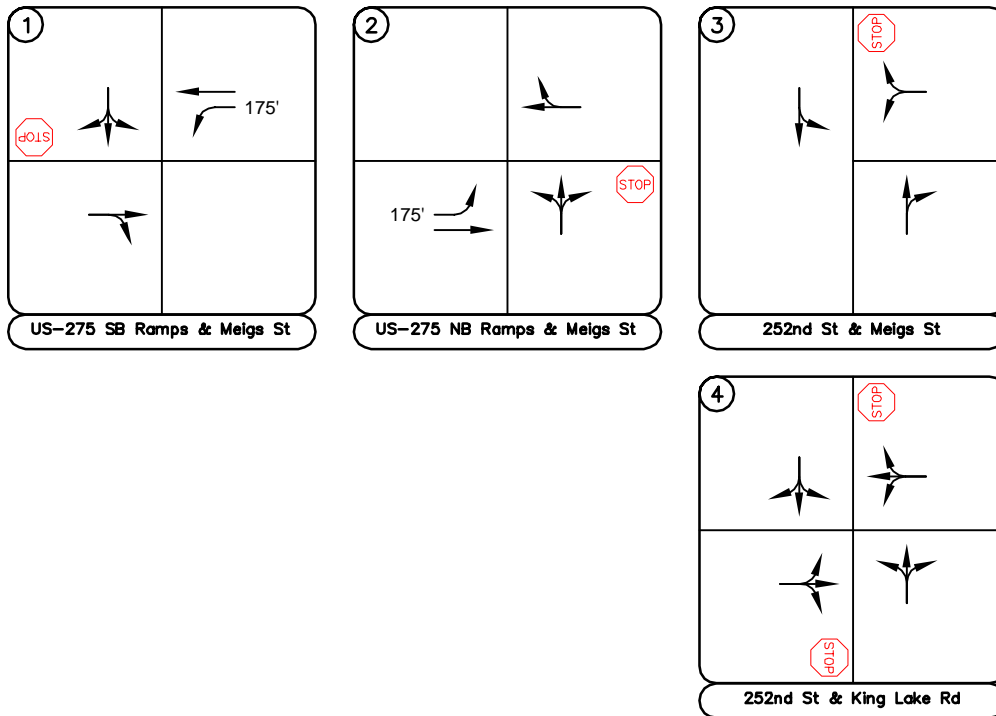


FIGURE 2
 Lago Villaggio
 Valley, Nebraska
 Existing Geometry & Control

LEGEND

- Study Area Key Intersection
- Stop-Controlled Approach
- Roadway Speed Limit
- 100' Turn Lane Length (feet)

3.3 Existing Traffic Volumes

Existing turning movement counts were conducted at the study intersections on Thursday, January 22, 2026 during the weekday morning and afternoon peak hours. The counts were conducted during the morning and afternoon peak hours of adjacent street traffic in 15-minute intervals from 7:00 AM to 9:00 AM and 4:00 PM to 6:00 PM on this count date. The existing intersection traffic volumes are shown in **Figure 3** with count sheets provided in **Appendix B**.

3.4 Unspecified Development Traffic Growth

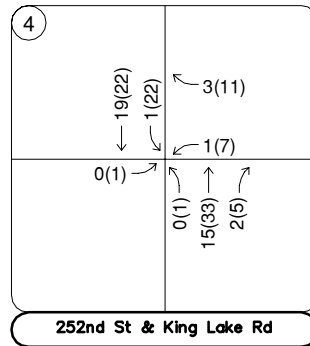
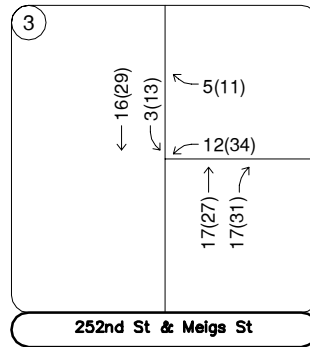
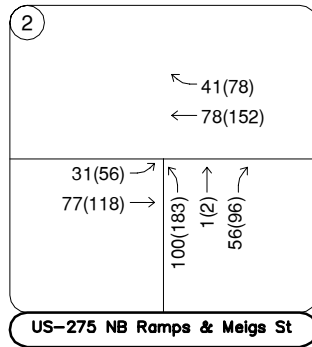
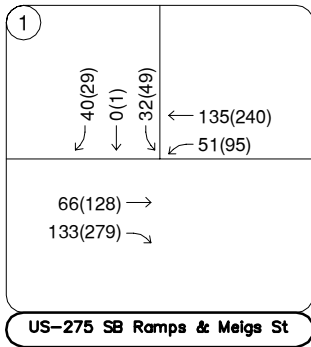
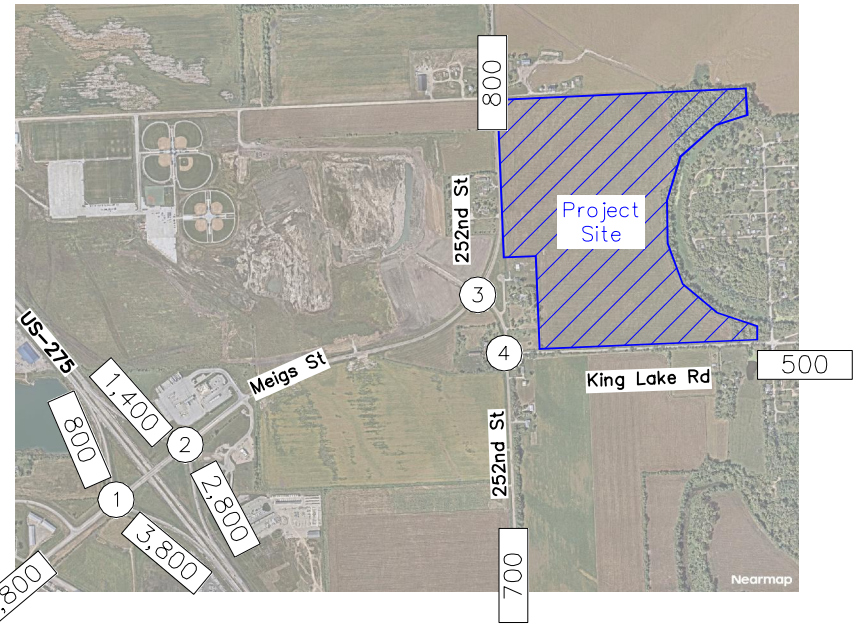
According to traffic projections from the Metropolitan Area Planning Agency (MAPA) traffic model, the area surrounding the site is expected to have an average 30-year growth factor of 1.61. This growth factor equates to an annual growth rate of 1.60 percent. This annual growth rate was used to estimate the short-term 2035 and long-term 2050 traffic volume projections at the study area intersections prior to construction of the project. The MAPA traffic projections are provided in **Appendix C**.

Additionally, a future known development, Elkhorn Valley Youth Sports Complex (also known as Catalina development), is anticipated to be constructed in this study area and was included in the background traffic volumes in this study for the long-term 2050 horizon. For purposes of this study, it was assumed this development would include 6 soccer fields, 17 baseball fields, 207 single family residential units, 480 multifamily units, a hotel, convenience store, along with approximately 312,500 square feet of commercial/retail uses. Trip generation and traffic assignment were applied to the surrounding street network for this development. The calculated background traffic volumes for 2035 and 2050 are shown in **Figure 4** and **Figure 5**, respectively.

Thurs, Jan 22, 2026
7:15 to 8:15AM
(4:00 to 5:00PM)

Thurs, Jan 22, 2026
7:30 to 8:30AM
(4:00 to 5:00PM)

Thurs, Jan 22, 2026
7:45 to 8:45AM
(4:30 to 5:30PM)



Thurs, Jan 22, 2026
7:45 to 8:45AM
(4:30 to 5:30PM)

FIGURE 3
Lago Villaggio
Valley, Nebraska
2026 Existing Traffic Volumes

LEGEND

- ⊗ Study Area Key Intersection
- XXX(XXX) Weekday AM(PM) Peak Hour Traffic Volumes
- XX,X00 Estimated Daily Traffic Volume

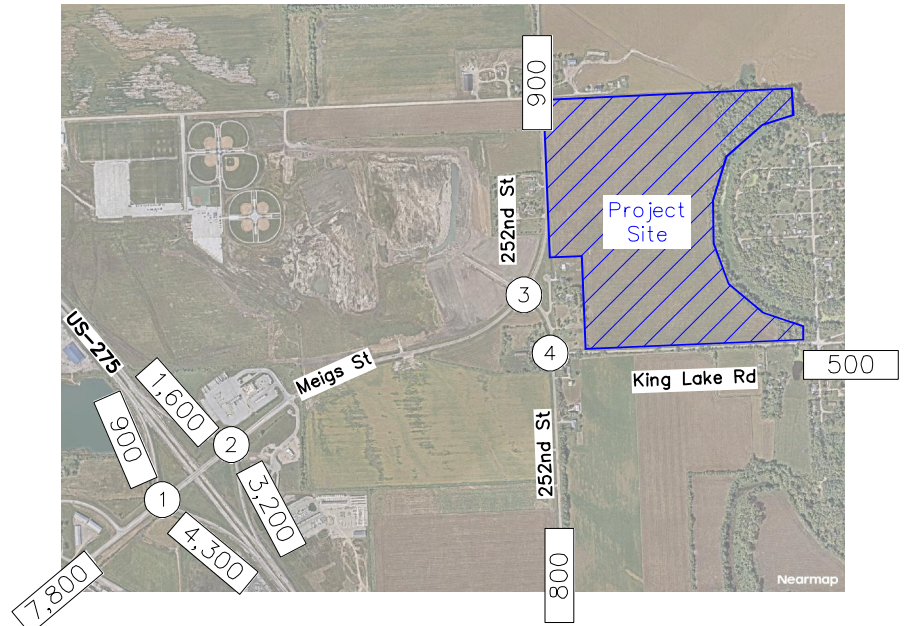
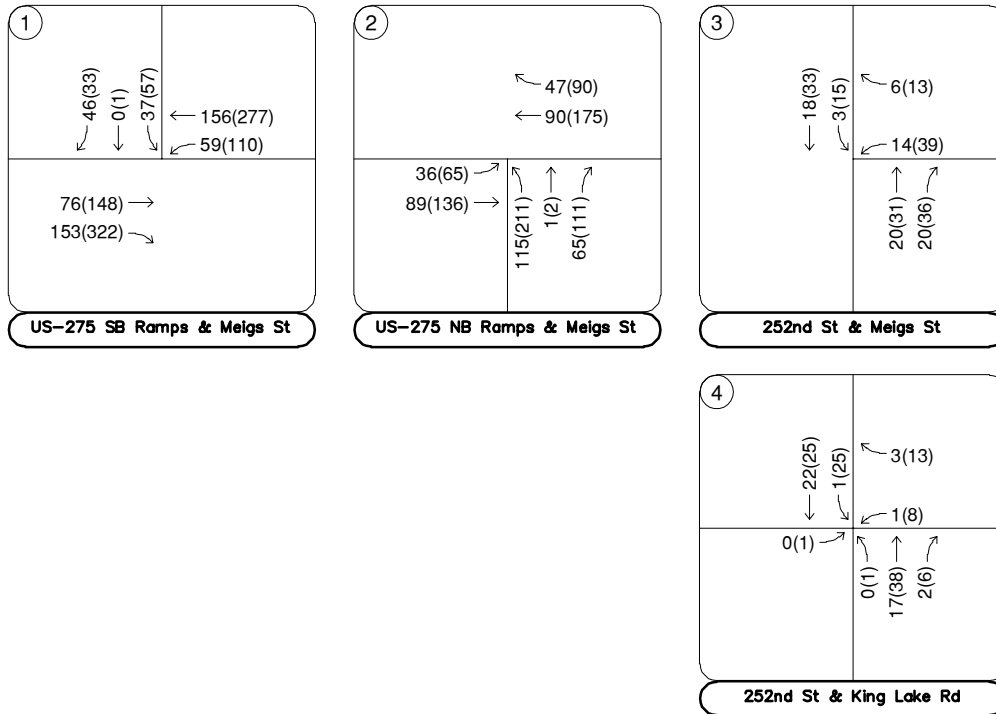
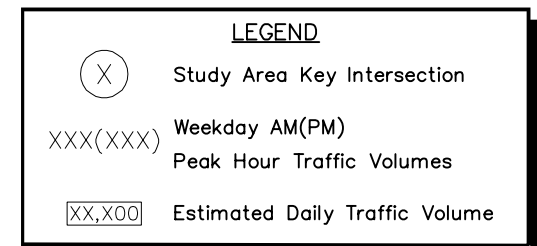


FIGURE 4
Lago Villaggio
Valley, Nebraska
2035 Background Traffic Volumes



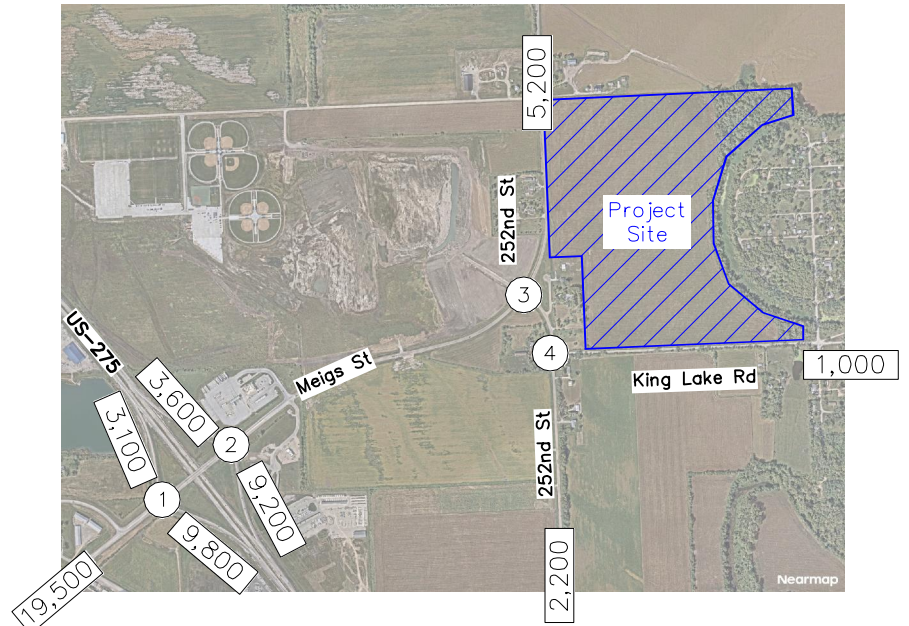
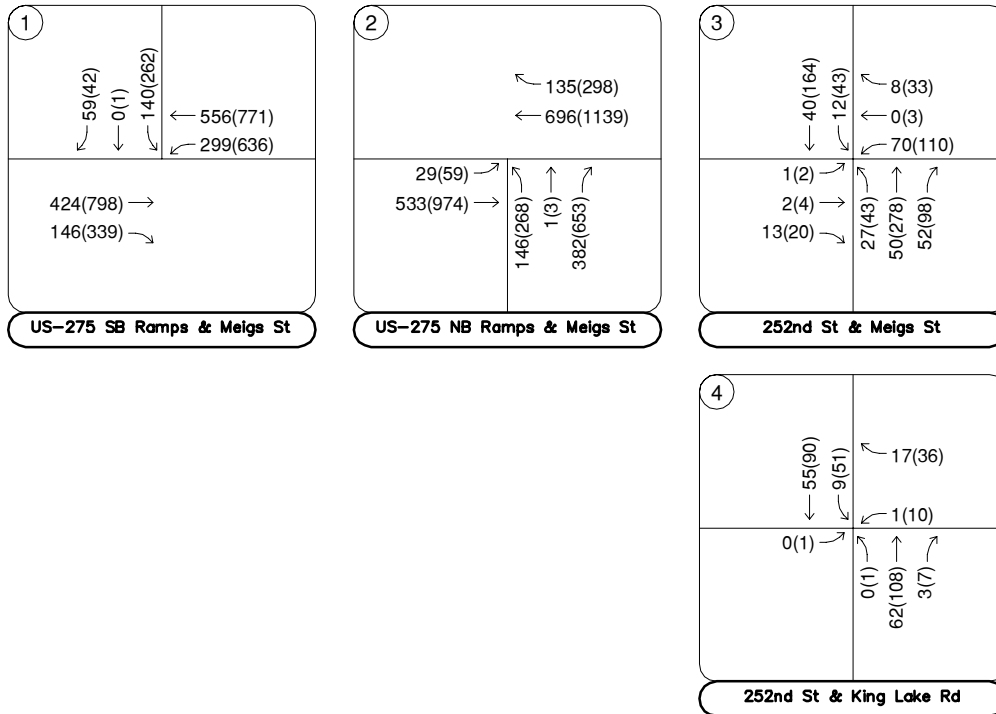


FIGURE 5
Lago Villaggio
Valley, Nebraska
2050 Background Traffic Volumes

LEGEND

(X) Study Area Key Intersection

xxx(XXX) Weekday AM(PM)
Peak Hour Traffic Volumes

xx,x00 Estimated Daily Traffic Volume

4.0 PROJECT TRAFFIC CHARACTERISTICS

4.1 Trip Generation

Site-generated traffic estimates are determined through a process known as trip generation. Rates and equations are applied to the proposed land use to estimate traffic generated by the development during a specific time interval. The acknowledged source for trip generation rates is the *Trip Generation Manual*¹ published by the Institute of Transportation Engineers (ITE). ITE has established trip rates in nationwide studies of similar land uses. For this study, Kimley-Horn used the ITE Trip Generation Report average rates/fitted curve equations that apply to Single-Family Detached Housing (ITE Land Used Code 210), Single-Family Attached Housing (ITE 215), and Multifamily Low-Rise Housing (ITE 220) for traffic associated with the development.

Lago Villaggio is expected to generate approximately 4,684 weekday daily trips, with 337 of these trips occurring during the morning peak hour and 425 of these trips occurring during the afternoon peak hour. Calculations were based on the procedure and information provided in the ITE *Trip Generation Manual, 12th Edition – Volume 1: User’s Guide and Handbook, 2025*. **Table 1** summarizes the estimated trip generation for the site. The trip generation worksheet is included in **Appendix D**.

Table 1 – Lago Villaggio Traffic Generation

Land Use and Size	Weekday Vehicle Trips						
	Daily	AM Peak Hour			PM Peak Hour		
		In	Out	Total	In	Out	Total
Single Family Detached Housing (ITE 210) – 294 Dwelling Units	2,638	55	148	203	161	99	260
Single Family Attached Housing (ITE 215) – 96 Dwelling Units	631	10	31	41	27	20	47
Multifamily Low-Rise Housing (ITE 220) – 230 Dwelling Units	1,415	22	71	93	73	45	118
Total Project Trips	4,684	87	250	337	261	164	425

¹ Institute of Transportation Engineers, *Trip Generation Manual*, Twelfth Edition, Washington DC, 2025.

4.2 Trip Distribution

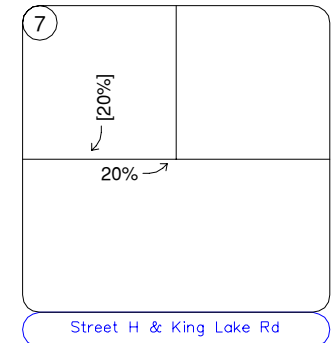
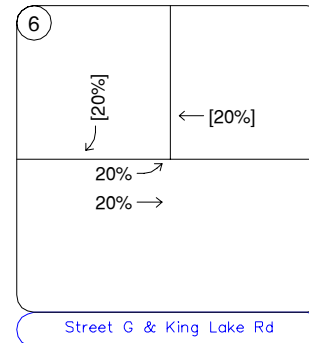
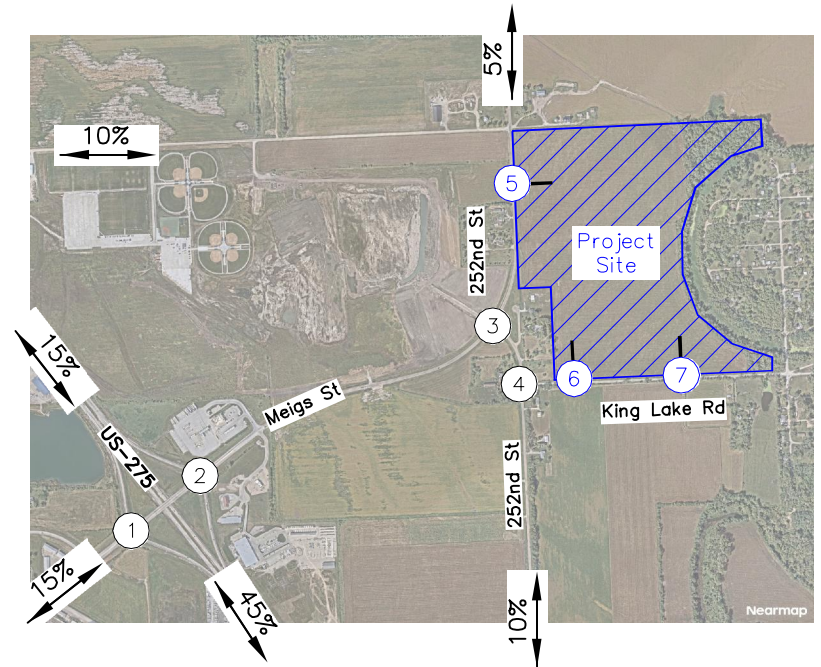
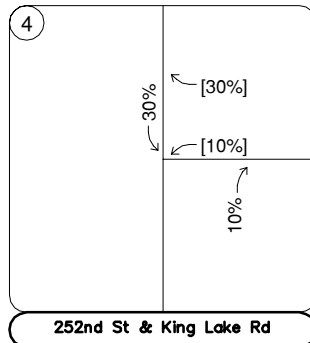
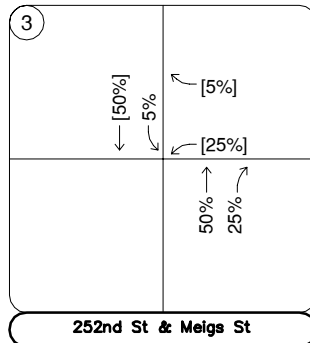
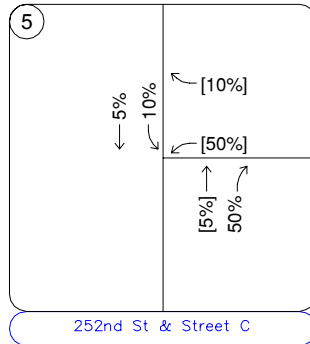
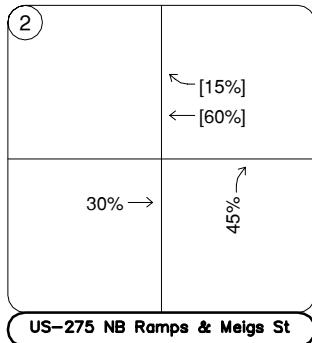
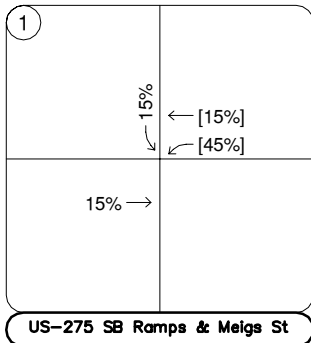
Distribution of site traffic on the street system was based on the area street system characteristics, existing traffic patterns, existing and anticipated surrounding demographic information, and the proposed access system for the project. The directional distribution of traffic is a means to quantify the percentage of site-generated traffic that approaches the site from a given direction and departs the site back to the original source. The project trip distribution for the proposed development is illustrated in **Figure 6**.

4.3 Traffic Assignment

The project's traffic assignment was obtained by applying the project trip distribution to the estimated traffic generation of the development shown in **Table 1**. Traffic assignment is shown in **Figure 7**.

4.4 Total (Background Plus Project) Traffic

Site traffic volumes were added to the background volumes to represent estimated traffic conditions for the short-term 2035 buildout horizon and long-term 2050 twenty-year planning horizon. These total traffic volumes for the study area are illustrated for the 2035 and 2050 horizon years in **Figure 8** and **Figure 9**, respectively.



LEGEND

- (X) Study Area Key Intersection
- (X) Project Access Intersection
- XX% External Trip Distribution Percentage
- XX%[XX%] Entering[Exiting] Trip Distribution Percentage

FIGURE 6
Lago Villaggio
Valley, Nebraska
Project Trip Distribution

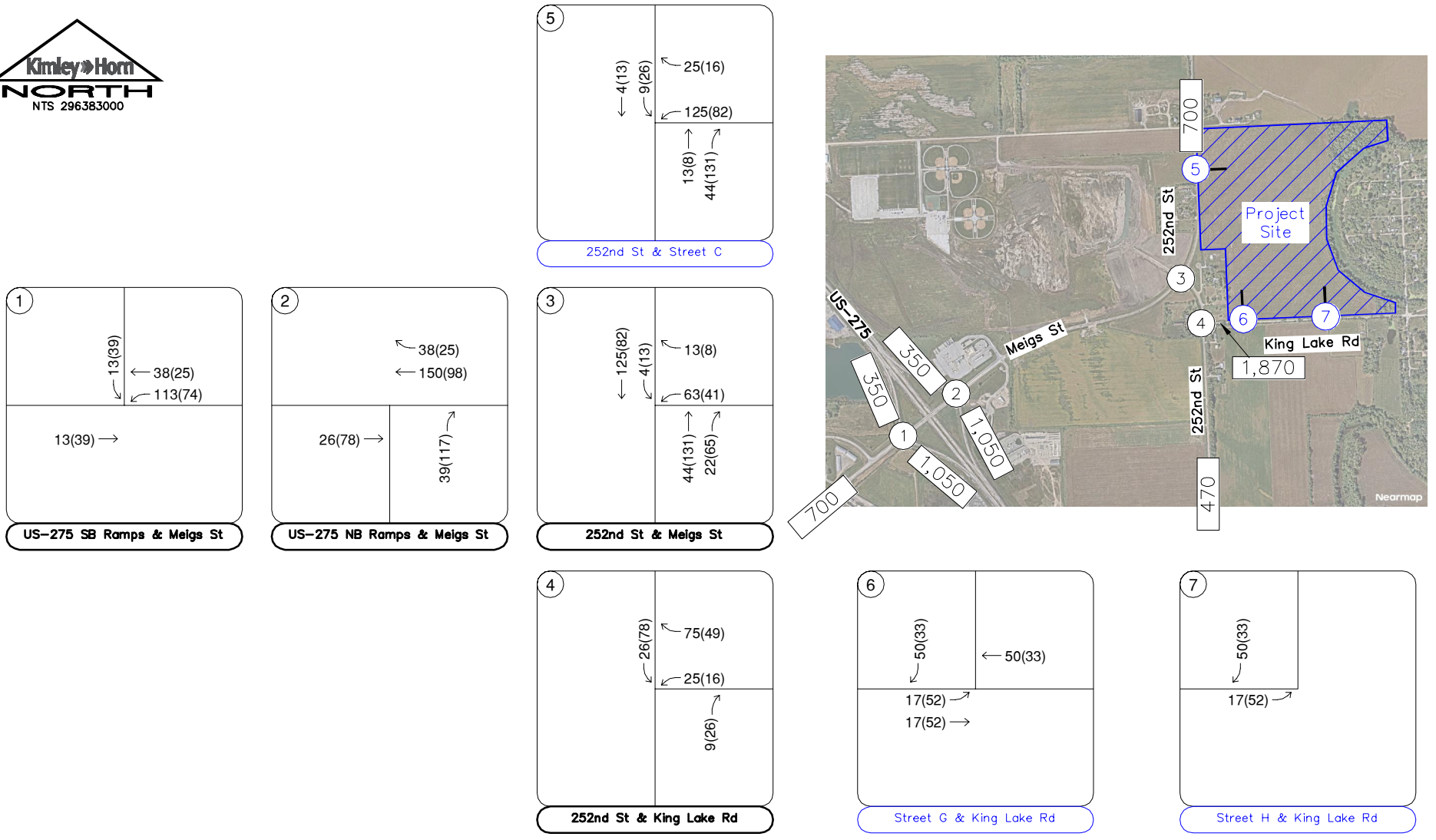
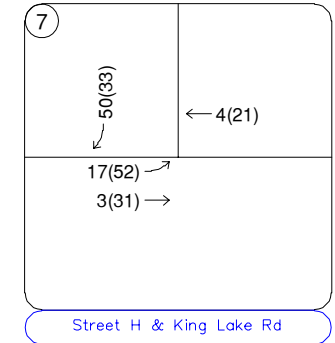
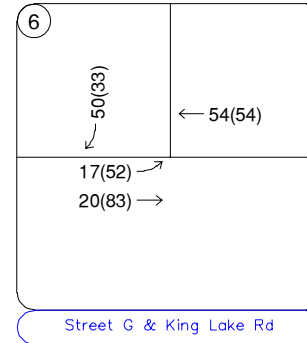
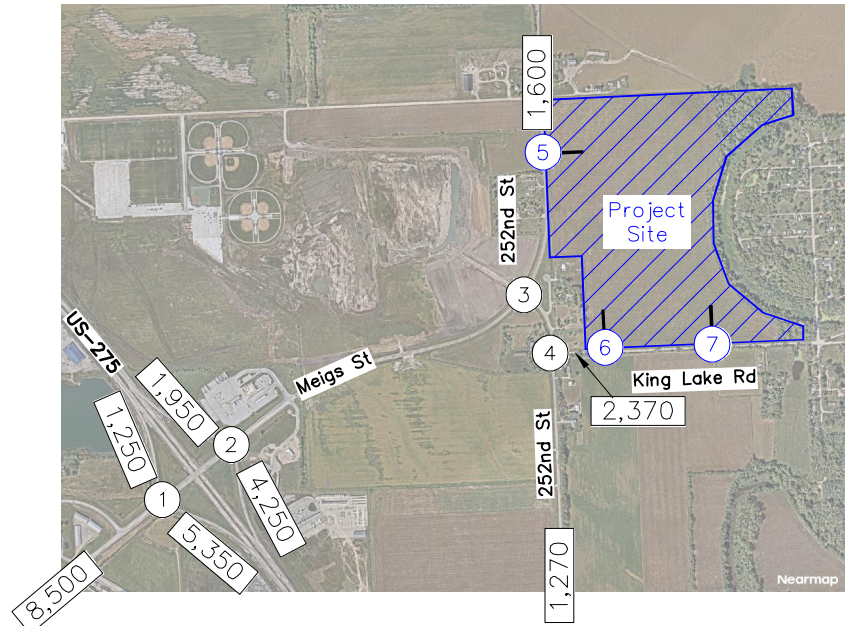
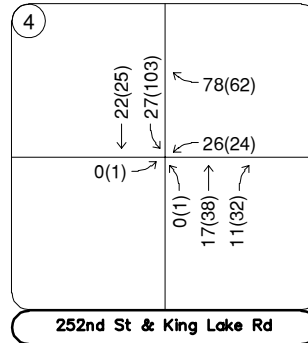
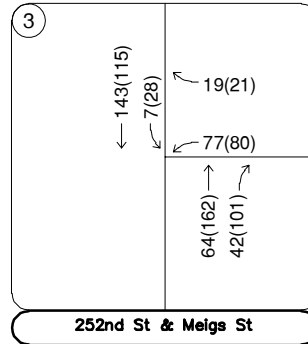
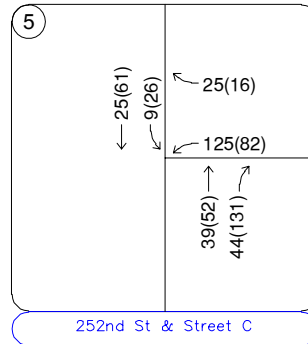
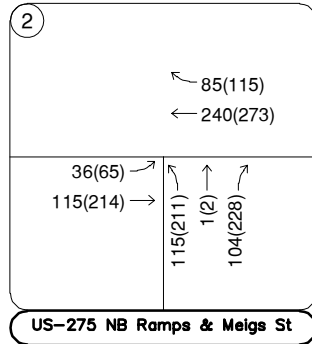
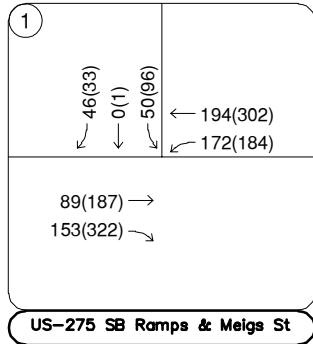


FIGURE 7
Lago Villaggio
Valley, Nebraska
Project Traffic Assignment

LEGEND

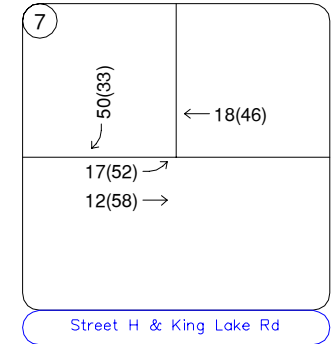
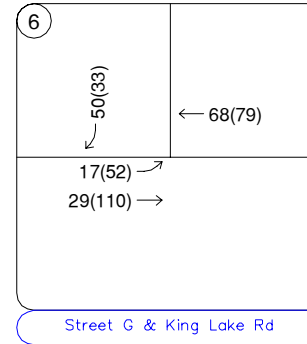
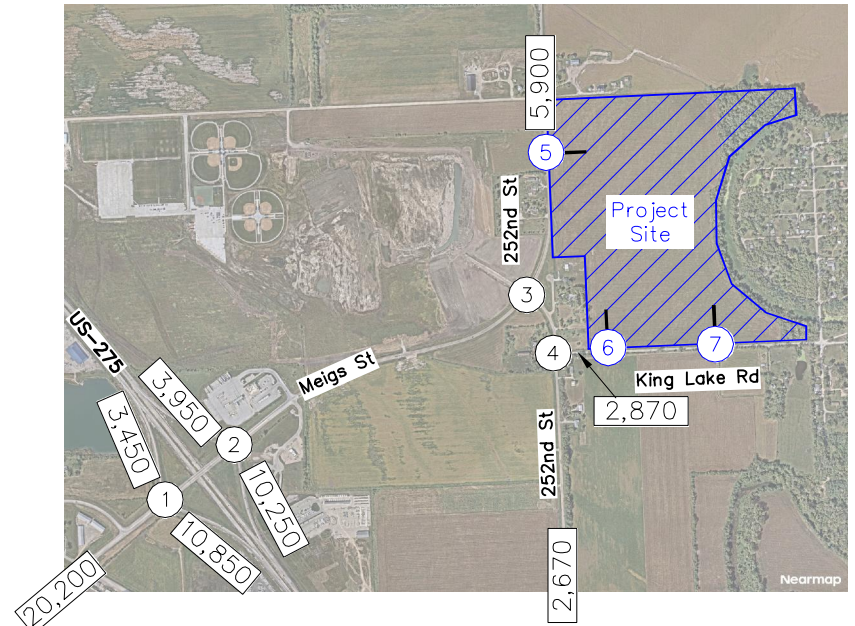
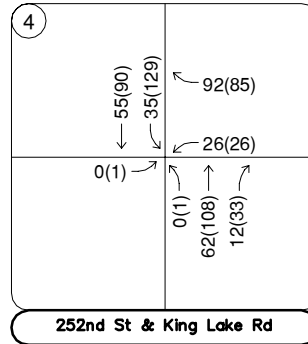
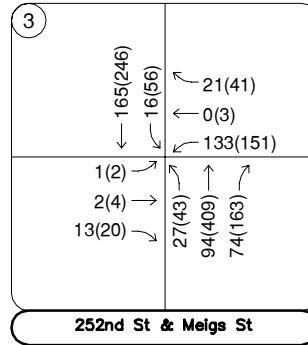
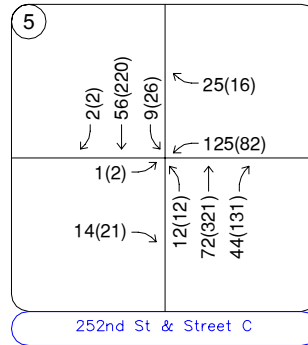
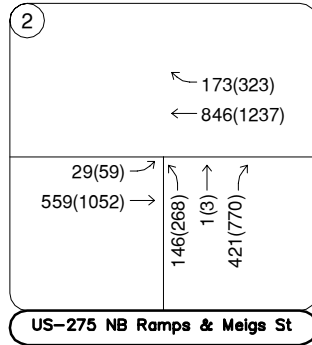
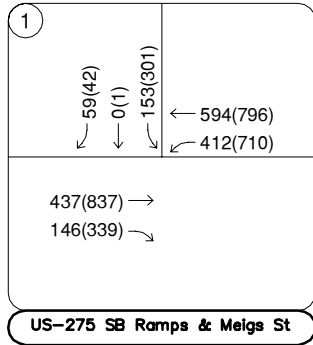
- X Study Area Key Intersection
- X Project Access Intersection
- XXX(XXX) Weekday AM(PM)
Peak Hour Traffic Volumes
- XX,X00 Estimated Daily Traffic Volume



LEGEND

- X Study Area Key Intersection
- X Project Access Intersection
- XXX(XXX) Weekday AM(PM) Peak Hour Traffic Volumes
- XX,X00 Estimated Daily Traffic Volume

FIGURE 8
Lago Villaggio
Valley, Nebraska
2035 Total Traffic Volumes



LEGEND



-  Study Area Key Intersection
-  Project Access Intersection
- XXX(XXX) Weekday AM(PM)
Peak Hour Traffic Volumes
- XX,X00 Estimated Daily Traffic Volume

FIGURE 9
Lago Villaggio
Valley, Nebraska
2050 Total Traffic Volumes

5.0 TRAFFIC OPERATIONS ANALYSIS

Kimley-Horn's analysis of traffic operations in the site vicinity was conducted to determine potential capacity deficiencies in the 2035 and 2050 development horizons at the identified key intersections. The acknowledged source for determining overall capacity is the *Highway Capacity Manual (HCM)*².

5.1 Analysis Methodology

Capacity analysis results are listed in terms of Level of Service (LOS). LOS is a qualitative term describing operating conditions a driver will experience while traveling on a particular street or highway during a specific time interval. It ranges from A (very little delay) to F (long delays and congestion). For intersections and roadways in this study area, standard traffic engineering practice recommends overall intersection LOS D and movement/approach LOS E as the minimum desirable thresholds for acceptable operations. **Table 2** shows the definition of level of service for signalized and unsignalized intersections.

Table 2 – Level of Service Definitions

Level of Service	Signalized Intersection Average Total Delay (sec/veh)	Unsignalized Intersection Average Total Delay (sec/veh)
A	≤ 10	≤ 10
B	> 10 and ≤ 20	> 10 and ≤ 15
C	> 20 and ≤ 35	> 15 and ≤ 25
D	> 35 and ≤ 55	> 25 and ≤ 35
E	> 55 and ≤ 80	> 35 and ≤ 50
F	> 80	> 50

Definitions provided from the Highway Capacity Manual, Seventh Edition, Transportation Research Board, 2022.

Study area intersections were analyzed based on average total delay analysis for signalized and unsignalized intersections. Under the unsignalized analysis, the LOS for a two-way stop-controlled intersection is determined by the computed or measured control delay and is defined for each minor movement. LOS for a two-way stop-controlled intersection is not defined for the whole intersection. LOS for signalized intersections are defined for each approach and for the overall intersection.

² Transportation Research Board, *Highway Capacity Manual*, Seventh Edition, Washington DC, 2022.

5.2 Key Intersection Operational Analysis

Calculations for the operational level of service at the key intersections for the study area are provided in **Appendix E**. The existing year analysis is based on the lane geometry and intersection control shown in **Figure 2**. Existing peak hour factors were utilized in the existing analysis year while the HCM urban standard of 0.92 was used for the 2035 and 2050 long-term horizon analysis years at intersections where this peak hour was not yet achieved in existing conditions. The existing heavy vehicle percentages obtained from the turning movement counts were also used in each horizon year. Synchro traffic analysis software was used to analyze the signalized and unsignalized key intersections for HCM level of service.

US-275 Southbound Ramps & Meigs Street (#1)

The US-275 Southbound Ramps & Meigs Street (#1) intersection is unsignalized and operates with stop control on the southbound off-ramp approach. The intersection movements operate acceptably at LOS C or better during both peak hours under existing conditions. With or without the addition of Lago Villaggio project traffic in the 2035 horizon, all movements are anticipated to continue operating acceptably with the existing intersection configuration and control. Therefore, no improvements or modifications are anticipated to be needed at this intersection based on the addition of project traffic through the 2035 horizon.

In the 2050 horizon with full buildout of the Elkhorn Valley development area, this intersection is expected to be signalized, with the eastbound Meigs Street approach being improved to provide a separate right turn lane while the westbound approach would provide dual left turn lanes. In this configuration, the intersection is expected to operate at an acceptable LOS D or better during both peak hours through the 2050 horizon with the addition of project traffic. However, if these projected traffic volumes are realized by 2050, it is recommended the City consider widening Meigs Street to the west of this intersection to two eastbound/westbound through lanes in each direction as vehicle queues would be long on the eastbound/westbound approaches with only one through lane in each direction. This improvement to two eastbound/westbound through lanes west of this intersection is largely due to the traffic volume generated by the Elkhorn Valley development, which is projected to add nearly 1,500 vehicles through this intersection during the afternoon peak hour at full buildout of that development. With this improvement, it is also suggested that a designated southbound left turn lane be added. **Table 3** provides the results of the LOS analysis conducted at this intersection.

Table 3 – US-275 SB Ramps & Meigs Street (#1) LOS Results

Scenario	AM Peak Hour		PM Peak Hour	
	Delay (sec/veh)	LOS	Delay (sec/veh)	LOS
2026 Existing				
Westbound Left	7.9	A	8.8	A
Southbound Approach	10.3	B	15.6	C
2035 Background				
Westbound Left	8.0	A	8.9	A
Southbound Approach	10.7	A	16.1	C
2035 Background Plus Project				
Westbound Left	8.4	A	9.4	A
Southbound Approach	14.3	B	29.1	D
2050 Background¹	20.8	C	40.6	D
2050 Background Plus Project¹	23.3	C	54.9	D
2050 Background Plus Project¹	20.3	C	39.4	D

¹ Signalized, EB Right Turn Lane, WB Dual Left Turn Lanes, SB Left Turn Lane, and Two EB/WB Through Lanes

US-275 Northbound Ramps & Meigs Street (#2)

The US-275 Northbound Ramps & Meigs Street (#2) intersection is unsignalized and operates with stop control on the northbound off-ramp approach. The intersection movements operate acceptably at LOS C or better during both peak hours under existing conditions. A northbound right turn lane is recommended to be constructed at this intersection with the addition of project traffic. With this improvement, the intersection movements are anticipated to continue operating acceptably through the 2035 horizon.

In 2050 with full buildout of the Elkhorn Valley development area, this intersection is expected to be signalized, with the eastbound Meigs Street approach being improved to provide a left turn lane and two through lanes while the westbound approach would provide two through lanes and a separate right turn lane. With the high volume of users making the northbound right turn movement, it is recommended that it operate with free movement as a channelized turn lane with an acceleration lane along eastbound Meigs Street constructed. In this configuration, the intersection is expected to operate at an acceptable LOS B or better during both peak hours through the 2050 horizon with the addition of project traffic. **Table 4** provides the results of the LOS analysis conducted at this intersection.

Table 4 – US-275 NB Ramps & Meigs Street (#2) LOS Results

Scenario	AM Peak Hour		PM Peak Hour	
	Delay (sec/veh)	LOS	Delay (sec/veh)	LOS
2026 Existing				
Northbound Approach	10.5	B	15.0	C
Eastbound Left	7.6	A	7.9	A
2035 Background				
Northbound Approach	11.1	B	18.5	C
Eastbound Left	7.6	A	8.1	A
2035 Background Plus Project¹				
Northbound Left/Through	13.4	B	25.2	D
Northbound Right	9.4	A	11.4	B
Eastbound Left	8.1	A	8.5	A
2050 Background²	16.3	B	20.1	C
2050 Background Plus Project²	8.4	A	11.6	B

¹Northbound right turn lane;

²Signalized, WB Right Turn Lane, Two EB Through Lanes, and NB Right Turn Lane operating with free movements

252nd Street & Meigs Street (#3)

The 252nd Street & Meigs Street (#3) 'T'-intersection is unsignalized and operates with stop control on the westbound 252nd Street approach. The intersection movements operate acceptably at LOS A during both peak hours under existing conditions. With or without the addition of Lago Villaggio project traffic, all movements at this intersection are anticipated to continue operating with acceptable level of service in 2035 with the existing intersection configuration and control. Therefore, no improvements or modifications are anticipated to be needed at this intersection based on the addition of project traffic in the 2035 horizon.

By 2050 with full buildout of the Elkhorn Valley development area, a new west leg is expected to be constructed at this intersection. A northbound right turn lane would be warranted at this intersection. Left turn lanes at intersections on higher speed roadways improve the safety of the intersection as left turning vehicles on the major street are removed from the major street through volume. As such, it is recommended that northbound and southbound left turn lanes be installed at this intersection. A westbound left turn lane is also recommended at this intersection. It is expected that this intersection will meet warrants for a traffic signal and may be needed operationally. A MUTCD Four-Hour Signal Warrant (Warrant 2) was performed for this intersection, shown in **Appendix G**. With these improvements and signalization, the intersection would be expected to operate at LOS B during both peak hours in the 2050 horizon. In addition, a single-lane roundabout was analyzed at this intersection. If the intersection is constructed as a roundabout, no turn lanes would be needed on the approaches to the intersection and the intersection would be expected to operate at LOS A during both peak hours in the 2050 horizon. **Table 5** provides the results of the LOS analysis conducted at this intersection.

Table 5 – 252nd Street & Meigs Street (#3) LOS Results

Scenario	AM Peak Hour		PM Peak Hour	
	Delay (sec/veh)	LOS	Delay (sec/veh)	LOS
2026 Existing				
Westbound Approach	8.8	A	9.2	A
Southbound Left	7.3	A	7.4	A
2035 Background				
Westbound Approach	8.8	A	9.3	A
Southbound Left	7.3	A	7.4	A
2035 Background Plus Project				
Westbound Approach	10.8	B	12.2	B
Southbound Left	7.5	A	7.9	A
2050 Background¹				
Eastbound Approach	7.4	A	7.7	A
Northbound Left	9.0	A	11.2	B
Southbound Left	10.5	B	22.0	C
Westbound Approach	7.5	A	8.3	A
2050 Background Plus Project – Signalized²	18.3	B	12.5	B
2050 Background Plus Project – Roundabout³	4.6	A	7.7	A

¹ New west leg, NB and SB left turn lanes, NB right turn lane and WB left turn lane;

² Note 1 + Signalized;

³ Single-lane roundabout

252nd Street & King Lake Road (#4)

The unsignalized intersection of 252nd Street and King Lake Road (#4) operates under stop control for the westbound approach. The eastbound approach was identified to be the farm driveway that is slightly misaligned to the intersection. The intersection movements operate acceptably at LOS A during both peak hours under existing conditions. With the existing lane configurations and the addition of project traffic, the movements at this intersection are expected to operate acceptably at LOS A during the peak hour throughout the 2050 horizon. Therefore, no improvements or modifications are anticipated to be needed at this intersection based on the addition of project traffic. **Table 6** provides the results of the level of service at this intersection.

Table 6 – 252nd Street & King Lake Road (#4) LOS Results

Scenario	AM Peak Hour		PM Peak Hour	
	Delay (sec/veh)	LOS	Delay (sec/veh)	LOS
2026 Existing				
Northbound Left	0.0	A	0.0	A
Eastbound Approach	0.0	A	0.0	A
Westbound Approach	8.5	A	8.9	A
Southbound Left	7.3	A	7.3	A
2035 Background				
Northbound Left	0.0	A	7.3	A
Eastbound Approach	0.0	A	9.4	B
Westbound Approach	8.5	A	8.9	A
Southbound Left	7.3	A	7.4	A
2035 Background Plus Project				
Northbound Left	0.0	A	7.3	A
Eastbound Approach	0.0	A	11.4	B
Westbound Approach	9.2	A	9.8	A
Southbound Left	7.3	A	7.6	A
2050 Background				
Northbound Left	0.0	A	7.4	A
Eastbound Approach	0.0	A	11.3	B
Westbound Approach	8.7	A	9.6	A
Southbound Left	7.4	A	7.6	A
2050 Background Plus Project				
Northbound Left	0.0	A	7.4	A
Eastbound Approach	0.0	A	14.3	B
Westbound Approach	9.6	A	11.0	B
Southbound Left	7.4	A	7.8	A

Project Accesses

With completion of the project, a full movement access along 252nd Street and two proposed full movement accesses along King Lake Road are proposed and were evaluated. It is recommended that R1-1 “STOP” signs be installed on the exiting approaches out of the development. **Table 7** provides the results of the level of service for the project accesses. As shown in the table, the project access intersections are anticipated to have all movements operating with acceptable LOS C or better during the peak hours in both the buildout year 2035 and the 2050 long-term horizons.

Table 7 – Project Access Level of Service Results

Intersection	2035 Total				2050 Total			
	AM Peak Hour		PM Peak Hour		AM Peak Hour		PM Peak Hour	
	Delay (sec/veh)	LOS	Delay (sec/veh)	LOS	Delay (sec/veh)	LOS	Delay (sec/veh)	LOS
252nd & Street C								
Northbound Left	-	-	-	-	7.4	A	7.7	A
Eastbound Approach	-	-	-	-	8.7	A	10.2	B
Westbound Approach	9.9	A	10.6	B	10.8	B	18.2	C
Southbound Left	7.4	A	7.7	A	7.5	A	8.4	A
Street G & King Lake								
Eastbound Left	7.4	A	7.4	A	7.4	A	7.5	A
Southbound Approach	8.8	A	8.7	A	8.9	A	8.8	A
Street H & King Lake								
Eastbound Left	7.3	A	7.3	A	7.3	A	7.4	A
Southbound Approach	8.5	A	8.5	A	8.6	A	8.7	A

5.3 Vehicle Queuing Analysis

A vehicle queuing analysis was conducted for the study area intersections. The queuing analysis was performed using Synchro presenting the results of the 95th percentile queue lengths. Results are shown in the following **Table 8** with calculations provided within the level of service operational sheets of **Appendix E** for unsignalized intersections and **Appendix G** for signalized intersections. Of note, vehicle queues were assumed to be a minimum of 25 feet (approximately one vehicle).

Table 8 – Turn Lane Queuing Analysis Results

Intersection Turn Lane	Existing Turn Lane Length	2035 Calculated Queue (AM/PM)	2035 Recommended Length	2050 Calculated Queue (AM/PM)	2050 Recommended Length
US-275 SB & Meigs (#1) – 1 EB/WB Through Lane					
Eastbound Through	C	-	C	377'/1037'	C
Eastbound Right	DNE	-	DNE	54'/187'	200'
Westbound Left	175'	25'/25'	175'	261'/389'	DL 400**
Westbound Through	C (675')	-	C (675')	192'/744'	C (675')
Southbound Through	C	25'/25'	C	215'/601'	C
US-275 SB & Meigs (#1) – Improved		Not Analyzed			
Eastbound Through	C			156'/421'	C
Eastbound Right	DNE			38'/172'	200'
Westbound Left	175'			247'/353'	DL 400**
Westbound Through	C (675')			115'/180'	C (675')
Southbound Left	DNE			196'/387'	400'
US-275 NB & Meigs (#2)					
Eastbound Left	175'	25'/25'	175'	25'/25'	175'
Westbound Right	DNE	-	DNE	52'/197'	200'
Northbound Right	DNE	25'/50'	150'	25'/25'	150' FREE
252nd & Meigs (#3)-Signalized		Not Analyzed			
Westbound Left	DNE			122'/144'	150'
Northbound Left	DNE			25'/25'	150'
Northbound Right	DNE			25'/31'	150'
Southbound Left	DNE			25'/25'	150'
252nd & C Street (#5)		Not Analyzed			
Northbound Left	DNE			25'/25'	150'
Southbound Left	DNE			25'/25'	150'

*=As Recommended by Elkhorn Valley; DNE = Does Not Exist; C = Continuous Turn Lane; DL=Dual Left Turn Lanes; **Red** Text = Turn Lane Constraint; **Blue** Text = Recommendation

All queues are anticipated to be accommodated within the existing or recommended turn lane lengths by the buildout horizon of 2035. By 2050, most queues are accommodated; however, the eastbound/westbound through vehicle queues at the US-275 Southbound Ramps & Meigs Street (#1) intersection are expected to be excessive if there is only one through lane in each direction at this intersection, largely due to the traffic generated by the Elkhorn Valley development.

As such, the intersection of US-275 Southbound Ramps and Meigs Street (#1) was analyzed for two different scenarios in the long-term 2050 horizon; the original scenario proposed included a single through lane for the eastbound and westbound approaches and an improved scenario with two through lanes for each of those approaches and a designated southbound left turn lane on the off-ramp. Dual westbound left turn lanes are proposed to be implemented for each scenario. The eastbound right turn lane will need to provide 200 feet of length to accommodate predicted queues. Without two westbound through lanes, the westbound through queue will extend past the intersection of US-275 Northbound Ramps and Meigs Street (#2), raising safety and operational concerns while the eastbound through queue could exceed 1,000 feet during the peak hours if there is only one eastbound through lane at the US-275 Southbound Ramps (#1) intersection. The additional through lanes alleviate that concern. It is recommended the City consider that this improvement be constructed along with other improvements in the study area as development occurs.

At the intersection of 252nd Street and Meigs Street (#3) and the intersection of 252nd Street and C Street (#5), northbound and southbound left turn lanes are recommended to improve intersection safety and operations. The recommended 150-foot turn lanes will contain the predicted queue for all of these turning movements.

5.4 Improvement Summary

Based on the results of the intersection operational and vehicle queuing analysis, the key intersection recommended improvements and control are shown for the 2035 buildout horizon and the 2050 long-term horizon in **Figure 10** and **Figure 11**, respectively.

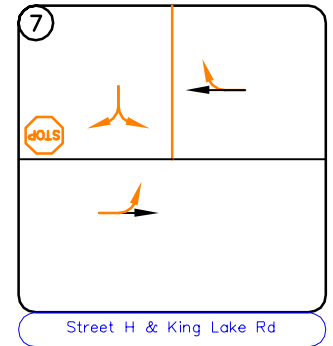
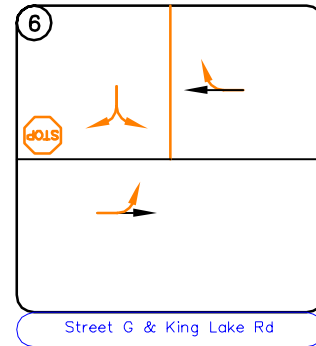
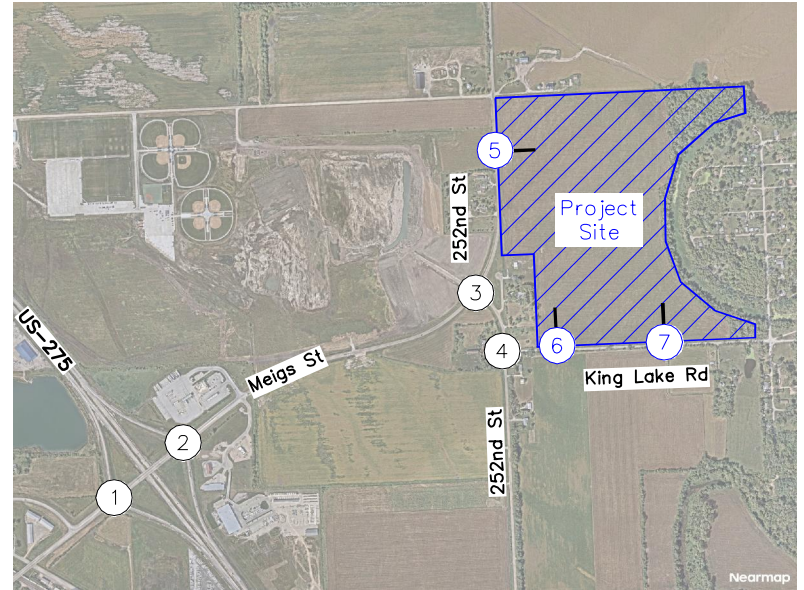
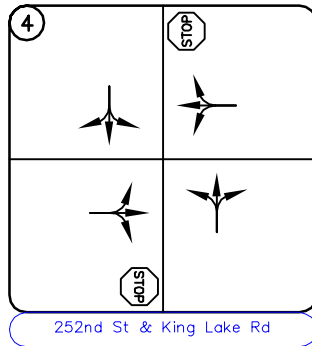
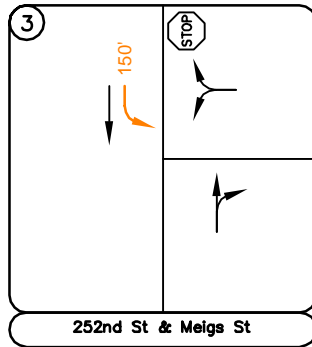
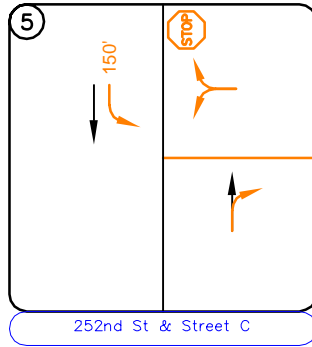
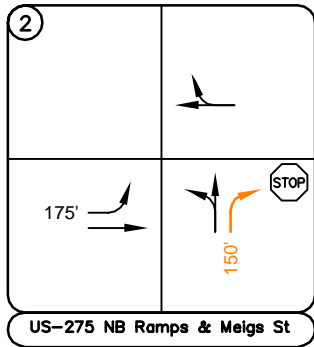
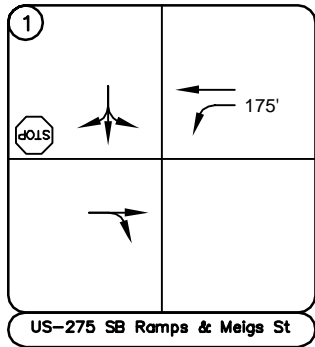


FIGURE 10
Lago Villaggio
Valley, Nebraska
2035 Recommended Geometry & Control

LEGEND

Study Area Key Intersection	Signalized Intersection
Project Access Intersection	Stop-Controlled Approach
Roundabout	Site-Specific Improvement
Yield-Controlled Approach	100' Turn Lane Length (feet)

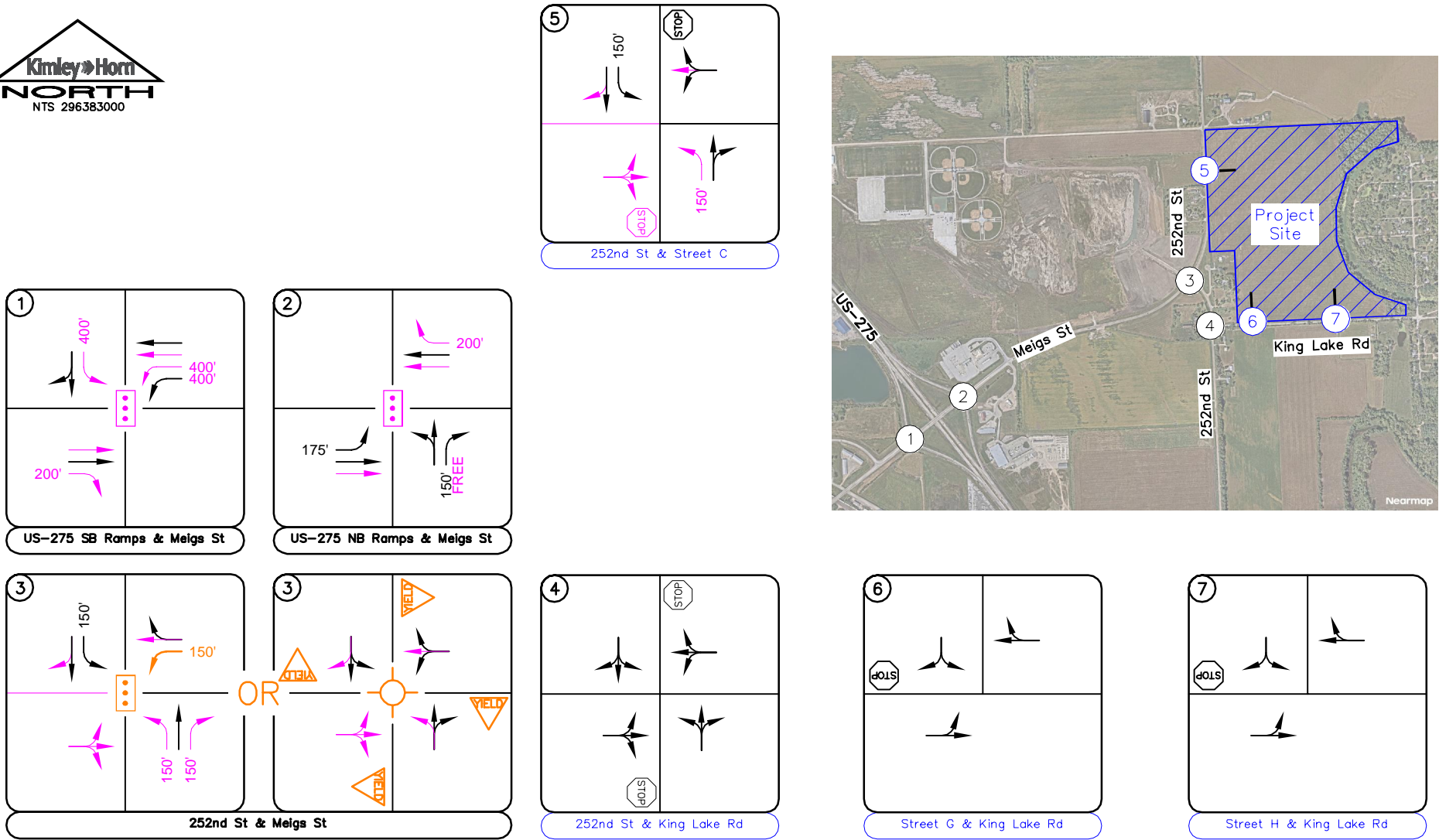


FIGURE 11
 Lago Villaggio
 Valley, Nebraska
 2050 Recommended Geometry & Control

LEGEND			
	Study Area Key Intersection		Signalized Intersection
	Project Access Intersection		Stop-Controlled Approach
	Roundabout		Site-Specific Improvement
	Yield-Controlled Approach		Improvement by Others
			100' Turn Lane Length (feet)

6.0 CONCLUSIONS AND RECOMMENDATIONS

Based on the analysis presented in this report, Kimley-Horn believes Lago Villaggio will be successfully incorporated into the existing and future roadway network. Analysis of the existing street network, the proposed project development, and expected traffic volumes resulted in the following recommendations:

2035 Near-Term Project Improvements

- With the completion of the project, one access is proposed on the east side of 252nd Street between Ida Street and Meigs Street. A southbound left turn lane is recommended at the 252nd Street access. A southbound left turn lane is also recommended at the 252nd Street & Meigs Street intersection.
- Two accesses are proposed along the north side of King Lake Road east of 252nd Street.
- All accesses are proposed to provide full turning movements and are recommended to have R1-1 “STOP” signs installed for control on the exiting approach. Each exiting approach is only anticipated to need a single lane for all exiting movements.
- A northbound right turn lane is recommended to be provided on the US-275 Northbound Off-Ramp approach to Meigs Street. While much of the traffic volume at this intersection is due to existing traffic volumes, the addition of project traffic to the northbound right turning movement also triggers this recommendation.

2050 Long-Term Improvements

With development of the Elkhorn Valley Youth Sports Complex, several surrounding area improvements should be constructed in association with that overall development. For purposes of this study, these additional recommended improvements are proposed in the 2050 horizon, but it is recommended the City, County, and NDOT (as appropriate) consider these improvements to be constructed alongside other roadway improvements in the study area planned as part of the Elkhorn Valley development to accommodate expected future growth.

- Both the US-275 Southbound (#1) and Northbound (#2) Ramps and Meigs Street intersections are anticipated to be signalized intersections. Likewise, Meigs Street may need an additional eastbound and westbound through lane to provide two through lanes in each direction through these two future signalized intersections.

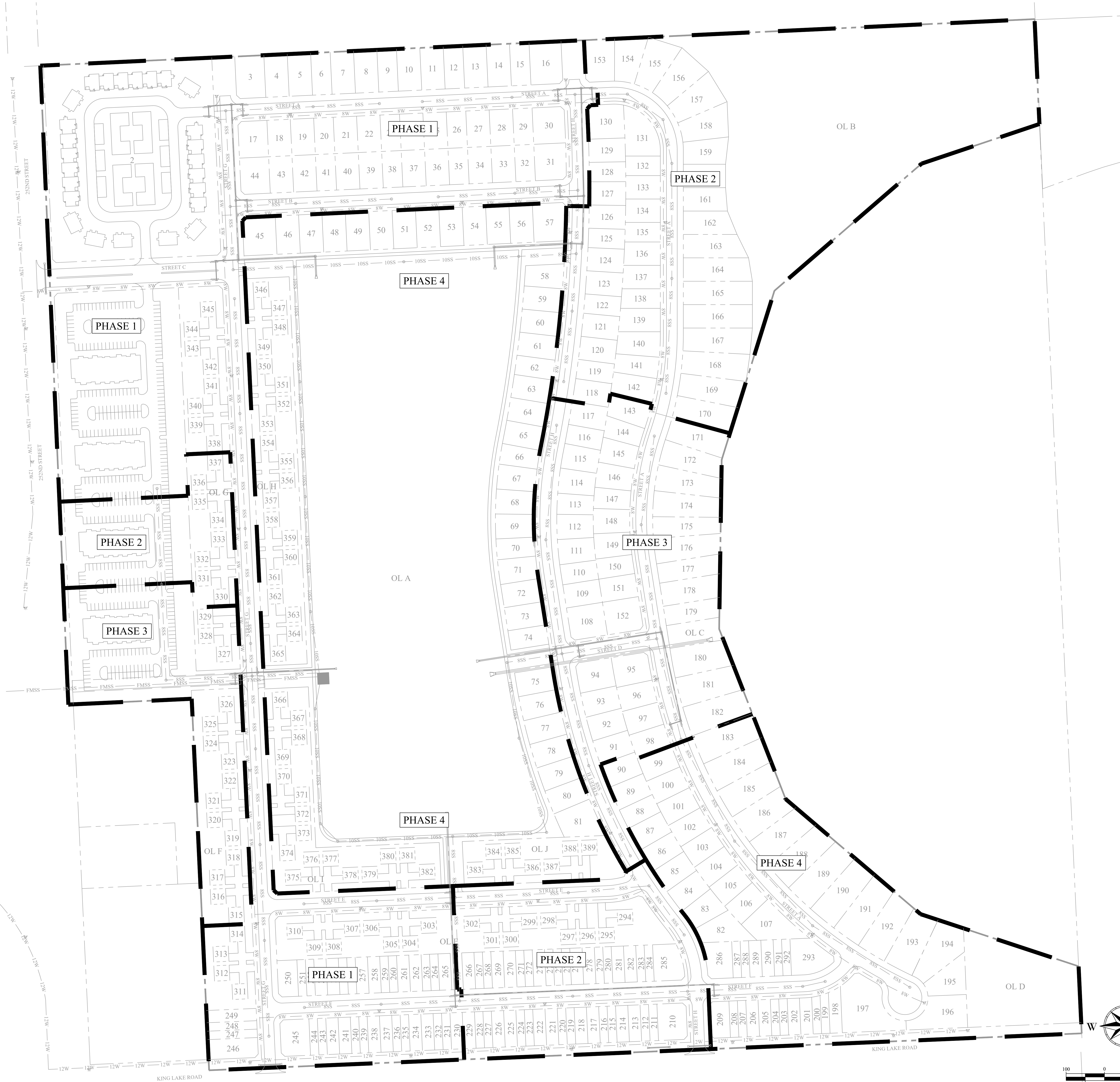
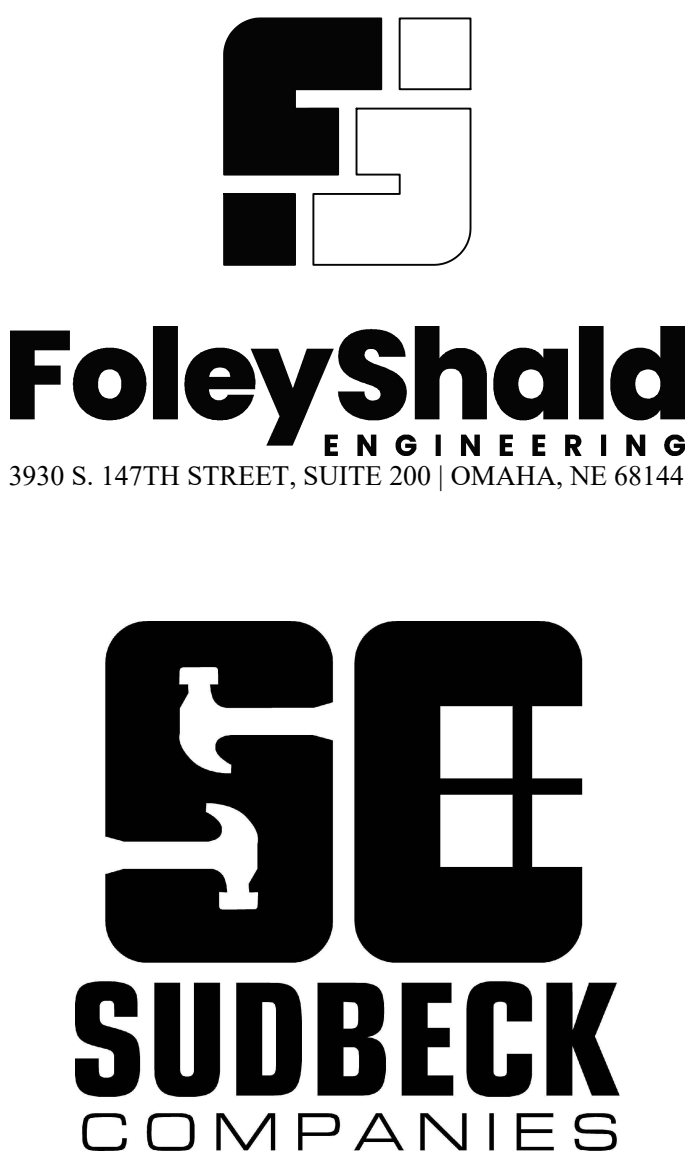
- At the US-275 and Meigs Street Southbound Ramps (#1) intersection, dual westbound left turn lanes, an eastbound right turn lane, and a separate southbound left turn lane on the off-ramp may be needed at this intersection.
- The intersection of US-275 Northbound Ramps and Meigs Street (#2) is recommended to provide a westbound right turn lane. It is recommended that the previously proposed northbound right lane to be constructed in 2035 be improved to operate with channelized and operate with free right turning movements with an acceleration lane along eastbound Meigs Street constructed due to the high expected traffic volume performing this right turn with Elkhorn Valley development.
- The intersection of 252nd Street and Meigs Street (#3) will have a fourth leg constructed as part of Elkhorn Valley development. It is expected this intersection will meet warrants for signalization and need either a traffic signal or a roundabout. It is recommended the City consider installing a traffic signal or a single-lane roundabout at this intersection. If signalized, northbound left, northbound right, southbound left, and westbound left turn lanes would be recommended.
- Similarly, the access intersection of 252nd Street and Street C (#5) is a proposed four-leg intersection with access to Elkhorn Valley provided on the west side of the roadway. This intersection is expected to operate acceptably with stop control on the minor street approaches, with the west leg providing access to Elkhorn Valley development and the east leg “Street C” serving as an access into the Lago Villaggio development. Northbound and southbound left turn lanes along 252nd Street are recommended at this intersection.

General Recommendations

- Any onsite or offsite improvements should be incorporated into the Civil Drawings and conform to standards of the City of Valley and Douglas County, as appropriate, as well as the Manual on Uniform Traffic Control Devices (MUTCD) – 11th Edition, 2023.

Appendix A: Conceptual Site Plan

VALLEY VILLAGE
PRELIMINARY PLAT
LOTS 1-389 AND OUTLOTS A-J



LEGEND

- EXISTING PROPERTY LINE
- PROPOSED PROPERTY LINE
- PHASE BOUNDARY
- PROPOSED WATER MAIN
- PROPOSED SANITARY SEWER
- PROPOSED FIRE HYDRANT
- PROPOSED STORM SEWER
- PROPOSED SANITARY MANHOLE
- PROPOSED CURB INLET

REVISIONS

NUMBER	DATE	DESCRIPTION

Valley Village
Valley, NE
FSE: CA-4197
FSE #: 104.001
November 14, 2025

Preliminary
NOT FOR CONSTRUCTION

Phasing Plan
Exhibit G



Appendix B: Intersection Count Sheets

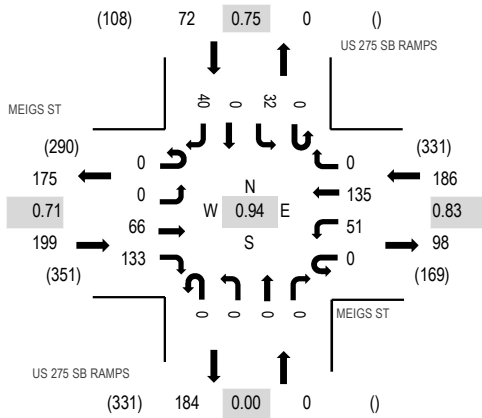
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Date: Thursday, January 22, 2026

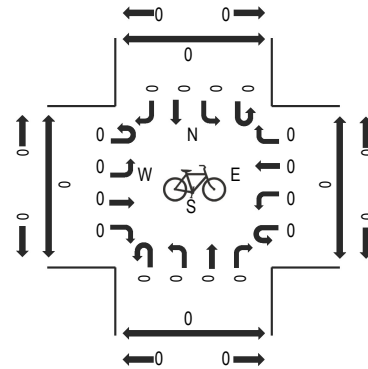
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Peak 15-Minutes: 07:30 AM - 07:45 AM

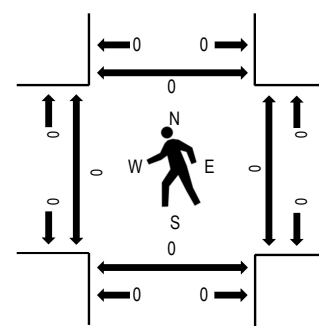
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	MEIGS ST Eastbound				MEIGS ST Westbound				US 275 SB RAMPS Northbound				US 275 SB RAMPS Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
7:00 AM	0	0	9	26	0	9	21	0	0	0	0	0	0	7	0	6	78	417	0	0	0	0
7:15 AM	0	0	10	32	0	20	24	0	0	0	0	0	0	3	0	9	98	457	0	0	0	0
7:30 AM	0	0	12	34	0	7	44	0	0	0	0	0	0	10	0	15	122	452	0	0	0	0
7:45 AM	0	0	14	24	0	17	39	0	0	0	0	0	0	9	0	16	119	421	0	0	0	0
8:00 AM	0	0	30	43	0	7	28	0	0	0	0	0	0	10	0	0	118	373	0	0	0	0
8:15 AM	0	0	19	30	0	10	26	0	0	0	0	0	0	6	0	2	93		0	0	0	0
8:30 AM	0	0	15	27	0	15	24	0	0	0	0	0	0	7	0	3	91		0	0	0	0
8:45 AM	0	0	6	20	0	10	30	0	0	0	0	0	0	2	0	3	71		0	0	0	0
Count Total	0	0	115	236	0	95	236	0	0	0	0	0	0	54	0	54	790		0	0	0	0
Peak Hour	0	0	66	133	0	51	135	0	0	0	0	0	0	32	0	40	457		0	0	0	0

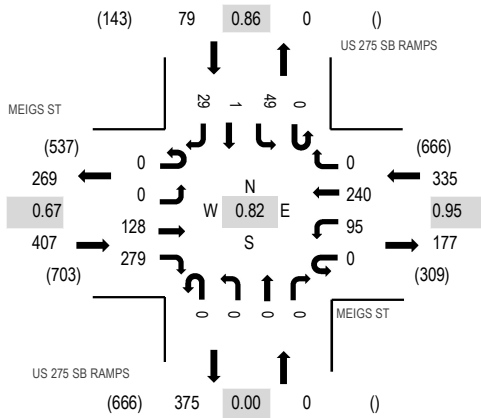
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Date: Thursday, January 22, 2026

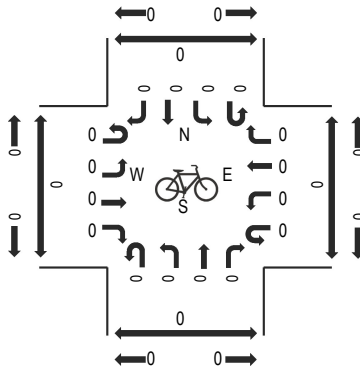
Peak Hour: 04:00 PM - 05:00 PM

Peak 15-Minutes: 04:00 PM - 04:15 PM

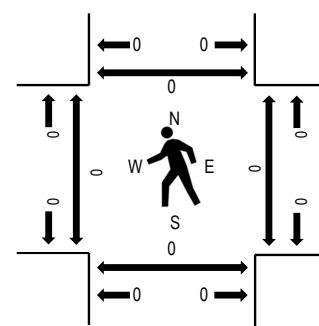
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	MEIGS ST Eastbound				MEIGS ST Westbound				US 275 SB RAMPS Northbound				US 275 SB RAMPS Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
4:00 PM	0	0	49	104	0	25	51	0	0	0	0	0	0	15	0	5	249	821	0	0	0	0
4:15 PM	0	0	34	63	0	21	57	0	0	0	0	0	0	13	0	10	198	744	0	0	0	0
4:30 PM	0	0	28	58	0	29	62	0	0	0	0	0	0	12	0	6	195	731	0	0	0	0
4:45 PM	0	0	17	54	0	20	70	0	0	0	0	0	0	9	1	8	179	710	0	0	0	0
5:00 PM	0	0	27	52	0	21	56	0	0	0	0	0	0	11	0	5	172	691	0	0	0	0
5:15 PM	0	0	24	56	0	21	67	0	0	0	0	0	0	12	0	5	185		0	0	0	0
5:30 PM	0	0	20	57	0	23	57	0	0	0	0	0	0	11	0	6	174		0	0	0	0
5:45 PM	0	0	19	41	0	19	67	0	0	0	0	0	0	8	1	5	160		0	0	0	0
Count Total	0	0	218	485	0	179	487	0	0	0	0	0	0	91	2	50	1,512		0	0	0	0
Peak Hour	0	0	128	279	0	95	240	0	0	0	0	0	0	49	1	29	821		0	0	0	0

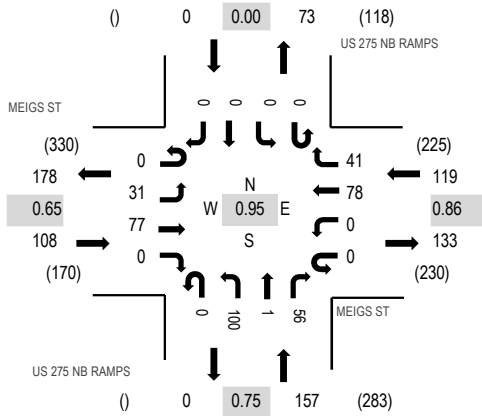
Location: 2 US 275 NB RAMPS & MEIGS ST AM

Date: Thursday, January 22, 2026

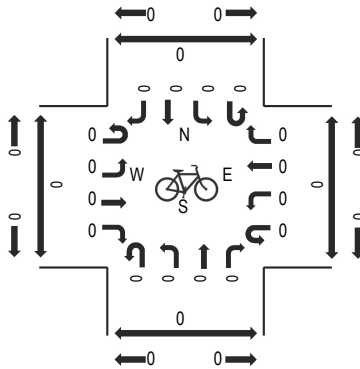
Peak Hour: 07:30 AM - 08:30 AM

Peak 15-Minutes: 07:30 AM - 07:45 AM

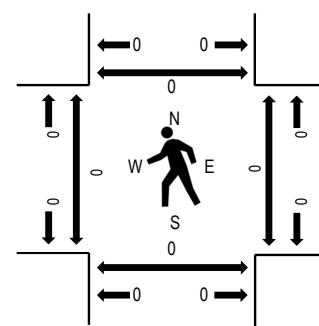
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	MEIGS ST Eastbound				MEIGS ST Westbound				US 275 NB RAMPS Northbound				US 275 NB RAMPS Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
7:00 AM	0	4	12	0	0	0	14	5	0	15	1	9	0	0	0	0	60	337	0	0	0	0
7:15 AM	0	6	9	0	0	0	23	6	0	21	0	11	0	0	0	0	76	377	0	0	0	0
7:30 AM	0	5	17	0	0	0	17	8	0	34	1	19	0	0	0	0	101	384	0	0	0	0
7:45 AM	0	2	19	0	0	0	28	9	0	28	0	14	0	0	0	0	100	370	0	0	0	0
8:00 AM	0	19	23	0	0	0	12	12	0	23	0	11	0	0	0	0	100	341	0	0	0	0
8:15 AM	0	5	18	0	0	0	21	12	0	15	0	12	0	0	0	0	83		0	0	0	0
8:30 AM	0	4	20	0	0	0	23	11	0	16	0	13	0	0	0	0	87		0	0	0	0
8:45 AM	0	1	6	0	0	0	17	7	0	23	0	17	0	0	0	0	71		0	0	0	0
Count Total	0	46	124	0	0	0	155	70	0	175	2	106	0	0	0	0	678		0	0	0	0
Peak Hour	0	31	77	0	0	0	78	41	0	100	1	56	0	0	0	0	384		0	0	0	0

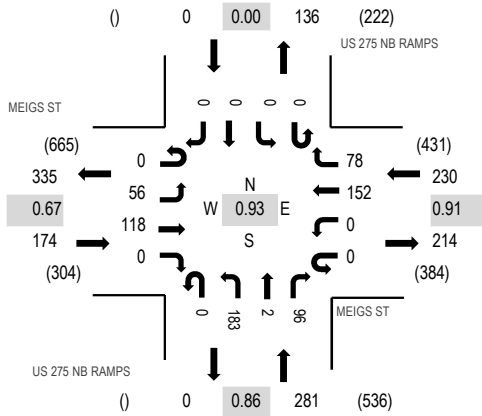
Location: 2 US 275 NB RAMPS & MEIGS ST PM

Date: Thursday, January 22, 2026

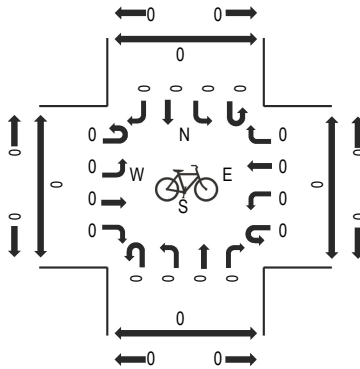
Peak Hour: 04:00 PM - 05:00 PM

Peak 15-Minutes: 04:00 PM - 04:15 PM

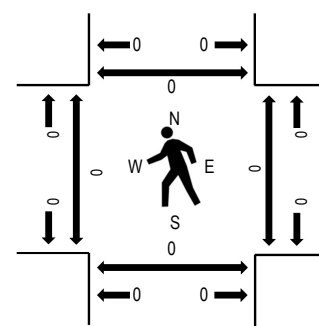
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians

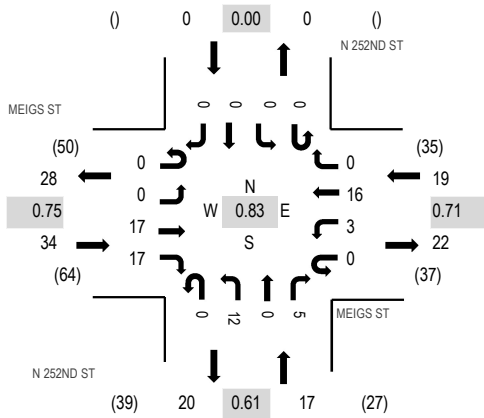


Note: Total study counts contained in parentheses.

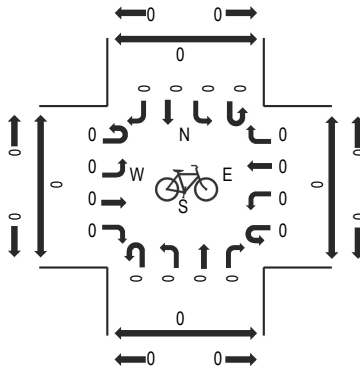
Traffic Counts - Motorized Vehicles

Interval Start Time	MEIGS ST Eastbound				MEIGS ST Westbound				US 275 NB RAMPS Northbound				US 275 NB RAMPS Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
4:00 PM	0	30	35	0	0	0	40	22	0	36	1	21	0	0	0	0	185	685	0	0	0	0
4:15 PM	0	15	30	0	0	0	39	19	0	38	0	27	0	0	0	0	168	643	0	0	0	0
4:30 PM	0	8	30	0	0	0	44	19	0	51	0	25	0	0	0	0	177	625	0	0	0	0
4:45 PM	0	3	23	0	0	0	29	18	0	58	1	23	0	0	0	0	155	597	0	0	0	0
5:00 PM	0	11	27	0	0	0	36	10	0	42	0	17	0	0	0	0	143	586	0	0	0	0
5:15 PM	0	5	30	0	0	0	42	13	0	44	0	16	0	0	0	0	150		0	0	0	0
5:30 PM	0	5	26	0	0	0	36	18	0	46	0	18	0	0	0	0	149		0	0	0	0
5:45 PM	0	8	18	0	0	0	30	16	0	54	0	18	0	0	0	0	144		0	0	0	0
Count Total	0	85	219	0	0	0	296	135	0	369	2	165	0	0	0	0	1,271		0	0	0	0
Peak Hour	0	56	118	0	0	0	152	78	0	183	2	96	0	0	0	0	685		0	0	0	0

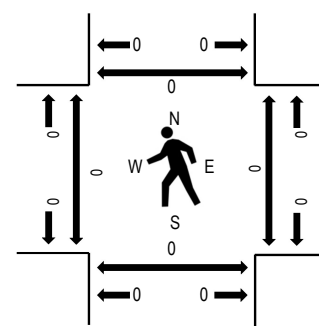
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians

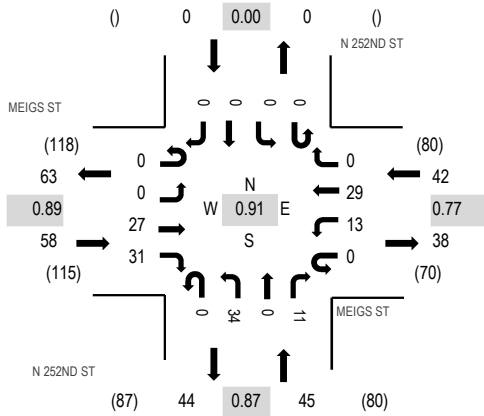


Note: Total study counts contained in parentheses.

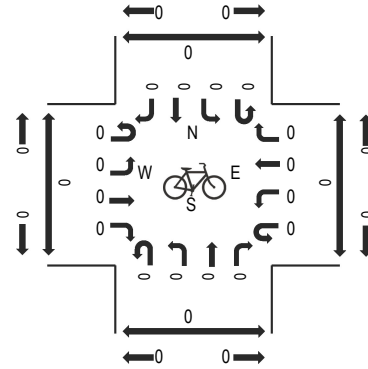
Traffic Counts - Motorized Vehicles

Interval Start Time	MEIGS ST Eastbound				MEIGS ST Westbound				N 252ND ST Northbound				N 252ND ST Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
7:00 AM	0	0	3	3	0	0	3	0	0	0	0	0	0	0	0	0	9	58	0	0	0	0
7:15 AM	0	0	4	4	0	1	2	0	0	4	0	1	0	0	0	0	16	66	0	0	0	0
7:30 AM	0	0	5	1	0	0	3	0	0	2	0	1	0	0	0	0	12	66	0	0	0	0
7:45 AM	0	0	3	5	0	1	5	0	0	6	0	1	0	0	0	0	21	70	0	0	0	0
8:00 AM	0	0	6	6	0	0	4	0	0	1	0	0	0	0	0	0	17	68	0	0	0	0
8:15 AM	0	0	5	3	0	1	3	0	0	3	0	1	0	0	0	0	16		0	0	0	0
8:30 AM	0	0	3	3	0	1	4	0	0	2	0	3	0	0	0	0	16		0	0	0	0
8:45 AM	0	0	0	10	0	0	7	0	0	1	0	1	0	0	0	0	19		0	0	0	0
Count Total	0	0	29	35	0	4	31	0	0	19	0	8	0	0	0	0	126		0	0	0	0
Peak Hour	0	0	17	17	0	3	16	0	0	12	0	5	0	0	0	0	70		0	0	0	0

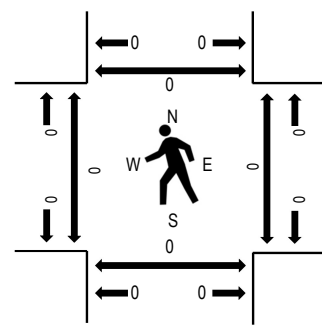
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	MEIGS ST Eastbound				MEIGS ST Westbound				N 252ND ST Northbound				N 252ND ST Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
4:00 PM	0	0	8	7	0	1	8	0	0	6	0	2	0	0	0	0	32	142	0	0	0	0
4:15 PM	0	0	6	11	0	0	9	0	0	6	0	4	0	0	0	0	36	143	0	0	0	0
4:30 PM	0	0	7	12	0	3	7	0	0	7	0	4	0	0	0	0	40	145	0	0	0	0
4:45 PM	0	0	9	8	0	3	6	0	0	6	0	2	0	0	0	0	34	132	0	0	0	0
5:00 PM	0	0	6	5	0	1	8	0	0	9	0	4	0	0	0	0	33	133	0	0	0	0
5:15 PM	0	0	5	6	0	6	8	0	0	12	0	1	0	0	0	0	38		0	0	0	0
5:30 PM	0	0	4	6	0	1	8	0	0	8	0	0	0	0	0	0	27		0	0	0	0
5:45 PM	0	0	6	9	0	8	3	0	0	7	0	2	0	0	0	0	35		0	0	0	0
Count Total	0	0	51	64	0	23	57	0	0	61	0	19	0	0	0	0	275		0	0	0	0
Peak Hour	0	0	27	31	0	13	29	0	0	34	0	11	0	0	0	0	145		0	0	0	0

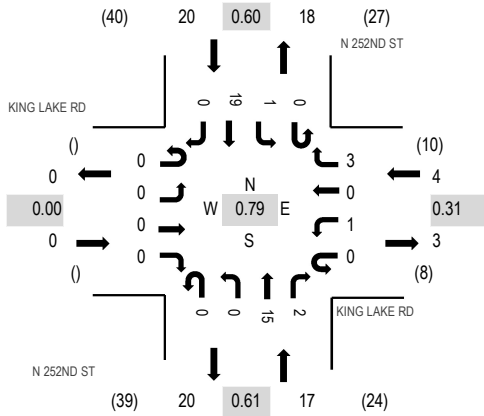
Location: 4 N 252ND ST & KING LAKE RD AM

Date: Thursday, January 22, 2026

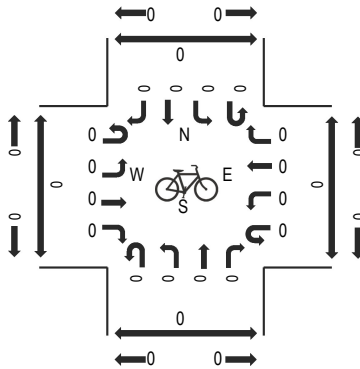
Peak Hour: 07:45 AM - 08:45 AM

Peak 15-Minutes: 07:45 AM - 08:00 AM

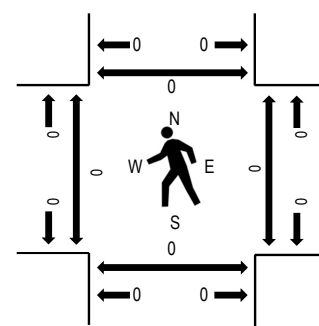
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	KING LAKE RD Eastbound				KING LAKE RD Westbound				N 252ND ST Northbound				N 252ND ST Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
7:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	1	3	0	4	33	0	0	0	0
7:15 AM	0	0	0	0	0	2	0	2	0	0	3	0	0	0	5	0	12	38	0	0	0	0
7:30 AM	0	0	0	0	0	0	0	1	0	0	2	0	0	0	1	0	4	35	0	0	0	0
7:45 AM	0	0	0	0	0	0	0	0	0	0	7	0	0	0	6	0	13	41	0	0	0	0
8:00 AM	0	0	0	0	0	0	0	0	0	0	1	2	0	0	6	0	9	41	0	0	0	0
8:15 AM	0	0	0	0	0	1	0	1	0	0	3	0	0	0	4	0	9		0	0	0	0
8:30 AM	0	0	0	0	0	0	0	2	0	0	4	0	0	1	3	0	10		0	0	0	0
8:45 AM	0	0	0	0	0	0	0	1	0	0	0	2	0	2	8	0	13		0	0	0	0
Count Total	0	0	0	0	0	3	0	7	0	0	20	4	0	4	36	0	74		0	0	0	0
Peak Hour	0	0	0	0	0	1	0	3	0	0	15	2	0	1	19	0	41		0	0	0	0

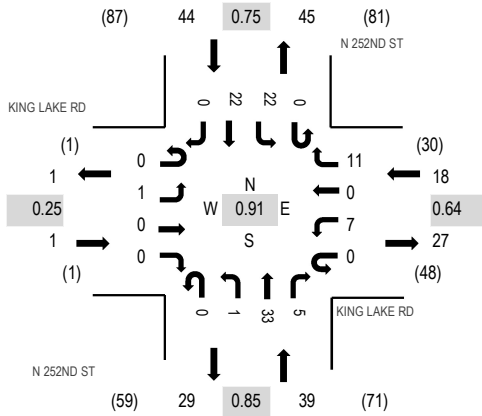
Location: 4 N 252ND ST & KING LAKE RD PM

Date: Thursday, January 22, 2026

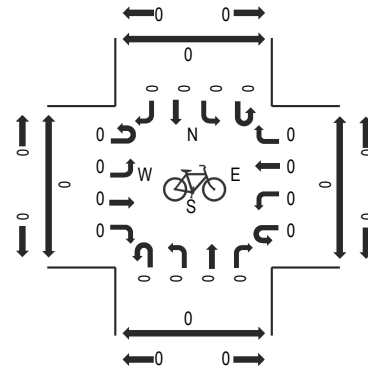
Peak Hour: 04:30 PM - 05:30 PM

Peak 15-Minutes: 04:30 PM - 04:45 PM

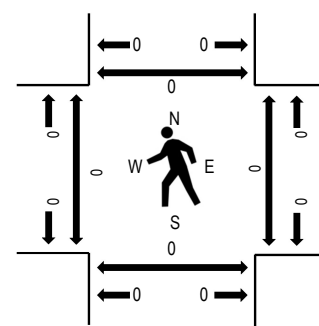
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	KING LAKE RD Eastbound				KING LAKE RD Westbound				N 252ND ST Northbound				N 252ND ST Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
4:00 PM	0	0	0	0	0	1	0	2	0	0	5	0	0	4	4	0	16	92	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	3	0	0	9	1	0	3	8	0	24	98	0	0	0	0
4:30 PM	0	0	0	0	0	1	0	3	0	0	7	2	0	7	8	0	28	102	0	0	0	0
4:45 PM	0	0	0	0	0	2	0	1	0	1	7	2	0	6	5	0	24	94	0	0	0	0
5:00 PM	0	0	0	0	0	1	0	3	0	0	11	1	0	4	2	0	22	97	0	0	0	0
5:15 PM	0	1	0	0	0	3	0	4	0	0	8	0	0	5	7	0	28		0	0	0	0
5:30 PM	0	0	0	0	0	2	0	1	0	0	7	3	0	2	5	0	20		0	0	0	0
5:45 PM	0	0	0	0	0	1	0	2	0	0	7	0	0	8	9	0	27		0	0	0	0
Count Total	0	1	0	0	0	11	0	19	0	1	61	9	0	39	48	0	189		0	0	0	0
Peak Hour	0	1	0	0	0	7	0	11	0	1	33	5	0	22	22	0	102		0	0	0	0

Appendix C: Future Traffic Projections

MAPA Traffic Projections:

Lago Villaggio

Location	Growth Factor	Annual Growth
US-275 from N-36 to W Maple	1.80	1.98%
US-275 from W Maple to W Center	1.50	1.36%
W Maple from US-275 to 216th St	1.53	1.44%
Total	1.61	1.60%

Appendix D: Trip Generation Worksheet

Lago Villaggio Daily Trip Generation Calculations

TRIP GENERATION CHARACTERISTICS					DIRECTIONAL DISTRIBUTION		BASELINE TRIPS			
Land Use + Code	Source	Scale	ITE Unit	Equation/Rate	Entering %	Exiting %	In	Out	Total	
1	(210) Single-Family Detached Housing	ITE 12th Ed	294	DU	$T = 8.07(X) + 265.45$	50%	50%	1,319	1,319	2,638
2	(215) Single-Family Attached Housing	ITE 12th Ed	96	DU	$T = 6.57(X)$	50%	50%	316	315	631
3	(220) Multifamily Housing (Low-Rise)	ITE 12th Ed	230	DU	$T = 5.63(X) + 120.45$	50%	50%	708	707	1,415
Total:								2,343	2,341	4,684

Lago Villaggio AM Peak Hour Trip Generation Calculations

TRIP GENERATION CHARACTERISTICS					DIRECTIONAL DISTRIBUTION		BASELINE TRIPS			
Land Use + Code	Source	Scale	ITE Unit	Equation/Rate	Entering %	Exiting %	In	Out	Total	
1	(210) Single-Family Detached Housing	ITE 12th Ed	294	DU	$T = 0.67(X) + 5.59$	27%	73%	55	148	203
2	(215) Single-Family Attached Housing	ITE 12th Ed	96	DU	$T = 0.59(X) - 15.25$	25%	75%	10	31	41
3	(220) Multifamily Housing (Low-Rise)	ITE 12th Ed	230	DU	$T = 0.35(X) + 12.93$	24%	76%	22	71	93
Total:								87	250	337

Lago Villaggio PM Peak Hour Trip Generation Calculations

TRIP GENERATION CHARACTERISTICS					DIRECTIONAL DISTRIBUTION		BASELINE TRIPS			
Land Use + Code	Source	Scale	ITE Unit	Equation/Rate	Entering %	Exiting %	In	Out	Total	
1	(210) Single-Family Detached Housing	ITE 12th Ed	294	DU	$\ln(T) = 0.92 \ln(X) + 0.33$	62%	38%	161	99	260
2	(215) Single-Family Attached Housing	ITE 12th Ed	96	DU	$T = 0.57(X) - 7.84$	57%	43%	27	20	47
3	(220) Multifamily Housing (Low-Rise)	ITE 12th Ed	230	DU	$T = 0.48(X) + 7.35$	62%	38%	73	45	118
Total:								261	164	425

Appendix E: Intersection Analysis Worksheets

Intersection												
Int Delay, s/veh	2.5											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↔		↔	↔						↔	
Traffic Vol, veh/h	0	66	133	51	135	0	0	0	0	32	0	40
Future Vol, veh/h	0	66	133	51	135	0	0	0	0	32	0	40
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	175	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	94	94	94	94	94	94	94	94	94	94	94	94
Heavy Vehicles, %	3	3	3	12	12	12	2	2	2	7	7	7
Mvmt Flow	0	70	141	54	144	0	0	0	0	34	0	43

Major/Minor	Major1			Major2			Minor2					
Conflicting Flow All	-	0	0	212	0	0				322	464	144
Stage 1	-	-	-	-	-	-				252	252	-
Stage 2	-	-	-	-	-	-				70	212	-
Critical Hdwy	-	-	-	4.22	-	-				6.47	6.57	6.27
Critical Hdwy Stg 1	-	-	-	-	-	-				5.47	5.57	-
Critical Hdwy Stg 2	-	-	-	-	-	-				5.47	5.57	-
Follow-up Hdwy	-	-	-	2.308	-	-				3.563	4.063	3.363
Pot Cap-1 Maneuver	0	-	-	1301	-	0				661	488	891
Stage 1	0	-	-	-	-	0				778	689	-
Stage 2	0	-	-	-	-	0				940	718	-
Platoon blocked, %		-	-	-								
Mov Cap-1 Maneuver	-	-	-	1301	-	-				634	0	891
Mov Cap-2 Maneuver	-	-	-	-	-	-				634	0	-
Stage 1	-	-	-	-	-	-				778	0	-
Stage 2	-	-	-	-	-	-				901	0	-

Approach	EB	WB	SB
HCM Ctrl Dly, s/v	0	2.16	10.31
HCM LOS			B

Minor Lane/Major Mvmt	EBT	EBR	WBL	WBT	SBLn1
Capacity (veh/h)	-	-	1301	-	755
HCM Lane V/C Ratio	-	-	0.042	-	0.101
HCM Ctrl Dly (s/v)	-	-	7.9	-	10.3
HCM Lane LOS	-	-	A	-	B
HCM 95th %tile Q(veh)	-	-	0.1	-	0.3

Intersection												
Int Delay, s/veh	2.5											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↔		↔	↔						↔	
Traffic Vol, veh/h	0	128	279	95	240	0	0	0	0	49	0	29
Future Vol, veh/h	0	128	279	95	240	0	0	0	0	49	0	29
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	175	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	82	82	82	82	82	82	82	82	82	82	82	82
Heavy Vehicles, %	2	2	2	3	3	3	2	2	2	15	15	15
Mvmt Flow	0	156	340	116	293	0	0	0	0	60	0	35

Major/Minor	Major1			Major2			Minor2			
Conflicting Flow All	-	0	0	496	0	0		680	1021	293
Stage 1	-	-	-	-	-	-		524	524	-
Stage 2	-	-	-	-	-	-		156	496	-
Critical Hdwy	-	-	-	4.13	-	-		6.55	6.65	6.35
Critical Hdwy Stg 1	-	-	-	-	-	-		5.55	5.65	-
Critical Hdwy Stg 2	-	-	-	-	-	-		5.55	5.65	-
Follow-up Hdwy	-	-	-	2.227	-	-		3.635	4.135	3.435
Pot Cap-1 Maneuver	0	-	-	1062	-	0		397	224	717
Stage 1	0	-	-	-	-	0		568	509	-
Stage 2	0	-	-	-	-	0		842	524	-
Platoon blocked, %		-	-	-						
Mov Cap-1 Maneuver	-	-	-	1062	-	-		354	0	717
Mov Cap-2 Maneuver	-	-	-	-	-	-		354	0	-
Stage 1	-	-	-	-	-	-		568	0	-
Stage 2	-	-	-	-	-	-		750	0	-

Approach	EB	WB	SB
HCM Ctrl Dly, s/v	0	2.5	15.55
HCM LOS			C

Minor Lane/Major Mvmt	EBT	EBR	WBL	WBT	SBLn1
Capacity (veh/h)	-	-	1062	-	436
HCM Lane V/C Ratio	-	-	0.109	-	0.218
HCM Ctrl Dly (s/v)	-	-	8.8	-	15.6
HCM Lane LOS	-	-	A	-	C
HCM 95th %tile Q(veh)	-	-	0.4	-	0.8

Intersection												
Int Delay, s/veh	2.6											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↔		↔	↔						↔	
Traffic Vol, veh/h	0	76	153	59	156	0	0	0	0	37	0	46
Future Vol, veh/h	0	76	153	59	156	0	0	0	0	37	0	46
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	175	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	94	94	94	94	94	94	94	94	94	94	94	94
Heavy Vehicles, %	3	3	3	12	12	12	2	2	2	7	7	7
Mvmt Flow	0	81	163	63	166	0	0	0	0	39	0	49

Major/Minor	Major1			Major2			Minor2			
Conflicting Flow All	-	0	0	244	0	0		372	535	166
Stage 1	-	-	-	-	-	-		291	291	-
Stage 2	-	-	-	-	-	-		81	244	-
Critical Hdwy	-	-	-	4.22	-	-		6.47	6.57	6.27
Critical Hdwy Stg 1	-	-	-	-	-	-		5.47	5.57	-
Critical Hdwy Stg 2	-	-	-	-	-	-		5.47	5.57	-
Follow-up Hdwy	-	-	-	2.308	-	-		3.563	4.063	3.363
Pot Cap-1 Maneuver	0	-	-	1266	-	0		619	445	866
Stage 1	0	-	-	-	-	0		747	662	-
Stage 2	0	-	-	-	-	0		930	695	-
Platoon blocked, %	-	-	-	-	-	-		-	-	-
Mov Cap-1 Maneuver	-	-	-	1266	-	-		588	0	866
Mov Cap-2 Maneuver	-	-	-	-	-	-		588	0	-
Stage 1	-	-	-	-	-	-		747	0	-
Stage 2	-	-	-	-	-	-		884	0	-

Approach	EB	WB	SB
HCM Ctrl Dly, s/v	0	2.19	10.74
HCM LOS			B

Minor Lane/Major Mvmt	EBT	EBR	WBL	WBT	SBLn1
Capacity (veh/h)	-	-	1266	-	715
HCM Lane V/C Ratio	-	-	0.05	-	0.123
HCM Ctrl Dly (s/v)	-	-	8	-	10.7
HCM Lane LOS	-	-	A	-	B
HCM 95th %tile Q(veh)	-	-	0.2	-	0.4

Intersection												
Int Delay, s/veh	2.6											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↔		↔	↔						↔	
Traffic Vol, veh/h	0	148	322	110	277	0	0	0	0	57	0	33
Future Vol, veh/h	0	148	322	110	277	0	0	0	0	57	0	33
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	175	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	3	3	3	2	2	2	15	15	15
Mvmt Flow	0	161	350	120	301	0	0	0	0	62	0	36

Major/Minor	Major1			Major2			Minor2			
Conflicting Flow All	-	0	0	511	0	0		701	1051	301
Stage 1	-	-	-	-	-	-		540	540	-
Stage 2	-	-	-	-	-	-		161	511	-
Critical Hdwy	-	-	-	4.13	-	-		6.55	6.65	6.35
Critical Hdwy Stg 1	-	-	-	-	-	-		5.55	5.65	-
Critical Hdwy Stg 2	-	-	-	-	-	-		5.55	5.65	-
Follow-up Hdwy	-	-	-	2.227	-	-		3.635	4.135	3.435
Pot Cap-1 Maneuver	0	-	-	1049	-	0		386	215	709
Stage 1	0	-	-	-	-	0		559	501	-
Stage 2	0	-	-	-	-	0		837	516	-
Platoon blocked, %	-	-	-	-	-	-		-	-	-
Mov Cap-1 Maneuver	-	-	-	1049	-	-		342	0	709
Mov Cap-2 Maneuver	-	-	-	-	-	-		342	0	-
Stage 1	-	-	-	-	-	-		559	0	-
Stage 2	-	-	-	-	-	-		742	0	-

Approach	EB	WB	SB
HCM Ctrl Dly, s/v	0	2.52	16.08
HCM LOS			C

Minor Lane/Major Mvmt	EBT	EBR	WBL	WBT	SBLn1
Capacity (veh/h)	-	-	1049	-	422
HCM Lane V/C Ratio	-	-	0.114	-	0.232
HCM Ctrl Dly (s/v)	-	-	8.9	-	16.1
HCM Lane LOS	-	-	A	-	C
HCM 95th %tile Q(veh)	-	-	0.4	-	0.9

Intersection												
Int Delay, s/veh	4											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↻		↻	↻						↻	
Traffic Vol, veh/h	0	89	153	172	194	0	0	0	0	50	0	46
Future Vol, veh/h	0	89	153	172	194	0	0	0	0	50	0	46
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	175	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	94	94	94	94	94	94	94	94	94	94	94	94
Heavy Vehicles, %	3	3	3	12	12	12	2	2	2	7	7	7
Mvmt Flow	0	95	163	183	206	0	0	0	0	53	0	49

Major/Minor	Major1			Major2			Minor2			
Conflicting Flow All	-	0	0	257	0	0		667	830	206
Stage 1	-	-	-	-	-	-		572	572	-
Stage 2	-	-	-	-	-	-		95	257	-
Critical Hdwy	-	-	-	4.22	-	-		6.47	6.57	6.27
Critical Hdwy Stg 1	-	-	-	-	-	-		5.47	5.57	-
Critical Hdwy Stg 2	-	-	-	-	-	-		5.47	5.57	-
Follow-up Hdwy	-	-	-	2.308	-	-		3.563	4.063	3.363
Pot Cap-1 Maneuver	0	-	-	1251	-	0		416	300	822
Stage 1	0	-	-	-	-	0		555	496	-
Stage 2	0	-	-	-	-	0		917	686	-
Platoon blocked, %		-	-	-	-	-				
Mov Cap-1 Maneuver	-	-	-	1251	-	-		355	0	822
Mov Cap-2 Maneuver	-	-	-	-	-	-		355	0	-
Stage 1	-	-	-	-	-	-		555	0	-
Stage 2	-	-	-	-	-	-		783	0	-

Approach	EB	WB	SB
HCM Ctrl Dly, s/v	0	3.93	14.32
HCM LOS			B

Minor Lane/Major Mvmt	EBT	EBR	WBL	WBT	SBLn1
Capacity (veh/h)	-	-	1251	-	488
HCM Lane V/C Ratio	-	-	0.146	-	0.209
HCM Ctrl Dly (s/v)	-	-	8.4	-	14.3
HCM Lane LOS	-	-	A	-	B
HCM 95th %tile Q(veh)	-	-	0.5	-	0.8

Intersection												
Int Delay, s/veh	4.9											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↔		↔	↔						↔	
Traffic Vol, veh/h	0	187	322	184	302	0	0	0	0	96	0	33
Future Vol, veh/h	0	187	322	184	302	0	0	0	0	96	0	33
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	175	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	3	3	3	2	2	2	2	2	2
Mvmt Flow	0	203	350	200	328	0	0	0	0	104	0	36

Major/Minor	Major1			Major2			Minor2			
Conflicting Flow All	-	0	0	553	0	0		932	1282	328
Stage 1	-	-	-	-	-	-		728	728	-
Stage 2	-	-	-	-	-	-		203	553	-
Critical Hdwy	-	-	-	4.13	-	-		6.42	6.52	6.22
Critical Hdwy Stg 1	-	-	-	-	-	-		5.42	5.52	-
Critical Hdwy Stg 2	-	-	-	-	-	-		5.42	5.52	-
Follow-up Hdwy	-	-	-	2.227	-	-		3.518	4.018	3.318
Pot Cap-1 Maneuver	0	-	-	1012	-	0		296	165	713
Stage 1	0	-	-	-	-	0		478	428	-
Stage 2	0	-	-	-	-	0		831	514	-
Platoon blocked, %		-	-	-	-	-				
Mov Cap-1 Maneuver	-	-	-	1012	-	-		238	0	713
Mov Cap-2 Maneuver	-	-	-	-	-	-		238	0	-
Stage 1	-	-	-	-	-	-		478	0	-
Stage 2	-	-	-	-	-	-		667	0	-

Approach	EB	WB	SB
HCM Ctrl Dly, s/v	0	3.57	29.06
HCM LOS			D

Minor Lane/Major Mvmt	EBT	EBR	WBL	WBT	SBLn1
Capacity (veh/h)	-	-	1012	-	286
HCM Lane V/C Ratio	-	-	0.198	-	0.49
HCM Ctrl Dly (s/v)	-	-	9.4	-	29.1
HCM Lane LOS	-	-	A	-	D
HCM 95th %tile Q(veh)	-	-	0.7	-	2.5

Timings
1: US-275 Southbound Ramps & Meigs St

2050 Background AM
02/12/2026



Lane Group	EBT	EBR	WBL	WBT	SBT
Lane Configurations	↑	↗	↖	↑	↕
Traffic Volume (vph)	424	146	299	556	0
Future Volume (vph)	424	146	299	556	0
Turn Type	NA	Perm	Prot	NA	NA
Protected Phases	2		1	6	4
Permitted Phases		2			
Detector Phase	2	2	1	6	4
Switch Phase					
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0
Minimum Split (s)	22.5	22.5	9.5	22.5	22.5
Total Split (s)	57.0	57.0	32.0	89.0	31.0
Total Split (%)	47.5%	47.5%	26.7%	74.2%	25.8%
Yellow Time (s)	3.5	3.5	3.5	3.5	3.5
All-Red Time (s)	1.0	1.0	1.0	1.0	1.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	4.5	4.5	4.5	4.5	4.5
Lead/Lag	Lag	Lag	Lead		
Lead-Lag Optimize?	Yes	Yes	Yes		
Recall Mode	C-Max	C-Max	None	C-Min	None

Intersection Summary

Cycle Length: 120
 Actuated Cycle Length: 120
 Offset: 0 (0%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 60
 Control Type: Actuated-Coordinated

Splits and Phases: 1: US-275 Southbound Ramps & Meigs St



HCM 7th Signalized Intersection Summary
 1: US-275 Southbound Ramps & Meigs St

2050 Background AM
 02/12/2026



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↑	↑	↑↑	↑						↑	
Traffic Volume (veh/h)	0	424	146	299	556	0	0	0	0	140	0	59
Future Volume (veh/h)	0	424	146	299	556	0	0	0	0	140	0	59
Initial Q (Qb), veh	0	0	0	0	0	0				0	0	0
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00				1.00	1.00	1.00
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00				1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00				1.00	1.00	1.00
Work Zone On Approach		No			No						No	
Adj Sat Flow, veh/h/ln	0	1856	1856	1722	1722	0				1796	1796	1796
Adj Flow Rate, veh/h	0	451	103	318	591	0				149	0	47
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94				0.94	0.94	0.94
Percent Heavy Veh, %	0	3	3	12	12	0				7	7	7
Cap, veh/h	0	1169	991	380	1355	0				174	0	55
Arrive On Green	0.00	0.63	0.63	0.24	1.00	0.00				0.14	0.00	0.14
Sat Flow, veh/h	0	1856	1572	3182	1722	0				1263	0	398
Grp Volume(v), veh/h	0	451	103	318	591	0				196	0	0
Grp Sat Flow(s),veh/h/ln	0	1856	1572	1591	1722	0				1661	0	0
Q Serve(g_s), s	0.0	14.3	3.1	11.4	0.0	0.0				13.8	0.0	0.0
Cycle Q Clear(g_c), s	0.0	14.3	3.1	11.4	0.0	0.0				13.8	0.0	0.0
Prop In Lane	0.00		1.00	1.00		0.00				0.76		0.24
Lane Grp Cap(c), veh/h	0	1169	991	380	1355	0				229	0	0
V/C Ratio(X)	0.00	0.39	0.10	0.84	0.44	0.00				0.85	0.00	0.00
Avail Cap(c_a), veh/h	0	1169	991	729	1355	0				367	0	0
HCM Platoon Ratio	1.00	1.00	1.00	2.00	2.00	1.00				1.00	1.00	1.00
Upstream Filter(I)	0.00	1.00	1.00	0.94	0.94	0.00				1.00	0.00	0.00
Uniform Delay (d), s/veh	0.0	10.8	8.8	44.6	0.0	0.0				50.5	0.0	0.0
Incr Delay (d2), s/veh	0.0	1.0	0.2	4.7	1.0	0.0				10.7	0.0	0.0
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0				0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.0	5.5	1.0	4.1	0.4	0.0				6.4	0.0	0.0
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	0.0	11.8	9.0	49.2	1.0	0.0				61.2	0.0	0.0
LnGrp LOS		B	A	D	A					E		
Approach Vol, veh/h		554			909						196	
Approach Delay, s/veh		11.3			17.9						61.2	
Approach LOS		B			B						E	
Timer - Assigned Phs	1	2		4		6						
Phs Duration (G+Y+Rc), s	18.8	80.1		21.1		98.9						
Change Period (Y+Rc), s	4.5	4.5		4.5		4.5						
Max Green Setting (Gmax), s	27.5	52.5		26.5		84.5						
Max Q Clear Time (g_c+I1), s	13.4	16.3		15.8		2.0						
Green Ext Time (p_c), s	0.9	3.1		0.8		4.0						
Intersection Summary												
HCM 7th Control Delay, s/veh				20.8								
HCM 7th LOS				C								

Timings
1: US-275 Southbound Ramps & Meigs St

2050 Background PM
02/12/2026



Lane Group	EBT	EBR	WBL	WBT	SBT
Lane Configurations	↑	↗	↖↗	↑	↕
Traffic Volume (vph)	798	339	636	771	1
Future Volume (vph)	798	339	636	771	1
Turn Type	NA	Perm	Prot	NA	NA
Protected Phases	2		1	6	4
Permitted Phases		2			
Detector Phase	2	2	1	6	4
Switch Phase					
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0
Minimum Split (s)	22.5	22.5	9.5	22.5	22.5
Total Split (s)	57.0	57.0	32.0	89.0	31.0
Total Split (%)	47.5%	47.5%	26.7%	74.2%	25.8%
Yellow Time (s)	3.5	3.5	3.5	3.5	3.5
All-Red Time (s)	1.0	1.0	1.0	1.0	1.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	4.5	4.5	4.5	4.5	4.5
Lead/Lag	Lag	Lag	Lead		
Lead-Lag Optimize?	Yes	Yes	Yes		
Recall Mode	C-Max	C-Max	None	C-Min	None

Intersection Summary

Cycle Length: 120
 Actuated Cycle Length: 120
 Offset: 0 (0%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 110
 Control Type: Actuated-Coordinated

Splits and Phases: 1: US-275 Southbound Ramps & Meigs St



HCM 7th Signalized Intersection Summary
 1: US-275 Southbound Ramps & Meigs St

2050 Background PM
 02/12/2026



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↑	↑	↑↑	↑						↑	
Traffic Volume (veh/h)	0	798	339	636	771	0	0	0	0	262	1	42
Future Volume (veh/h)	0	798	339	636	771	0	0	0	0	262	1	42
Initial Q (Qb), veh	0	0	0	0	0	0				0	0	0
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00				1.00	1.00	1.00
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00				1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00				1.00	1.00	1.00
Work Zone On Approach		No			No						No	
Adj Sat Flow, veh/h/ln	0	1870	1870	1856	1856	0				1678	1678	1678
Adj Flow Rate, veh/h	0	867	245	691	838	0				285	1	35
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92				0.92	0.92	0.92
Percent Heavy Veh, %	0	2	2	3	3	0				15	15	15
Cap, veh/h	0	851	721	737	1313	0				305	1	37
Arrive On Green	0.00	0.45	0.45	0.43	1.00	0.00				0.22	0.22	0.22
Sat Flow, veh/h	0	1870	1585	3428	1856	0				1400	5	172
Grp Volume(v), veh/h	0	867	245	691	838	0				321	0	0
Grp Sat Flow(s),veh/h/ln	0	1870	1585	1714	1856	0				1577	0	0
Q Serve(g_s), s	0.0	54.6	12.0	23.1	0.0	0.0				24.0	0.0	0.0
Cycle Q Clear(g_c), s	0.0	54.6	12.0	23.1	0.0	0.0				24.0	0.0	0.0
Prop In Lane	0.00		1.00	1.00		0.00				0.89		0.11
Lane Grp Cap(c), veh/h	0	851	721	737	1313	0				343	0	0
V/C Ratio(X)	0.00	1.02	0.34	0.94	0.64	0.00				0.94	0.00	0.00
Avail Cap(c_a), veh/h	0	851	721	786	1313	0				348	0	0
HCM Platoon Ratio	1.00	1.00	1.00	2.00	2.00	1.00				1.00	1.00	1.00
Upstream Filter(I)	0.00	1.00	1.00	0.42	0.42	0.00				1.00	0.00	0.00
Uniform Delay (d), s/veh	0.0	32.7	21.1	33.4	0.0	0.0				46.1	0.0	0.0
Incr Delay (d2), s/veh	0.0	35.7	1.3	9.3	1.0	0.0				31.9	0.0	0.0
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0				0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.0	31.3	4.5	7.8	0.4	0.0				12.4	0.0	0.0
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	0.0	68.4	22.4	42.7	1.0	0.0				78.0	0.0	0.0
LnGrp LOS		F	C	D	A					E		
Approach Vol, veh/h		1112			1529						321	
Approach Delay, s/veh		58.3			19.9						78.0	
Approach LOS		E			B						E	
Timer - Assigned Phs	1	2		4		6						
Phs Duration (G+Y+Rc), s	30.3	59.1		30.6		89.4						
Change Period (Y+Rc), s	4.5	4.5		4.5		4.5						
Max Green Setting (Gmax), s	27.5	52.5		26.5		84.5						
Max Q Clear Time (g_c+I1), s	25.1	56.6		26.0		2.0						
Green Ext Time (p_c), s	0.7	0.0		0.1		6.9						
Intersection Summary												
HCM 7th Control Delay, s/veh				40.6								
HCM 7th LOS				D								

Timings
1: US-275 Southbound Ramps & Meigs St

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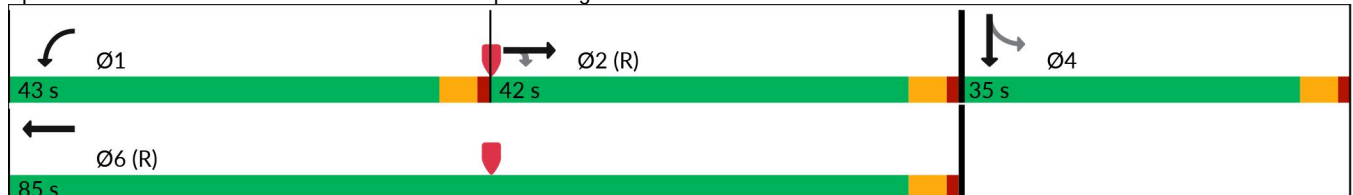


Lane Group	EBT	EBR	WBL	WBT	SBL	SBT
Lane Configurations	↑↑	↑	↔	↑↑	↑	↓
Traffic Volume (vph)	437	146	412	594	153	0
Future Volume (vph)	437	146	412	594	153	0
Turn Type	NA	Perm	Prot	NA	Perm	NA
Protected Phases	2		1	6		4
Permitted Phases		2			4	
Detector Phase	2	2	1	6	4	4
Switch Phase						
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0	5.0
Minimum Split (s)	22.5	22.5	9.5	22.5	22.5	22.5
Total Split (s)	42.0	42.0	43.0	85.0	35.0	35.0
Total Split (%)	35.0%	35.0%	35.8%	70.8%	29.2%	29.2%
Yellow Time (s)	3.5	3.5	3.5	3.5	3.5	3.5
All-Red Time (s)	1.0	1.0	1.0	1.0	1.0	1.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	4.5	4.5	4.5	4.5	4.5	4.5
Lead/Lag	Lag	Lag	Lead			
Lead-Lag Optimize?	Yes	Yes	Yes			
Recall Mode	C-Max	C-Max	None	C-Max	None	None

Intersection Summary

Cycle Length: 120
 Actuated Cycle Length: 120
 Offset: 0 (0%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 60
 Control Type: Actuated-Coordinated

Splits and Phases: 1: US-275 Southbound Ramps & Meigs St



HCM 7th Signalized Intersection Summary
 1: US-275 Southbound Ramps & Meigs St

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Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↑↑	↑	↑↑	↑↑					↑	↑	
Traffic Volume (veh/h)	0	437	146	412	594	0	0	0	0	153	0	59
Future Volume (veh/h)	0	437	146	412	594	0	0	0	0	153	0	59
Initial Q (Qb), veh	0	0	0	0	0	0				0	0	0
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00				1.00	1.00	1.00
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00				1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00				1.00	1.00	1.00
Work Zone On Approach		No			No						No	
Adj Sat Flow, veh/h/ln	0	1856	1856	1722	1722	0				1796	1796	1796
Adj Flow Rate, veh/h	0	465	103	438	632	0				163	0	47
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94				0.94	0.94	0.94
Percent Heavy Veh, %	0	3	3	12	12	0				7	7	7
Cap, veh/h	0	2164	965	505	2650	0				197	0	175
Arrive On Green	0.00	0.61	0.61	0.32	1.00	0.00				0.12	0.00	0.12
Sat Flow, veh/h	0	3618	1572	3182	3358	0				1711	0	1522
Grp Volume(v), veh/h	0	465	103	438	632	0				163	0	47
Grp Sat Flow(s),veh/h/ln	0	1763	1572	1591	1636	0				1711	0	1522
Q Serve(g_s), s	0.0	7.0	3.2	15.6	0.0	0.0				11.2	0.0	3.4
Cycle Q Clear(g_c), s	0.0	7.0	3.2	15.6	0.0	0.0				11.2	0.0	3.4
Prop In Lane	0.00		1.00	1.00		0.00				1.00		1.00
Lane Grp Cap(c), veh/h	0	2164	965	505	2650	0				197	0	175
V/C Ratio(X)	0.00	0.21	0.11	0.87	0.24	0.00				0.83	0.00	0.27
Avail Cap(c_a), veh/h	0	2164	965	1021	2650	0				435	0	387
HCM Platoon Ratio	1.00	1.00	1.00	2.00	2.00	1.00				1.00	1.00	1.00
Upstream Filter(I)	0.00	1.00	1.00	0.90	0.90	0.00				1.00	0.00	1.00
Uniform Delay (d), s/veh	0.0	10.3	9.6	39.8	0.0	0.0				51.9	0.0	48.5
Incr Delay (d2), s/veh	0.0	0.2	0.2	4.2	0.2	0.0				8.5	0.0	0.8
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0				0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.0	2.6	1.1	5.2	0.1	0.0				5.3	0.0	1.3
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	0.0	10.5	9.8	44.0	0.2	0.0				60.5	0.0	49.3
LnGrp LOS		B	A	D	A					E		D
Approach Vol, veh/h		568			1070						210	
Approach Delay, s/veh		10.4			18.1						58.0	
Approach LOS		B			B						E	
Timer - Assigned Phs	1	2		4		6						
Phs Duration (G+Y+Rc), s	23.5	78.1		18.3		101.7						
Change Period (Y+Rc), s	4.5	4.5		4.5		4.5						
Max Green Setting (Gmax), s	38.5	37.5		30.5		80.5						
Max Q Clear Time (g_c+I1), s	17.6	9.0		13.2		2.0						
Green Ext Time (p_c), s	1.5	3.2		0.6		4.5						

Intersection Summary												
HCM 7th Control Delay, s/veh				20.3								
HCM 7th LOS				C								

Notes
 User approved pedestrian interval to be less than phase max green.

Timings
1: US-275 Southbound Ramps & Meigs St

2050 Total AM
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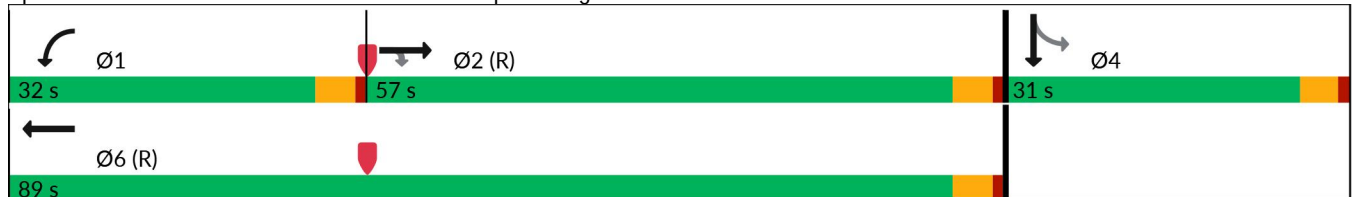


Lane Group	EBT	EBR	WBL	WBT	SBT
Lane Configurations	↑	↗	↖	↑	↕
Traffic Volume (vph)	437	146	412	594	0
Future Volume (vph)	437	146	412	594	0
Turn Type	NA	Perm	Prot	NA	NA
Protected Phases	2		1	6	4
Permitted Phases		2			
Detector Phase	2	2	1	6	4
Switch Phase					
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0
Minimum Split (s)	22.5	22.5	9.5	22.5	22.5
Total Split (s)	57.0	57.0	32.0	89.0	31.0
Total Split (%)	47.5%	47.5%	26.7%	74.2%	25.8%
Yellow Time (s)	3.5	3.5	3.5	3.5	3.5
All-Red Time (s)	1.0	1.0	1.0	1.0	1.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	4.5	4.5	4.5	4.5	4.5
Lead/Lag	Lag	Lag	Lead		
Lead-Lag Optimize?	Yes	Yes	Yes		
Recall Mode	C-Max	C-Max	None	C-Max	None

Intersection Summary

Cycle Length: 120
 Actuated Cycle Length: 120
 Offset: 0 (0%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 60
 Control Type: Actuated-Coordinated

Splits and Phases: 1: US-275 Southbound Ramps & Meigs St



HCM 7th Signalized Intersection Summary
 1: US-275 Southbound Ramps & Meigs St

2050 Total AM
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Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↑	↑	↑↑	↑						↑	
Traffic Volume (veh/h)	0	437	146	412	594	0	0	0	0	153	0	59
Future Volume (veh/h)	0	437	146	412	594	0	0	0	0	153	0	59
Initial Q (Qb), veh	0	0	0	0	0	0				0	0	0
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00				1.00	1.00	1.00
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00				1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00				1.00	1.00	1.00
Work Zone On Approach		No			No						No	
Adj Sat Flow, veh/h/ln	0	1856	1856	1722	1722	0				1796	1796	1796
Adj Flow Rate, veh/h	0	465	103	438	632	0				163	0	47
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94				0.94	0.94	0.94
Percent Heavy Veh, %	0	3	3	12	12	0				7	7	7
Cap, veh/h	0	1085	919	498	1341	0				189	0	55
Arrive On Green	0.00	0.58	0.58	0.31	1.00	0.00				0.15	0.00	0.15
Sat Flow, veh/h	0	1856	1572	3182	1722	0				1292	0	373
Grp Volume(v), veh/h	0	465	103	438	632	0				210	0	0
Grp Sat Flow(s),veh/h/ln	0	1856	1572	1591	1722	0				1665	0	0
Q Serve(g_s), s	0.0	16.7	3.5	15.7	0.0	0.0				14.8	0.0	0.0
Cycle Q Clear(g_c), s	0.0	16.7	3.5	15.7	0.0	0.0				14.8	0.0	0.0
Prop In Lane	0.00		1.00	1.00		0.00				0.78		0.22
Lane Grp Cap(c), veh/h	0	1085	919	498	1341	0				244	0	0
V/C Ratio(X)	0.00	0.43	0.11	0.88	0.47	0.00				0.86	0.00	0.00
Avail Cap(c_a), veh/h	0	1085	919	729	1341	0				368	0	0
HCM Platoon Ratio	1.00	1.00	1.00	2.00	2.00	1.00				1.00	1.00	1.00
Upstream Filter(I)	0.00	1.00	1.00	0.90	0.90	0.00				1.00	0.00	0.00
Uniform Delay (d), s/veh	0.0	13.8	11.1	40.1	0.0	0.0				50.0	0.0	0.0
Incr Delay (d2), s/veh	0.0	1.2	0.2	7.7	1.1	0.0				12.6	0.0	0.0
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0				0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.0	6.8	1.2	5.5	0.4	0.0				7.0	0.0	0.0
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	0.0	15.1	11.3	47.9	1.1	0.0				62.6	0.0	0.0
LnGrp LOS		B	B	D	A					E		
Approach Vol, veh/h		568			1070						210	
Approach Delay, s/veh		14.4			20.2						62.6	
Approach LOS		B			C						E	
Timer - Assigned Phs	1	2		4		6						
Phs Duration (G+Y+Rc), s	23.3	74.7		22.1		97.9						
Change Period (Y+Rc), s	4.5	4.5		4.5		4.5						
Max Green Setting (Gmax), s	27.5	52.5		26.5		84.5						
Max Q Clear Time (g_c+I1), s	17.7	18.7		16.8		2.0						
Green Ext Time (p_c), s	1.1	3.1		0.8		4.4						
Intersection Summary												
HCM 7th Control Delay, s/veh				23.3								
HCM 7th LOS				C								

Timings
1: US-275 Southbound Ramps & Meigs St

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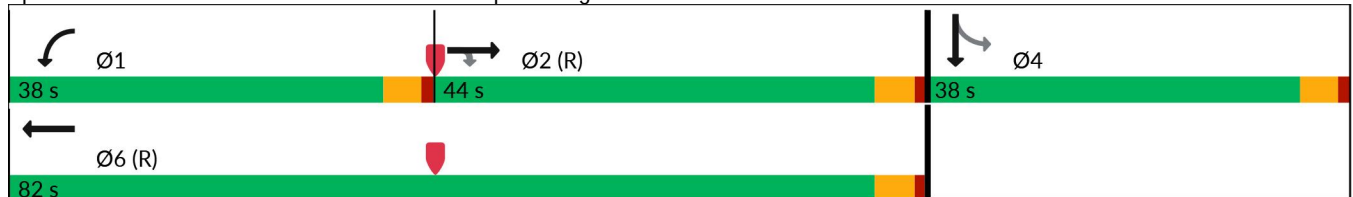


Lane Group	EBT	EBR	WBL	WBT	SBL	SBT
Lane Configurations	↑↑	↑	↔	↑↑	↑	↓
Traffic Volume (vph)	837	339	710	796	301	1
Future Volume (vph)	837	339	710	796	301	1
Turn Type	NA	Perm	Prot	NA	Perm	NA
Protected Phases	2		1	6		4
Permitted Phases		2			4	
Detector Phase	2	2	1	6	4	4
Switch Phase						
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0	5.0
Minimum Split (s)	22.5	22.5	9.5	22.5	22.5	22.5
Total Split (s)	44.0	44.0	38.0	82.0	38.0	38.0
Total Split (%)	36.7%	36.7%	31.7%	68.3%	31.7%	31.7%
Yellow Time (s)	3.5	3.5	3.5	3.5	3.5	3.5
All-Red Time (s)	1.0	1.0	1.0	1.0	1.0	1.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	4.5	4.5	4.5	4.5	4.5	4.5
Lead/Lag	Lag	Lag	Lead			
Lead-Lag Optimize?	Yes	Yes	Yes			
Recall Mode	C-Max	C-Max	None	C-Max	None	None

Intersection Summary

Cycle Length: 120
 Actuated Cycle Length: 120
 Offset: 0 (0%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 75
 Control Type: Actuated-Coordinated

Splits and Phases: 1: US-275 Southbound Ramps & Meigs St



HCM 7th Signalized Intersection Summary
 1: US-275 Southbound Ramps & Meigs St

2050 Total PM - Improved #1
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Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↑↑	↑	↑↑	↑↑					↑	↑	
Traffic Volume (veh/h)	0	837	339	710	796	0	0	0	0	301	1	42
Future Volume (veh/h)	0	837	339	710	796	0	0	0	0	301	1	42
Initial Q (Qb), veh	0	0	0	0	0	0				0	0	0
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00				1.00	1.00	1.00
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00				1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00				1.00	1.00	1.00
Work Zone On Approach		No			No						No	
Adj Sat Flow, veh/h/ln	0	1870	1870	1856	1856	0				1678	1678	1678
Adj Flow Rate, veh/h	0	910	245	772	865	0				327	1	35
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92				0.92	0.92	0.92
Percent Heavy Veh, %	0	2	2	3	3	0				15	15	15
Cap, veh/h	0	1468	655	862	2475	0				356	9	310
Arrive On Green	0.00	0.41	0.41	0.08	0.23	0.00				0.22	0.22	0.22
Sat Flow, veh/h	0	3647	1585	3428	3618	0				1598	40	1388
Grp Volume(v), veh/h	0	910	245	772	865	0				327	0	36
Grp Sat Flow(s),veh/h/ln	0	1777	1585	1714	1763	0				1598	0	1428
Q Serve(g_s), s	0.0	24.2	12.9	26.8	24.6	0.0				24.0	0.0	2.4
Cycle Q Clear(g_c), s	0.0	24.2	12.9	26.8	24.6	0.0				24.0	0.0	2.4
Prop In Lane	0.00		1.00	1.00		0.00				1.00		0.97
Lane Grp Cap(c), veh/h	0	1468	655	862	2475	0				356	0	318
V/C Ratio(X)	0.00	0.62	0.37	0.90	0.35	0.00				0.92	0.00	0.11
Avail Cap(c_a), veh/h	0	1468	655	957	2475	0				446	0	399
HCM Platoon Ratio	1.00	1.00	1.00	0.33	0.33	1.00				1.00	1.00	1.00
Upstream Filter(I)	0.00	1.00	1.00	0.76	0.76	0.00				1.00	0.00	1.00
Uniform Delay (d), s/veh	0.0	27.8	24.4	53.5	23.2	0.0				45.5	0.0	37.2
Incr Delay (d2), s/veh	0.0	2.0	1.6	8.1	0.3	0.0				20.9	0.0	0.2
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0				0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.0	10.2	4.9	13.2	11.6	0.0				11.5	0.0	0.9
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	0.0	29.8	26.1	61.6	23.5	0.0				66.4	0.0	37.3
LnGrp LOS		C	C	E	C					E		D
Approach Vol, veh/h		1155			1637						363	
Approach Delay, s/veh		29.0			41.5						63.5	
Approach LOS		C			D						E	
Timer - Assigned Phs	1	2		4		6						
Phs Duration (G+Y+Rc), s	34.7	54.1		31.3		88.7						
Change Period (Y+Rc), s	4.5	4.5		4.5		4.5						
Max Green Setting (Gmax), s	33.5	39.5		33.5		77.5						
Max Q Clear Time (g_c+I1), s	28.8	26.2		26.0		26.6						
Green Ext Time (p_c), s	1.4	5.5		0.8		6.6						
Intersection Summary												
HCM 7th Control Delay, s/veh				39.4								
HCM 7th LOS				D								

Timings
1: US-275 Southbound Ramps & Meigs St

2050 Total PM
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Lane Group	EBT	EBR	WBL	WBT	SBT
Lane Configurations	↑	↗	↖↗	↑	↕
Traffic Volume (vph)	837	339	710	796	1
Future Volume (vph)	837	339	710	796	1
Turn Type	NA	Perm	Prot	NA	NA
Protected Phases	2		1	6	4
Permitted Phases		2			
Detector Phase	2	2	1	6	4
Switch Phase					
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0
Minimum Split (s)	22.5	22.5	9.5	22.5	22.5
Total Split (s)	60.0	60.0	31.0	91.0	29.0
Total Split (%)	50.0%	50.0%	25.8%	75.8%	24.2%
Yellow Time (s)	3.5	3.5	3.5	3.5	3.5
All-Red Time (s)	1.0	1.0	1.0	1.0	1.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	4.5	4.5	4.5	4.5	4.5
Lead/Lag	Lag	Lag	Lead		
Lead-Lag Optimize?	Yes	Yes	Yes		
Recall Mode	C-Max	C-Max	None	C-Max	None

Intersection Summary

Cycle Length: 120
 Actuated Cycle Length: 120
 Offset: 0 (0%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 130
 Control Type: Actuated-Coordinated

Splits and Phases: 1: US-275 Southbound Ramps & Meigs St



HCM 7th Signalized Intersection Summary
 1: US-275 Southbound Ramps & Meigs St

2050 Total PM
 02/12/2026



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↑	↗	↘	↑						↕	
Traffic Volume (veh/h)	0	837	339	710	796	0	0	0	0	301	1	42
Future Volume (veh/h)	0	837	339	710	796	0	0	0	0	301	1	42
Initial Q (Qb), veh	0	0	0	0	0	0				0	0	0
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00				1.00	1.00	1.00
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00				1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00				1.00	1.00	1.00
Work Zone On Approach		No			No						No	
Adj Sat Flow, veh/h/ln	0	1870	1870	1856	1856	0				1678	1678	1678
Adj Flow Rate, veh/h	0	910	245	772	865	0				327	1	35
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92				0.92	0.92	0.92
Percent Heavy Veh, %	0	2	2	3	3	0				15	15	15
Cap, veh/h	0	865	733	757	1338	0				290	1	31
Arrive On Green	0.00	0.46	0.46	0.29	0.96	0.00				0.20	0.20	0.20
Sat Flow, veh/h	0	1870	1585	3428	1856	0				1423	4	152
Grp Volume(v), veh/h	0	910	245	772	865	0				363	0	0
Grp Sat Flow(s),veh/h/ln	0	1870	1585	1714	1856	0				1579	0	0
Q Serve(g_s), s	0.0	55.5	11.8	26.5	6.1	0.0				24.5	0.0	0.0
Cycle Q Clear(g_c), s	0.0	55.5	11.8	26.5	6.1	0.0				24.5	0.0	0.0
Prop In Lane	0.00		1.00	1.00		0.00				0.90		0.10
Lane Grp Cap(c), veh/h	0	865	733	757	1338	0				322	0	0
V/C Ratio(X)	0.00	1.05	0.33	1.02	0.65	0.00				1.13	0.00	0.00
Avail Cap(c_a), veh/h	0	865	733	757	1338	0				322	0	0
HCM Platoon Ratio	1.00	1.00	1.00	1.33	1.33	1.00				1.00	1.00	1.00
Upstream Filter(I)	0.00	1.00	1.00	0.16	0.16	0.00				1.00	0.00	0.00
Uniform Delay (d), s/veh	0.0	32.3	20.5	42.4	0.8	0.0				47.8	0.0	0.0
Incr Delay (d2), s/veh	0.0	45.2	1.2	18.4	0.4	0.0				88.6	0.0	0.0
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0				0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.0	33.9	4.4	12.0	0.9	0.0				17.5	0.0	0.0
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	0.0	77.4	21.7	60.7	1.2	0.0				136.3	0.0	0.0
LnGrp LOS		F	C	F	A					F		
Approach Vol, veh/h		1155			1637						363	
Approach Delay, s/veh		65.6			29.3						136.3	
Approach LOS		E			C						F	
Timer - Assigned Phs	1	2		4		6						
Phs Duration (G+Y+Rc), s	31.0	60.0		29.0		91.0						
Change Period (Y+Rc), s	4.5	4.5		4.5		4.5						
Max Green Setting (Gmax), s	26.5	55.5		24.5		86.5						
Max Q Clear Time (g_c+I1), s	28.5	57.5		26.5		8.1						
Green Ext Time (p_c), s	0.0	0.0		0.0		7.3						
Intersection Summary												
HCM 7th Control Delay, s/veh				54.9								
HCM 7th LOS				D								

Intersection												
Int Delay, s/veh	4.9											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↑			↗			↕				
Traffic Vol, veh/h	31	77	0	0	78	41	100	0	56	0	0	0
Future Vol, veh/h	31	77	0	0	78	41	100	0	56	0	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	175	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	95	95	95	95	95	95	95	95	95	95	95	95
Heavy Vehicles, %	8	8	8	21	21	21	2	2	2	2	2	2
Mvmt Flow	33	81	0	0	82	43	105	0	59	0	0	0

Major/Minor	Major1	Major2	Minor1
Conflicting Flow All	125	0	0
Stage 1	-	-	-
Stage 2	-	-	-
Critical Hdwy	4.18	-	-
Critical Hdwy Stg 1	-	-	-
Critical Hdwy Stg 2	-	-	-
Follow-up Hdwy	2.272	-	-
Pot Cap-1 Maneuver	1425	0	0
Stage 1	-	0	0
Stage 2	-	0	0
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	1425	-	-
Mov Cap-2 Maneuver	-	-	-
Stage 1	-	-	-
Stage 2	-	-	-

Approach	EB	WB	NB
HCM Ctrl Dly, s/v	2.18	0	10.55
HCM LOS			B

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	WBT	WBR
Capacity (veh/h)	813	1425	-	-	-
HCM Lane V/C Ratio	0.202	0.023	-	-	-
HCM Ctrl Dly (s/v)	10.5	7.6	-	-	-
HCM Lane LOS	B	A	-	-	-
HCM 95th %tile Q(veh)	0.8	0.1	-	-	-

Intersection												
Int Delay, s/veh	6.8											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↑			↗			↕				
Traffic Vol, veh/h	56	118	0	0	152	78	183	0	96	0	0	0
Future Vol, veh/h	56	118	0	0	152	78	183	0	96	0	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	175	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	93	93	93	93	93	93	93	93	93	93	93	93
Heavy Vehicles, %	8	8	8	5	5	5	2	2	2	2	2	2
Mvmt Flow	60	127	0	0	163	84	197	0	103	0	0	0

Major/Minor	Major1	Major2	Minor1
Conflicting Flow All	247	0	- - - 0 411 495 127
Stage 1	-	-	- - - 247 247 -
Stage 2	-	-	- - - 163 247 -
Critical Hdwy	4.18	-	- - - 6.42 6.52 6.22
Critical Hdwy Stg 1	-	-	- - - 5.42 5.52 -
Critical Hdwy Stg 2	-	-	- - - 5.42 5.52 -
Follow-up Hdwy	2.272	-	- - - 3.518 4.018 3.318
Pot Cap-1 Maneuver	1284	- 0 0	- - 597 476 923
Stage 1	-	- 0 0	- - 794 702 -
Stage 2	-	- 0 0	- - 866 702 -
Platoon blocked, %		-	- -
Mov Cap-1 Maneuver	1284	- - -	- - 569 0 923
Mov Cap-2 Maneuver	-	- - -	- - 569 0 -
Stage 1	-	- - -	- - 757 0 -
Stage 2	-	- - -	- - 866 0 -

Approach	EB	WB	NB
HCM Ctrl Dly, s/v	2.56	0	15.03
HCM LOS			C

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	WBT	WBR
Capacity (veh/h)	656	1284	-	-	-
HCM Lane V/C Ratio	0.457	0.047	-	-	-
HCM Ctrl Dly (s/v)	15	7.9	-	-	-
HCM Lane LOS	C	A	-	-	-
HCM 95th %tile Q(veh)	2.4	0.1	-	-	-

Intersection												
Int Delay, s/veh	5.1											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↑			↗			↕				
Traffic Vol, veh/h	36	89	0	0	90	47	115	0	65	0	0	0
Future Vol, veh/h	36	89	0	0	90	47	115	0	65	0	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	175	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	95	95	95	95	95	95	95	95	95	95	95	95
Heavy Vehicles, %	8	8	8	21	21	21	2	2	2	2	2	2
Mvmt Flow	38	94	0	0	95	49	121	0	68	0	0	0

Major/Minor	Major1	Major2	Minor1
Conflicting Flow All	144	0	- - - 0 264 314 94
Stage 1	-	-	- - - 169 169 -
Stage 2	-	-	- - - 95 144 -
Critical Hdwy	4.18	-	- - - 6.42 6.52 6.22
Critical Hdwy Stg 1	-	-	- - - 5.42 5.52 -
Critical Hdwy Stg 2	-	-	- - - 5.42 5.52 -
Follow-up Hdwy	2.272	-	- - - 3.518 4.018 3.318
Pot Cap-1 Maneuver	1402	- 0 0	- - - 725 602 963
Stage 1	-	- 0 0	- - - 860 758 -
Stage 2	-	- 0 0	- - - 929 778 -
Platoon blocked, %	-	-	- -
Mov Cap-1 Maneuver	1402	- - -	- - - 705 0 963
Mov Cap-2 Maneuver	-	- - -	- - - 705 0 -
Stage 1	-	- - -	- - - 837 0 -
Stage 2	-	- - -	- - - 929 0 -

Approach	EB	WB	NB
HCM Ctrl Dly, s/v	2.2	0	11.08
HCM LOS			B

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	WBT	WBR
Capacity (veh/h)	781	1402	-	-	-
HCM Lane V/C Ratio	0.243	0.027	-	-	-
HCM Ctrl Dly (s/v)	11.1	7.6	-	-	-
HCM Lane LOS	B	A	-	-	-
HCM 95th %tile Q(veh)	0.9	0.1	-	-	-

Intersection												
Int Delay, s/veh	8.2											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗			↖			↗				
Traffic Vol, veh/h	65	136	0	0	175	90	211	0	111	0	0	0
Future Vol, veh/h	65	136	0	0	175	90	211	0	111	0	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	175	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	93	93	93	93	93	93	93	93	93	93	93	93
Heavy Vehicles, %	8	8	8	5	5	5	2	2	2	2	2	2
Mvmt Flow	70	146	0	0	188	97	227	0	119	0	0	0

Major/Minor	Major1	Major2	Minor1
Conflicting Flow All	285	0	-
Stage 1	-	-	-
Stage 2	-	-	-
Critical Hdwy	4.18	-	-
Critical Hdwy Stg 1	-	-	-
Critical Hdwy Stg 2	-	-	-
Follow-up Hdwy	2.272	-	-
Pot Cap-1 Maneuver	1244	0	0
Stage 1	-	0	0
Stage 2	-	0	0
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	1244	-	-
Mov Cap-2 Maneuver	-	-	-
Stage 1	-	-	-
Stage 2	-	-	-

Approach	EB	WB	NB
HCM Ctrl Dly, s/v	2.61	0	18.51
HCM LOS			C

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	WBT	WBR
Capacity (veh/h)	607	1244	-	-	-
HCM Lane V/C Ratio	0.57	0.056	-	-	-
HCM Ctrl Dly (s/v)	18.5	8.1	-	-	-
HCM Lane LOS	C	A	-	-	-
HCM 95th %tile Q(veh)	3.6	0.2	-	-	-

Intersection												
Int Delay, s/veh	4											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↑			↗			↖	↗			
Traffic Vol, veh/h	36	115	0	0	240	85	115	0	104	0	0	0
Future Vol, veh/h	36	115	0	0	240	85	115	0	104	0	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	175	-	-	-	-	-	-	-	0	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	95	95	95	95	95	95	95	95	95	95	95	95
Heavy Vehicles, %	8	8	8	21	21	21	2	2	2	2	2	2
Mvmt Flow	38	121	0	0	253	89	121	0	109	0	0	0

Major/Minor	Major1	Major2	Minor1
Conflicting Flow All	342	0	- - - 0 449 539 121
Stage 1	-	-	- - - 197 197 -
Stage 2	-	-	- - - 253 342 -
Critical Hdwy	4.18	-	- - - 6.42 6.52 6.22
Critical Hdwy Stg 1	-	-	- - - 5.42 5.52 -
Critical Hdwy Stg 2	-	-	- - - 5.42 5.52 -
Follow-up Hdwy	2.272	-	- - - 3.518 4.018 3.318
Pot Cap-1 Maneuver	1184	- 0 0	- - 567 449 930
Stage 1	-	- 0 0	- - 836 738 -
Stage 2	-	- 0 0	- - 789 638 -
Platoon blocked, %		-	- -
Mov Cap-1 Maneuver	1184	- - -	- 549 0 930
Mov Cap-2 Maneuver	-	- - -	- 549 0 -
Stage 1	-	- - -	- 810 0 -
Stage 2	-	- - -	- 789 0 -

Approach	EB	WB	NB
HCM Ctrl Dly, s/v	1.94	0	11.49
HCM LOS			B

Minor Lane/Major Mvmt	NBLn1	NBLn2	EBL	EBT	WBT	WBR
Capacity (veh/h)	549	930	1184	-	-	-
HCM Lane V/C Ratio	0.22	0.118	0.032	-	-	-
HCM Ctrl Dly (s/v)	13.4	9.4	8.1	-	-	-
HCM Lane LOS	B	A	A	-	-	-
HCM 95th %tile Q(veh)	0.8	0.4	0.1	-	-	-

Intersection												
Int Delay, s/veh	7.7											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗			↖			↖	↗			
Traffic Vol, veh/h	65	214	0	0	273	115	211	0	228	0	0	0
Future Vol, veh/h	65	214	0	0	273	115	211	0	228	0	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	175	-	-	-	-	-	-	-	0	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	93	93	93	93	93	93	93	93	93	93	93	93
Heavy Vehicles, %	8	8	8	5	5	5	2	2	2	2	2	2
Mvmt Flow	70	230	0	0	294	124	227	0	245	0	0	0

Major/Minor	Major1	Major2	Minor1
Conflicting Flow All	417	0	- - - 0 663 787 230
Stage 1	-	-	- - - 370 370 -
Stage 2	-	-	- - - 294 417 -
Critical Hdwy	4.18	-	- - - 6.42 6.52 6.22
Critical Hdwy Stg 1	-	-	- - - 5.42 5.52 -
Critical Hdwy Stg 2	-	-	- - - 5.42 5.52 -
Follow-up Hdwy	2.272	-	- - - 3.518 4.018 3.318
Pot Cap-1 Maneuver	1110	- 0 0	- - 426 324 809
Stage 1	-	- 0 0	- - 699 620 -
Stage 2	-	- 0 0	- - 757 591 -
Platoon blocked, %		-	- -
Mov Cap-1 Maneuver	1110	- - -	- - 399 0 809
Mov Cap-2 Maneuver	-	- - -	- - 399 0 -
Stage 1	-	- - -	- - 655 0 -
Stage 2	-	- - -	- - 757 0 -

Approach	EB	WB	NB
HCM Ctrl Dly, s/v	1.97	0	18.04
HCM LOS			C

Minor Lane/Major Mvmt	NBLn1	NBLn2	EBL	EBT	WBT	WBR
Capacity (veh/h)	399	809	1110	-	-	-
HCM Lane V/C Ratio	0.568	0.303	0.063	-	-	-
HCM Ctrl Dly (s/v)	25.2	11.4	8.5	-	-	-
HCM Lane LOS	D	B	A	-	-	-
HCM 95th %tile Q(veh)	3.4	1.3	0.2	-	-	-

Timings
2: US-275 Northbound Ramps & Meigs St

2050 Background AM
02/12/2026



Lane Group	EBL	EBT	WBT	WBR	NBT	NBR
Lane Configurations	↘	↑↑	↑↑	↗	↘	↗
Traffic Volume (vph)	29	533	696	135	1	382
Future Volume (vph)	29	533	696	135	1	382
Turn Type	pm+pt	NA	NA	Perm	NA	Perm
Protected Phases	5	2	6		4	
Permitted Phases	2			6		4
Detector Phase	5	2	6	6	4	4
Switch Phase						
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0	5.0
Minimum Split (s)	22.5	22.5	22.5	22.5	22.5	22.5
Total Split (s)	23.0	78.0	55.0	55.0	42.0	42.0
Total Split (%)	19.2%	65.0%	45.8%	45.8%	35.0%	35.0%
Yellow Time (s)	3.5	3.5	3.5	3.5	3.5	3.5
All-Red Time (s)	1.0	1.0	1.0	1.0	1.0	1.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	4.5	4.5	4.5	4.5	4.5	4.5
Lead/Lag	Lead		Lag	Lag		
Lead-Lag Optimize?	Yes		Yes	Yes		
Recall Mode	Min	C-Min	C-Max	C-Max	None	None

Intersection Summary

Cycle Length: 120
 Actuated Cycle Length: 120
 Offset: 0 (0%), Referenced to phase 2:EBTL and 6:WBT, Start of Green
 Natural Cycle: 70
 Control Type: Actuated-Coordinated

Splits and Phases: 2: US-275 Northbound Ramps & Meigs St



HCM 7th Signalized Intersection Summary
 2: US-275 Northbound Ramps & Meigs St

2050 Background AM
 02/12/2026



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↘	↑↑			↑↑	↗		↘	↗			
Traffic Volume (veh/h)	29	533	0	0	696	135	146	1	382	0	0	0
Future Volume (veh/h)	29	533	0	0	696	135	146	1	382	0	0	0
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0			
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00			
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00			
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00			
Work Zone On Approach		No			No			No				
Adj Sat Flow, veh/h/ln	1781	1781	0	0	1589	1589	1870	1870	1870			
Adj Flow Rate, veh/h	31	561	0	0	733	142	154	1	268			
Peak Hour Factor	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95			
Percent Heavy Veh, %	8	8	0	0	21	21	2	2	2			
Cap, veh/h	456	2473	0	0	1967	877	344	2	308			
Arrive On Green	0.08	1.00	0.00	0.00	0.65	0.65	0.19	0.19	0.19			
Sat Flow, veh/h	1697	3474	0	0	3098	1346	1770	11	1585			
Grp Volume(v), veh/h	31	561	0	0	733	142	155	0	268			
Grp Sat Flow(s),veh/h/ln	1697	1692	0	0	1509	1346	1782	0	1585			
Q Serve(g_s), s	0.6	0.0	0.0	0.0	13.4	4.9	9.2	0.0	19.7			
Cycle Q Clear(g_c), s	0.6	0.0	0.0	0.0	13.4	4.9	9.2	0.0	19.7			
Prop In Lane	1.00		0.00	0.00		1.00	0.99		1.00			
Lane Grp Cap(c), veh/h	456	2473	0	0	1967	877	346	0	308			
V/C Ratio(X)	0.07	0.23	0.00	0.00	0.37	0.16	0.45	0.00	0.87			
Avail Cap(c_a), veh/h	647	2473	0	0	1967	877	557	0	495			
HCM Platoon Ratio	2.00	2.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00			
Upstream Filter(I)	0.92	0.92	0.00	0.00	1.00	1.00	1.00	0.00	1.00			
Uniform Delay (d), s/veh	5.9	0.0	0.0	0.0	9.6	8.1	42.7	0.0	46.9			
Incr Delay (d2), s/veh	0.1	0.2	0.0	0.0	0.5	0.4	0.9	0.0	9.5			
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0			
%ile BackOfQ(50%),veh/ln	0.2	0.1	0.0	0.0	4.0	1.4	4.2	0.0	16.9			
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	6.0	0.2	0.0	0.0	10.2	8.5	43.6	0.0	56.3			
LnGrp LOS	A	A			B	A	D		E			
Approach Vol, veh/h		592			875			423				
Approach Delay, s/veh		0.5			9.9			51.7				
Approach LOS		A			A			D				
Timer - Assigned Phs		2		4	5	6						
Phs Duration (G+Y+Rc), s		92.2		27.8	9.5	82.7						
Change Period (Y+Rc), s		4.5		4.5	4.5	4.5						
Max Green Setting (Gmax), s		73.5		37.5	18.5	50.5						
Max Q Clear Time (g_c+I1), s		2.0		21.7	2.6	15.4						
Green Ext Time (p_c), s		3.9		1.6	0.0	5.8						
Intersection Summary												
HCM 7th Control Delay, s/veh				16.3								
HCM 7th LOS				B								

Timings
2: US-275 Northbound Ramps & Meigs St

2050 Background PM
02/19/2026



Lane Group	EBL	EBT	WBT	WBR	NBT	NBR
Lane Configurations	↘	↑↑	↑↑	↗	↘	↗
Traffic Volume (vph)	59	974	1139	298	3	653
Future Volume (vph)	59	974	1139	298	3	653
Turn Type	pm+pt	NA	NA	Perm	NA	Perm
Protected Phases	5	2	6		4	
Permitted Phases	2			6		4
Detector Phase	5	2	6	6	4	4
Switch Phase						
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0	5.0
Minimum Split (s)	9.5	22.5	22.5	22.5	22.5	22.5
Total Split (s)	9.6	62.0	52.4	52.4	58.0	58.0
Total Split (%)	8.0%	51.7%	43.7%	43.7%	48.3%	48.3%
Yellow Time (s)	3.5	3.5	3.5	3.5	3.5	3.5
All-Red Time (s)	1.0	1.0	1.0	1.0	1.0	1.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	4.5	4.5	4.5	4.5	4.5	4.5
Lead/Lag	Lead		Lag	Lag		
Lead-Lag Optimize?	Yes		Yes	Yes		
Recall Mode	None	C-Max	C-Max	C-Max	None	None

Intersection Summary

Cycle Length: 120
 Actuated Cycle Length: 120
 Offset: 0 (0%), Referenced to phase 2:EBTL and 6:WBT, Start of Green
 Natural Cycle: 90
 Control Type: Actuated-Coordinated

Splits and Phases: 2: US-275 Northbound Ramps & Meigs St



HCM 7th Signalized Intersection Summary
 2: US-275 Northbound Ramps & Meigs St

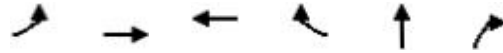
2050 Background PM
 02/19/2026



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↘	↑↑			↑↑	↗		↘	↗			
Traffic Volume (veh/h)	59	974	0	0	1139	298	268	3	653	0	0	0
Future Volume (veh/h)	59	974	0	0	1139	298	268	3	653	0	0	0
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0			
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00			
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00			
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00			
Work Zone On Approach		No			No			No				
Adj Sat Flow, veh/h/ln	1781	1781	0	0	1826	1826	1870	1870	1870			
Adj Flow Rate, veh/h	63	1047	0	0	1225	320	288	3	468			
Peak Hour Factor	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93			
Percent Heavy Veh, %	8	8	0	0	5	5	2	2	2			
Cap, veh/h	206	2024	0	0	1817	811	577	6	518			
Arrive On Green	0.07	1.00	0.00	0.00	0.52	0.52	0.33	0.33	0.33			
Sat Flow, veh/h	1697	3474	0	0	3561	1547	1764	18	1585			
Grp Volume(v), veh/h	63	1047	0	0	1225	320	291	0	468			
Grp Sat Flow(s),veh/h/ln	1697	1692	0	0	1735	1547	1782	0	1585			
Q Serve(g_s), s	2.0	0.0	0.0	0.0	31.2	14.9	15.8	0.0	33.8			
Cycle Q Clear(g_c), s	2.0	0.0	0.0	0.0	31.2	14.9	15.8	0.0	33.8			
Prop In Lane	1.00		0.00	0.00		1.00	0.99		1.00			
Lane Grp Cap(c), veh/h	206	2024	0	0	1817	811	583	0	518			
V/C Ratio(X)	0.31	0.52	0.00	0.00	0.67	0.39	0.50	0.00	0.90			
Avail Cap(c_a), veh/h	216	2024	0	0	1817	811	795	0	707			
HCM Platoon Ratio	2.00	2.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00			
Upstream Filter(I)	0.09	0.09	0.00	0.00	1.00	1.00	1.00	0.00	1.00			
Uniform Delay (d), s/veh	16.2	0.0	0.0	0.0	21.0	17.1	32.5	0.0	38.6			
Incr Delay (d2), s/veh	0.1	0.1	0.0	0.0	2.0	1.4	0.7	0.0	11.9			
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0			
%ile BackOfQ(50%),veh/ln	0.7	0.0	0.0	0.0	12.2	5.3	6.9	0.0	27.8			
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	16.2	0.1	0.0	0.0	23.0	18.6	33.1	0.0	50.5			
LnGrp LOS	B	A			C	B	C		D			
Approach Vol, veh/h		1110			1545			759				
Approach Delay, s/veh		1.0			22.1			43.8				
Approach LOS		A			C			D				
Timer - Assigned Phs		2		4	5	6						
Phs Duration (G+Y+Rc), s		76.3		43.7	8.9	67.4						
Change Period (Y+Rc), s		4.5		4.5	4.5	4.5						
Max Green Setting (Gmax), s		57.5		53.5	5.1	47.9						
Max Q Clear Time (g_c+I1), s		2.0		35.8	4.0	33.2						
Green Ext Time (p_c), s		8.8		3.4	0.0	8.0						
Intersection Summary												
HCM 7th Control Delay, s/veh				20.1								
HCM 7th LOS				C								

Timings
2: US-275 Northbound Ramps & Meigs St

2050 Total AM
02/13/2026

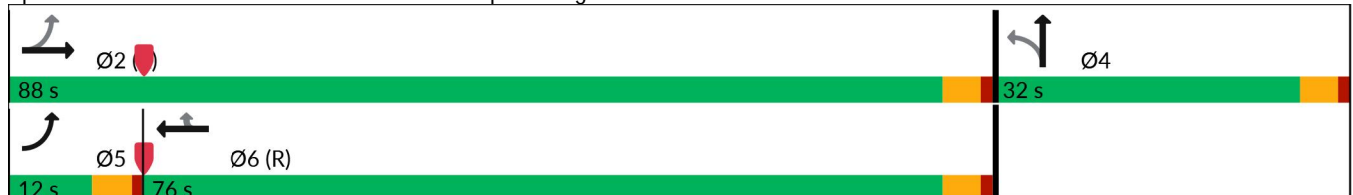


Lane Group	EBL	EBT	WBT	WBR	NBT	NBR
Lane Configurations	↘	↑↑	↑↑	↗	↘	↗
Traffic Volume (vph)	29	559	846	173	1	421
Future Volume (vph)	29	559	846	173	1	421
Turn Type	pm+pt	NA	NA	Perm	NA	Free
Protected Phases	5	2	6		4	
Permitted Phases	2			6		Free
Detector Phase	5	2	6	6	4	
Switch Phase						
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0	
Minimum Split (s)	9.5	22.5	22.5	22.5	22.5	
Total Split (s)	12.0	88.0	76.0	76.0	32.0	
Total Split (%)	10.0%	73.3%	63.3%	63.3%	26.7%	
Yellow Time (s)	3.5	3.5	3.5	3.5	3.5	
All-Red Time (s)	1.0	1.0	1.0	1.0	1.0	
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	
Total Lost Time (s)	4.5	4.5	4.5	4.5	4.5	
Lead/Lag	Lead		Lag	Lag		
Lead-Lag Optimize?	Yes		Yes	Yes		
Recall Mode	None	C-Max	C-Max	C-Max	None	

Intersection Summary

Cycle Length: 120
 Actuated Cycle Length: 120
 Offset: 0 (0%), Referenced to phase 2:EBTL and 6:WBT, Start of Green
 Natural Cycle: 60
 Control Type: Actuated-Coordinated

Splits and Phases: 2: US-275 Northbound Ramps & Meigs St



HCM 7th Signalized Intersection Summary
 2: US-275 Northbound Ramps & Meigs St

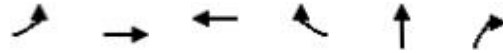
2050 Total AM
 02/13/2026



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↘	↑↑			↑↑	↗		↘	↗			
Traffic Volume (veh/h)	29	559	0	0	846	173	146	1	421	0	0	0
Future Volume (veh/h)	29	559	0	0	846	173	146	1	421	0	0	0
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0			
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00			
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00			
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00			
Work Zone On Approach		No			No			No				
Adj Sat Flow, veh/h/ln	1781	1781	0	0	1589	1589	1870	1870	1870			
Adj Flow Rate, veh/h	31	588	0	0	891	182	154	1	0			
Peak Hour Factor	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95			
Percent Heavy Veh, %	8	8	0	0	21	21	2	2	2			
Cap, veh/h	431	2769	0	0	2276	1015	189	1				
Arrive On Green	0.05	1.00	0.00	0.00	0.75	0.75	0.11	0.11	0.00			
Sat Flow, veh/h	1697	3474	0	0	3098	1346	1770	11	1585			
Grp Volume(v), veh/h	31	588	0	0	891	182	155	0	0			
Grp Sat Flow(s),veh/h/ln	1697	1692	0	0	1509	1346	1782	0	1585			
Q Serve(g_s), s	0.4	0.0	0.0	0.0	12.4	4.6	10.2	0.0	0.0			
Cycle Q Clear(g_c), s	0.4	0.0	0.0	0.0	12.4	4.6	10.2	0.0	0.0			
Prop In Lane	1.00		0.00	0.00		1.00	0.99		1.00			
Lane Grp Cap(c), veh/h	431	2769	0	0	2276	1015	190	0				
V/C Ratio(X)	0.07	0.21	0.00	0.00	0.39	0.18	0.81	0.00				
Avail Cap(c_a), veh/h	492	2769	0	0	2276	1015	408	0				
HCM Platoon Ratio	2.00	2.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00			
Upstream Filter(I)	0.89	0.89	0.00	0.00	0.97	0.97	1.00	0.00	0.00			
Uniform Delay (d), s/veh	3.3	0.0	0.0	0.0	5.2	4.2	52.4	0.0	0.0			
Incr Delay (d2), s/veh	0.1	0.2	0.0	0.0	0.5	0.4	8.2	0.0	0.0			
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0			
%ile BackOfQ(50%),veh/ln	0.1	0.1	0.0	0.0	3.1	1.1	5.0	0.0	0.0			
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	3.4	0.2	0.0	0.0	5.6	4.6	60.6	0.0	0.0			
LnGrp LOS	A	A			A	A	E					
Approach Vol, veh/h		619			1073			155				
Approach Delay, s/veh		0.3			5.5			60.6				
Approach LOS		A			A			E				
Timer - Assigned Phs		2		4	5	6						
Phs Duration (G+Y+Rc), s		102.7		17.3	7.7	95.0						
Change Period (Y+Rc), s		4.5		4.5	4.5	4.5						
Max Green Setting (Gmax), s		83.5		27.5	7.5	71.5						
Max Q Clear Time (g_c+I1), s		2.0		12.2	2.4	14.4						
Green Ext Time (p_c), s		4.1		0.7	0.0	8.0						
Intersection Summary												
HCM 7th Control Delay, s/veh			8.4									
HCM 7th LOS			A									
Notes												
Unsignalized Delay for [NBR, SBR] is excluded from calculations of the approach delay and intersection delay.												

Timings
2: US-275 Northbound Ramps & Meigs St

2050 Total PM
02/13/2026

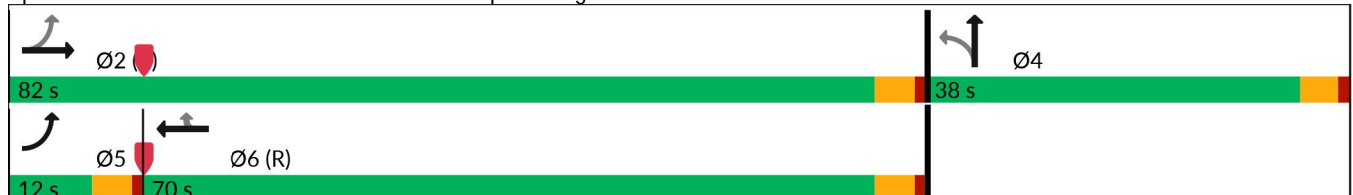


Lane Group	EBL	EBT	WBT	WBR	NBT	NBR
Lane Configurations	↘	↑↑	↑↑	↗	↘	↗
Traffic Volume (vph)	59	1052	1237	323	3	770
Future Volume (vph)	59	1052	1237	323	3	770
Turn Type	pm+pt	NA	NA	Perm	NA	Free
Protected Phases	5	2	6		4	
Permitted Phases	2			6		Free
Detector Phase	5	2	6	6	4	
Switch Phase						
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0	
Minimum Split (s)	9.5	22.5	22.5	22.5	22.5	
Total Split (s)	12.0	82.0	70.0	70.0	38.0	
Total Split (%)	10.0%	68.3%	58.3%	58.3%	31.7%	
Yellow Time (s)	3.5	3.5	3.5	3.5	3.5	
All-Red Time (s)	1.0	1.0	1.0	1.0	1.0	
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	
Total Lost Time (s)	4.5	4.5	4.5	4.5	4.5	
Lead/Lag	Lead		Lag	Lag		
Lead-Lag Optimize?	Yes		Yes	Yes		
Recall Mode	None	C-Max	C-Max	C-Max	None	

Intersection Summary

Cycle Length: 120
 Actuated Cycle Length: 120
 Offset: 50.4 (42%), Referenced to phase 2:EBTL and 6:WBT, Start of Green
 Natural Cycle: 65
 Control Type: Actuated-Coordinated

Splits and Phases: 2: US-275 Northbound Ramps & Meigs St



HCM 7th Signalized Intersection Summary
 2: US-275 Northbound Ramps & Meigs St

2050 Total PM
 02/13/2026



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↘	↗			↗	↘		↘	↗			
Traffic Volume (veh/h)	59	1052	0	0	1237	323	268	3	770	0	0	0
Future Volume (veh/h)	59	1052	0	0	1237	323	268	3	770	0	0	0
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0			
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00			
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00			
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00			
Work Zone On Approach		No			No			No				
Adj Sat Flow, veh/h/ln	1781	1781	0	0	1826	1826	1870	1870	1870			
Adj Flow Rate, veh/h	63	1131	0	0	1330	347	288	3	0			
Peak Hour Factor	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93			
Percent Heavy Veh, %	8	8	0	0	5	5	2	2	2			
Cap, veh/h	250	2499	0	0	2305	1028	329	3				
Arrive On Green	0.07	1.00	0.00	0.00	0.66	0.66	0.19	0.19	0.00			
Sat Flow, veh/h	1697	3474	0	0	3561	1547	1764	18	1585			
Grp Volume(v), veh/h	63	1131	0	0	1330	347	291	0	0			
Grp Sat Flow(s),veh/h/ln	1697	1692	0	0	1735	1547	1782	0	1585			
Q Serve(g_s), s	1.3	0.0	0.0	0.0	25.0	11.6	19.0	0.0	0.0			
Cycle Q Clear(g_c), s	1.3	0.0	0.0	0.0	25.0	11.6	19.0	0.0	0.0			
Prop In Lane	1.00		0.00	0.00		1.00	0.99		1.00			
Lane Grp Cap(c), veh/h	250	2499	0	0	2305	1028	333	0				
V/C Ratio(X)	0.25	0.45	0.00	0.00	0.58	0.34	0.87	0.00				
Avail Cap(c_a), veh/h	294	2499	0	0	2305	1028	498	0				
HCM Platoon Ratio	2.00	2.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00			
Upstream Filter(I)	0.09	0.09	0.00	0.00	0.93	0.93	1.00	0.00	0.00			
Uniform Delay (d), s/veh	8.5	0.0	0.0	0.0	11.0	8.7	47.4	0.0	0.0			
Incr Delay (d2), s/veh	0.0	0.1	0.0	0.0	1.0	0.8	11.0	0.0	0.0			
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0			
%ile BackOfQ(50%),veh/ln	0.4	0.0	0.0	0.0	8.5	3.6	9.5	0.0	0.0			
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	8.6	0.1	0.0	0.0	12.0	9.5	58.5	0.0	0.0			
LnGrp LOS	A	A			B	A	E					
Approach Vol, veh/h		1194			1677			291				
Approach Delay, s/veh		0.5			11.5			58.5				
Approach LOS		A			B			E				
Timer - Assigned Phs		2		4	5	6						
Phs Duration (G+Y+Rc), s		93.1		26.9	8.9	84.2						
Change Period (Y+Rc), s		4.5		4.5	4.5	4.5						
Max Green Setting (Gmax), s		77.5		33.5	7.5	65.5						
Max Q Clear Time (g_c+I1), s		2.0		21.0	3.3	27.0						
Green Ext Time (p_c), s		10.0		1.4	0.0	14.2						
Intersection Summary												
HCM 7th Control Delay, s/veh				11.6								
HCM 7th LOS				B								
Notes												
Unsignalized Delay for [NBR] is excluded from calculations of the approach delay and intersection delay.												

Intersection						
Int Delay, s/veh	2.4					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	W		T			T
Traffic Vol, veh/h	12	5	17	17	3	16
Future Vol, veh/h	12	5	17	17	3	16
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	83	83	83	83	83	83
Heavy Vehicles, %	2	2	3	3	2	2
Mvmt Flow	14	6	20	20	4	19

Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	57	31	0	0	41	0
Stage 1	31	-	-	-	-	-
Stage 2	27	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218	-
Pot Cap-1 Maneuver	950	1044	-	-	1568	-
Stage 1	992	-	-	-	-	-
Stage 2	996	-	-	-	-	-
Platoon blocked, %			-	-		
Mov Cap-1 Maneuver	948	1044	-	-	1568	-
Mov Cap-2 Maneuver	948	-	-	-	-	-
Stage 1	992	-	-	-	-	-
Stage 2	994	-	-	-	-	-

Approach	WB	NB	SB
HCM Ctrl Dly, s/v	8.77	0	1.15
HCM LOS	A		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	974	284
HCM Lane V/C Ratio	-	-	0.021	0.002
HCM Ctrl Dly (s/v)	-	-	8.8	7.3
HCM Lane LOS	-	-	A	A
HCM 95th %tile Q(veh)	-	-	0.1	0

Intersection						
Int Delay, s/veh	3.5					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	W		T			T
Traffic Vol, veh/h	34	11	27	31	13	29
Future Vol, veh/h	34	11	27	31	13	29
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	91	91	91	91	91	91
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	37	12	30	34	14	32

Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	107	47	0	0	64	0
Stage 1	47	-	-	-	-	-
Stage 2	60	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218	-
Pot Cap-1 Maneuver	890	1023	-	-	1539	-
Stage 1	976	-	-	-	-	-
Stage 2	962	-	-	-	-	-
Platoon blocked, %			-	-		
Mov Cap-1 Maneuver	882	1023	-	-	1539	-
Mov Cap-2 Maneuver	882	-	-	-	-	-
Stage 1	976	-	-	-	-	-
Stage 2	953	-	-	-	-	-

Approach	WB	NB	SB
HCM Ctrl Dly, s/v	9.17	0	2.28
HCM LOS	A		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	913	557
HCM Lane V/C Ratio	-	-	0.054	0.009
HCM Ctrl Dly (s/v)	-	-	9.2	7.4
HCM Lane LOS	-	-	A	A
HCM 95th %tile Q(veh)	-	-	0.2	0

Intersection						
Int Delay, s/veh	2.4					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	W		T			T
Traffic Vol, veh/h	14	6	20	20	3	18
Future Vol, veh/h	14	6	20	20	3	18
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	83	83	83	83	83	83
Heavy Vehicles, %	2	2	3	3	2	2
Mvmt Flow	17	7	24	24	4	22

Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	65	36	0	0	48	0
Stage 1	36	-	-	-	-	-
Stage 2	29	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218	-
Pot Cap-1 Maneuver	940	1036	-	-	1559	-
Stage 1	986	-	-	-	-	-
Stage 2	994	-	-	-	-	-
Platoon blocked, %			-	-	-	-
Mov Cap-1 Maneuver	938	1036	-	-	1559	-
Mov Cap-2 Maneuver	938	-	-	-	-	-
Stage 1	986	-	-	-	-	-
Stage 2	991	-	-	-	-	-

Approach	WB	NB	SB
HCM Ctrl Dly, s/v	8.82	0	1.04
HCM LOS	A		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	966	257
HCM Lane V/C Ratio	-	-	0.025	0.002
HCM Ctrl Dly (s/v)	-	-	8.8	7.3
HCM Lane LOS	-	-	A	A
HCM 95th %tile Q(veh)	-	-	0.1	0

Intersection						
Int Delay, s/veh	3.6					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	W		T			T
Traffic Vol, veh/h	39	13	31	36	15	33
Future Vol, veh/h	39	13	31	36	15	33
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	91	91	91	91	91	91
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	43	14	34	40	16	36

Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	123	54	0	0	74	0
Stage 1	54	-	-	-	-	-
Stage 2	69	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218	-
Pot Cap-1 Maneuver	872	1013	-	-	1526	-
Stage 1	969	-	-	-	-	-
Stage 2	954	-	-	-	-	-
Platoon blocked, %			-	-		
Mov Cap-1 Maneuver	862	1013	-	-	1526	-
Mov Cap-2 Maneuver	862	-	-	-	-	-
Stage 1	969	-	-	-	-	-
Stage 2	943	-	-	-	-	-

Approach	WB	NB	SB
HCM Ctrl Dly, s/v	9.29	0	2.31
HCM LOS	A		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	896	563
HCM Lane V/C Ratio	-	-	0.064	0.011
HCM Ctrl Dly (s/v)	-	-	9.3	7.4
HCM Lane LOS	-	-	A	A
HCM 95th %tile Q(veh)	-	-	0.2	0

Intersection						
Int Delay, s/veh	3.1					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	Y		T		T	T
Traffic Vol, veh/h	77	19	64	42	7	143
Future Vol, veh/h	77	19	64	42	7	143
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	150	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	83	83	83	83	83	83
Heavy Vehicles, %	2	2	3	3	2	2
Mvmt Flow	93	23	77	51	8	172

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	292	102	0	0	128
Stage 1	102	-	-	-	-
Stage 2	189	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	699	953	-	-	1458
Stage 1	922	-	-	-	-
Stage 2	843	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	695	953	-	-	1458
Mov Cap-2 Maneuver	695	-	-	-	-
Stage 1	922	-	-	-	-
Stage 2	838	-	-	-	-

Approach	WB	NB	SB
HCM Ctrl Dly, s/v	10.82	0	0.35
HCM LOS	B		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	734	1458
HCM Lane V/C Ratio	-	-	0.157	0.006
HCM Ctrl Dly (s/v)	-	-	10.8	7.5
HCM Lane LOS	-	-	B	A
HCM 95th %tile Q(veh)	-	-	0.6	0

Intersection						
Int Delay, s/veh	2.9					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	↘		↗		↘	↗
Traffic Vol, veh/h	80	21	162	101	28	115
Future Vol, veh/h	80	21	162	101	28	115
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	150	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	91	91	91	91	91	91
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	88	23	178	111	31	126

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	421	234	0	0	289
Stage 1	234	-	-	-	-
Stage 2	188	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	589	806	-	-	1273
Stage 1	805	-	-	-	-
Stage 2	844	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	575	806	-	-	1273
Mov Cap-2 Maneuver	575	-	-	-	-
Stage 1	805	-	-	-	-
Stage 2	824	-	-	-	-

Approach	WB	NB	SB
HCM Ctrl Dly, s/v	12.2	0	1.55
HCM LOS	B		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	611	1273
HCM Lane V/C Ratio	-	-	0.182	0.024
HCM Ctrl Dly (s/v)	-	-	12.2	7.9
HCM Lane LOS	-	-	B	A
HCM 95th %tile Q(veh)	-	-	0.7	0.1

Intersection												
Int Delay, s/veh	4.5											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		+			+			+	+		+	
Traffic Vol, veh/h	1	2	13	70	0	8	27	50	52	12	40	0
Future Vol, veh/h	1	2	13	70	0	8	27	50	52	12	40	0
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	0	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	83	92	83	92	83	83	83	83	92
Heavy Vehicles, %	2	2	2	2	2	2	2	3	3	2	2	2
Mvmt Flow	1	2	14	84	0	10	29	60	63	14	48	0

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	196	259	48	197	196	60	48	0	0	123	0	0
Stage 1	77	77	-	119	119	-	-	-	-	-	-	-
Stage 2	119	182	-	78	77	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	763	646	1021	762	699	1005	1559	-	-	1464	-	-
Stage 1	932	831	-	886	797	-	-	-	-	-	-	-
Stage 2	886	749	-	931	831	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	733	626	1021	726	678	1005	1559	-	-	1464	-	-
Mov Cap-2 Maneuver	733	626	-	726	678	-	-	-	-	-	-	-
Stage 1	922	822	-	867	781	-	-	-	-	-	-	-
Stage 2	859	734	-	906	822	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Ctrl Dly, s/v	8.97		10.51		1.42		1.73	
HCM LOS	A		B					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	590	-	-	925	747	415	-
HCM Lane V/C Ratio	0.019	-	-	0.019	0.126	0.01	-
HCM Ctrl Dly (s/v)	7.4	0	-	9	10.5	7.5	0
HCM Lane LOS	A	A	-	A	B	A	A
HCM 95th %tile Q(veh)	0.1	-	-	0.1	0.4	0	-

Intersection												
Int Delay, s/veh	5.2											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕	↕		↕	
Traffic Vol, veh/h	2	4	20	110	3	33	43	278	98	43	164	0
Future Vol, veh/h	2	4	20	110	3	33	43	278	98	43	164	0
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	0	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	91	92	91	92	91	91	91	91	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	2	4	22	121	3	36	47	305	108	47	180	0

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	675	781	180	676	674	305	180	0	0	413	0	0
Stage 1	275	275	-	399	399	-	-	-	-	-	-	-
Stage 2	401	507	-	277	275	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	368	326	863	367	376	734	1395	-	-	1146	-	-
Stage 1	731	683	-	627	602	-	-	-	-	-	-	-
Stage 2	626	539	-	729	683	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	316	297	863	322	343	734	1395	-	-	1146	-	-
Mov Cap-2 Maneuver	316	297	-	322	343	-	-	-	-	-	-	-
Stage 1	698	651	-	599	576	-	-	-	-	-	-	-
Stage 2	565	516	-	674	651	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Ctrl Dly, s/v	11.24		22.01		0.78		1.72	
HCM LOS	B		C					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	239	-	-	605	370	374	-	-
HCM Lane V/C Ratio	0.033	-	-	0.047	0.434	0.041	-	-
HCM Ctrl Dly (s/v)	7.7	0	-	11.2	22	8.3	0	-
HCM Lane LOS	A	A	-	B	C	A	A	-
HCM 95th %tile Q(veh)	0.1	-	-	0.1	2.1	0.1	-	-

Intersection				
Intersection Delay, s/veh	4.6			
Intersection LOS	A			
Approach	EB	WB	NB	SB
Entry Lanes	1	1	1	1
Conflicting Circle Lanes	1	1	1	1
Adj Approach Flow, veh/h	17	185	231	218
Demand Flow Rate, veh/h	17	189	238	222
Vehicles Circulating, veh/h	385	147	22	193
Vehicles Exiting, veh/h	30	113	380	143
Ped Vol Crossing Leg, #/h	0	0	0	0
Ped Cap Adj	1.000	1.000	1.000	1.000
Approach Delay, s/veh	4.0	4.5	4.2	5.0
Approach LOS	A	A	A	A
Lane	Left	Left	Left	Left
Designated Moves	LTR	LTR	LTR	LTR
Assumed Moves	LTR	LTR	LTR	LTR
RT Channelized				
Lane Util	1.000	1.000	1.000	1.000
Follow-Up Headway, s	2.609	2.609	2.609	2.609
Critical Headway, s	4.976	4.976	4.976	4.976
A (Intercept)	1380	1380	1380	1380
B (Slope)	1.02e-3	1.02e-3	1.02e-3	1.02e-3
Entry Flow, veh/h	17	189	238	222
Cap Entry Lane, veh/h	932	1188	1349	1133
Entry HV Adj Factor	0.998	0.979	0.969	0.982
Flow Entry, veh/h	17	185	231	218
Cap Entry, veh/h	930	1163	1307	1113
V/C Ratio	0.018	0.159	0.176	0.196
Control Delay, s/veh	4.0	4.5	4.2	5.0
LOS	A	A	A	A
95th %tile Queue, veh	0	1	1	1

Timings
3: Meigs St & 252nd St

2050 Total AM
02/13/2026

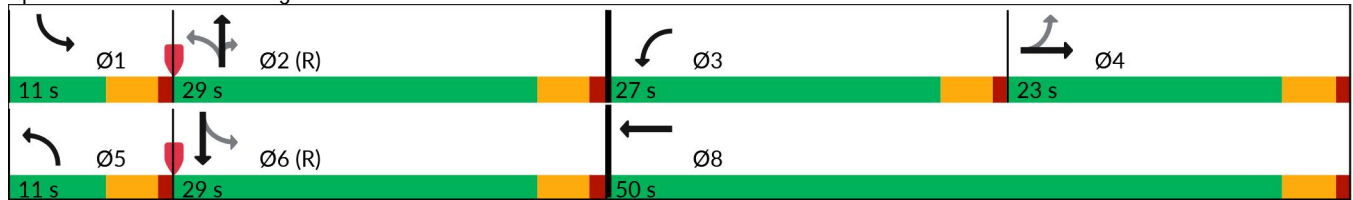


Lane Group	EBL	EBT	WBL	WBT	NBL	NBT	NBR	SBL	SBT
Lane Configurations		↔	↗	↘	↗	↗	↗	↗	↘
Traffic Volume (vph)	1	2	133	0	27	94	74	16	165
Future Volume (vph)	1	2	133	0	27	94	74	16	165
Turn Type	Perm	NA	Prot	NA	pm+pt	NA	Perm	pm+pt	NA
Protected Phases		4	3	8	5	2		1	6
Permitted Phases	4				2		2	6	
Detector Phase	4	4	3	8	5	2	2	1	6
Switch Phase									
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0
Minimum Split (s)	22.5	22.5	22.5	22.5	9.5	22.5	22.5	9.5	22.5
Total Split (s)	23.0	23.0	27.0	50.0	11.0	29.0	29.0	11.0	29.0
Total Split (%)	25.6%	25.6%	30.0%	55.6%	12.2%	32.2%	32.2%	12.2%	32.2%
Yellow Time (s)	3.5	3.5	3.5	3.5	3.5	3.5	3.5	3.5	3.5
All-Red Time (s)	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0
Lost Time Adjust (s)		0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)		4.5	4.5	4.5	4.5	4.5	4.5	4.5	4.5
Lead/Lag	Lag	Lag	Lead		Lead	Lag	Lag	Lead	Lag
Lead-Lag Optimize?	Yes	Yes	Yes		Yes	Yes	Yes	Yes	Yes
Recall Mode	None	None	Max	None	None	C-Max	C-Max	None	C-Max

Intersection Summary

Cycle Length: 90
 Actuated Cycle Length: 90
 Offset: 0 (0%), Referenced to phase 2:NBTL and 6:SBTL, Start of Green
 Natural Cycle: 80
 Control Type: Actuated-Coordinated

Splits and Phases: 3: Meigs St & 252nd St



HCM 7th Signalized Intersection Summary
 3: Meigs St & 252nd St

2050 Total AM
 02/13/2026



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		+		+	+		+	+	+	+	+	+
Traffic Volume (veh/h)	1	2	13	133	0	21	27	94	74	16	165	0
Future Volume (veh/h)	1	2	13	133	0	21	27	94	74	16	165	0
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1856	1856	1870	1870	1870
Adj Flow Rate, veh/h	1	2	14	160	0	25	29	113	89	19	199	0
Peak Hour Factor	0.92	0.92	0.92	0.83	0.92	0.83	0.92	0.83	0.83	0.83	0.83	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	3	3	2	2	2
Cap, veh/h	45	8	48	445	0	533	633	915	775	660	908	0
Arrive On Green	0.04	0.04	0.04	0.25	0.00	0.34	0.03	0.49	0.49	0.02	0.49	0.00
Sat Flow, veh/h	66	218	1325	1781	0	1585	1781	1856	1572	1781	1870	0
Grp Volume(v), veh/h	17	0	0	160	0	25	29	113	89	19	199	0
Grp Sat Flow(s),veh/h/ln	1609	0	0	1781	0	1585	1781	1856	1572	1781	1870	0
Q Serve(g_s), s	0.0	0.0	0.0	6.7	0.0	1.0	0.7	3.0	2.7	0.5	5.5	0.0
Cycle Q Clear(g_c), s	0.9	0.0	0.0	6.7	0.0	1.0	0.7	3.0	2.7	0.5	5.5	0.0
Prop In Lane	0.06		0.82	1.00		1.00	1.00		1.00	1.00		0.00
Lane Grp Cap(c), veh/h	100	0	0	445	0	533	633	915	775	660	908	0
V/C Ratio(X)	0.17	0.00	0.00	0.36	0.00	0.05	0.05	0.12	0.11	0.03	0.22	0.00
Avail Cap(c_a), veh/h	372	0	0	445	0	801	710	915	775	752	908	0
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	0.00	1.00	0.00	1.00	0.98	0.98	0.98	1.00	1.00	0.00
Uniform Delay (d), s/veh	42.2	0.0	0.0	27.8	0.0	20.2	11.0	12.3	12.3	11.1	13.3	0.0
Incr Delay (d2), s/veh	0.8	0.0	0.0	2.2	0.0	0.0	0.0	0.3	0.3	0.0	0.6	0.0
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.4	0.0	0.0	2.9	0.0	0.3	0.3	1.2	0.9	0.2	2.4	0.0
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	43.0	0.0	0.0	30.1	0.0	20.2	11.0	12.6	12.6	11.1	13.9	0.0
LnGrp LOS	D			C		C	B	B	B	B	B	
Approach Vol, veh/h		17			185			231			218	
Approach Delay, s/veh		43.0			28.7			12.4			13.7	
Approach LOS		D			C			B			B	
Timer - Assigned Phs	1	2	3	4	5	6		8				
Phs Duration (G+Y+Rc), s	6.4	48.9	27.0	7.8	7.1	48.2		34.8				
Change Period (Y+Rc), s	4.5	4.5	4.5	4.5	4.5	4.5		4.5				
Max Green Setting (Gmax), s	6.5	24.5	22.5	18.5	6.5	24.5		45.5				
Max Q Clear Time (g_c+I1), s	2.5	5.0	8.7	2.9	2.7	7.5		3.0				
Green Ext Time (p_c), s	0.0	0.7	0.3	0.0	0.0	0.9		0.1				
Intersection Summary												
HCM 7th Control Delay, s/veh			18.3									
HCM 7th LOS			B									

Intersection				
Intersection Delay, s/veh	7.7			
Intersection LOS	A			
Approach	EB	WB	NB	SB
Entry Lanes	1	1	1	1
Conflicting Circle Lanes	1	1	1	1
Adj Approach Flow, veh/h	28	212	669	328
Demand Flow Rate, veh/h	28	216	683	334
Vehicles Circulating, veh/h	501	504	68	218
Vehicles Exiting, veh/h	51	247	461	502
Ped Vol Crossing Leg, #/h	0	0	0	0
Ped Cap Adj	1.000	1.000	1.000	1.000
Approach Delay, s/veh	4.7	7.3	8.7	6.3
Approach LOS	A	A	A	A
Lane	Left	Left	Left	Left
Designated Moves	LTR	LTR	LTR	LTR
Assumed Moves	LTR	LTR	LTR	LTR
RT Channelized				
Lane Util	1.000	1.000	1.000	1.000
Follow-Up Headway, s	2.609	2.609	2.609	2.609
Critical Headway, s	4.976	4.976	4.976	4.976
A (Intercept)	1380	1380	1380	1380
B (Slope)	1.02e-3	1.02e-3	1.02e-3	1.02e-3
Entry Flow, veh/h	28	216	683	334
Cap Entry Lane, veh/h	828	825	1287	1105
Entry HV Adj Factor	0.997	0.981	0.980	0.981
Flow Entry, veh/h	28	212	669	328
Cap Entry, veh/h	825	810	1261	1084
V/C Ratio	0.034	0.262	0.531	0.302
Control Delay, s/veh	4.7	7.3	8.7	6.3
LOS	A	A	A	A
95th %tile Queue, veh	0	1	3	1

Timings
3: Meigs St & 252nd St

2050 Total PM
02/13/2026



Lane Group	EBL	EBT	WBL	WBT	NBL	NBT	NBR	SBL	SBT
Lane Configurations		↔	↗	↘	↗	↘	↗	↗	↘
Traffic Volume (vph)	2	4	151	3	43	409	163	56	246
Future Volume (vph)	2	4	151	3	43	409	163	56	246
Turn Type	Perm	NA	Perm	NA	pm+pt	NA	Perm	pm+pt	NA
Protected Phases		4		8	5	2		1	6
Permitted Phases	4		8		2		2	6	
Detector Phase	4	4	8	8	5	2	2	1	6
Switch Phase									
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0
Minimum Split (s)	22.5	22.5	22.5	22.5	9.5	22.5	22.5	9.5	22.5
Total Split (s)	30.0	30.0	30.0	30.0	11.0	49.0	49.0	11.0	49.0
Total Split (%)	33.3%	33.3%	33.3%	33.3%	12.2%	54.4%	54.4%	12.2%	54.4%
Yellow Time (s)	3.5	3.5	3.5	3.5	3.5	3.5	3.5	3.5	3.5
All-Red Time (s)	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0
Lost Time Adjust (s)		0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)		4.5	4.5	4.5	4.5	4.5	4.5	4.5	4.5
Lead/Lag					Lead	Lag	Lag	Lead	Lag
Lead-Lag Optimize?					Yes	Yes	Yes	Yes	Yes
Recall Mode	None	None	None	None	None	C-Max	C-Max	None	C-Max

Intersection Summary

Cycle Length: 90
 Actuated Cycle Length: 90
 Offset: 0 (0%), Referenced to phase 2:NBTL and 6:SBTL, Start of Green
 Natural Cycle: 55
 Control Type: Actuated-Coordinated

Splits and Phases: 3: Meigs St & 252nd St



HCM 7th Signalized Intersection Summary
 3: Meigs St & 252nd St

2050 Total PM
 02/13/2026



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		+		+	+		+	+	+	+	+	+
Traffic Volume (veh/h)	2	4	20	151	3	41	43	409	163	56	246	0
Future Volume (veh/h)	2	4	20	151	3	41	43	409	163	56	246	0
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	2	4	22	164	3	45	47	445	177	61	267	0
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	47	44	177	278	14	208	836	1249	1059	610	1259	0
Arrive On Green	0.14	0.14	0.14	0.14	0.14	0.14	0.04	0.67	0.67	0.04	0.67	0.00
Sat Flow, veh/h	32	317	1279	1385	100	1500	1781	1870	1585	1781	1870	0
Grp Volume(v), veh/h	28	0	0	164	0	48	47	445	177	61	267	0
Grp Sat Flow(s),veh/h/ln	1628	0	0	1385	0	1600	1781	1870	1585	1781	1870	0
Q Serve(g_s), s	0.0	0.0	0.0	8.6	0.0	2.4	0.7	9.3	3.8	0.9	4.9	0.0
Cycle Q Clear(g_c), s	1.3	0.0	0.0	10.0	0.0	2.4	0.7	9.3	3.8	0.9	4.9	0.0
Prop In Lane	0.07		0.79	1.00		0.94	1.00		1.00	1.00		0.00
Lane Grp Cap(c), veh/h	269	0	0	278	0	222	836	1249	1059	610	1259	0
V/C Ratio(X)	0.10	0.00	0.00	0.59	0.00	0.22	0.06	0.36	0.17	0.10	0.21	0.00
Avail Cap(c_a), veh/h	500	0	0	478	0	453	897	1249	1059	661	1259	0
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	0.00	1.00	0.00	1.00	0.86	0.86	0.86	1.00	1.00	0.00
Uniform Delay (d), s/veh	34.0	0.0	0.0	37.5	0.0	34.4	4.1	6.5	5.6	4.4	5.6	0.0
Incr Delay (d2), s/veh	0.2	0.0	0.0	2.0	0.0	0.5	0.0	0.7	0.3	0.1	0.4	0.0
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.5	0.0	0.0	3.5	0.0	0.9	0.2	3.0	1.0	0.2	1.6	0.0
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	34.1	0.0	0.0	39.5	0.0	34.9	4.1	7.2	5.9	4.5	6.0	0.0
LnGrp LOS	C			D		C	A	A	A	A	A	
Approach Vol, veh/h		28			212			669			328	
Approach Delay, s/veh		34.1			38.5			6.6			5.7	
Approach LOS		C			D			A			A	
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	8.4	64.6		17.0	8.0	65.1		17.0				
Change Period (Y+Rc), s	4.5	4.5		4.5	4.5	4.5		4.5				
Max Green Setting (Gmax), s	6.5	44.5		25.5	6.5	44.5		25.5				
Max Q Clear Time (g_c+I1), s	2.9	11.3		3.3	2.7	6.9		12.0				
Green Ext Time (p_c), s	0.0	3.3		0.1	0.0	1.5		0.5				
Intersection Summary												
HCM 7th Control Delay, s/veh				12.5								
HCM 7th LOS				B								

Intersection												
Int Delay, s/veh	1											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		+			+			+			+	
Traffic Vol, veh/h	0	0	0	1	0	3	0	15	2	1	19	0
Future Vol, veh/h	0	0	0	1	0	3	0	15	2	1	19	0
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	79	92	79	92	79	79	79	79	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	0	0	0	1	0	4	0	19	3	1	24	0

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	46	48	24	47	47	20	24	0	0	22	0	0
Stage 1	27	27	-	20	20	-	-	-	-	-	-	-
Stage 2	19	22	-	27	27	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	956	843	1052	954	845	1057	1591	-	-	1594	-	-
Stage 1	991	873	-	998	878	-	-	-	-	-	-	-
Stage 2	1000	877	-	991	873	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	952	843	1052	954	844	1057	1591	-	-	1594	-	-
Mov Cap-2 Maneuver	952	843	-	954	844	-	-	-	-	-	-	-
Stage 1	990	872	-	998	878	-	-	-	-	-	-	-
Stage 2	996	877	-	990	872	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Ctrl Dly, s/v	0	8.51	0	0.36
HCM LOS	A	A		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	1591	-	-	-	1029	90	-
HCM Lane V/C Ratio	-	-	-	-	0.005	0.001	-
HCM Ctrl Dly (s/v)	0	-	-	0	8.5	7.3	0
HCM Lane LOS	A	-	-	A	A	A	A
HCM 95th %tile Q(veh)	0	-	-	-	0	0	-

Intersection												
Int Delay, s/veh	3.2											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		+			+			+			+	
Traffic Vol, veh/h	0	0	0	7	0	11	0	33	5	22	22	0
Future Vol, veh/h	0	0	0	7	0	11	0	33	5	22	22	0
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	91	92	91	92	91	91	91	91	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	0	0	0	8	0	12	0	36	5	24	24	0

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	109	114	24	112	112	39	24	0	0	42	0	0
Stage 1	73	73	-	39	39	-	-	-	-	-	-	-
Stage 2	36	42	-	73	73	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	870	776	1052	866	779	1033	1591	-	-	1567	-	-
Stage 1	937	835	-	976	862	-	-	-	-	-	-	-
Stage 2	979	860	-	937	835	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	846	764	1052	853	766	1033	1591	-	-	1567	-	-
Mov Cap-2 Maneuver	846	764	-	853	766	-	-	-	-	-	-	-
Stage 1	922	822	-	976	862	-	-	-	-	-	-	-
Stage 2	968	860	-	922	822	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Ctrl Dly, s/v	0		8.85		0		3.67	
HCM LOS	A		A					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	1591	-	-	-	954	900	-
HCM Lane V/C Ratio	-	-	-	-	0.021	0.015	-
HCM Ctrl Dly (s/v)	0	-	-	0	8.9	7.3	0
HCM Lane LOS	A	-	-	A	A	A	A
HCM 95th %tile Q(veh)	0	-	-	-	0.1	0	-

Intersection												
Int Delay, s/veh	0.9											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		+			+			+			+	
Traffic Vol, veh/h	0	0	0	1	0	3	0	17	2	1	22	0
Future Vol, veh/h	0	0	0	1	0	3	0	17	2	1	22	0
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	79	92	79	92	79	79	79	79	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	0	0	0	1	0	4	0	22	3	1	28	0

Major/Minor	Minor2		Minor1		Major1			Major2				
Conflicting Flow All	52	54	28	53	53	23	28	0	0	24	0	0
Stage 1	30	30	-	23	23	-	-	-	-	-	-	-
Stage 2	22	24	-	30	30	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	947	837	1047	945	838	1054	1586	-	-	1591	-	-
Stage 1	986	870	-	995	876	-	-	-	-	-	-	-
Stage 2	997	875	-	986	870	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	943	836	1047	945	837	1054	1586	-	-	1591	-	-
Mov Cap-2 Maneuver	943	836	-	945	837	-	-	-	-	-	-	-
Stage 1	986	869	-	995	876	-	-	-	-	-	-	-
Stage 2	993	875	-	986	869	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Ctrl Dly, s/v	0		8.53		0		0.32	
HCM LOS	A		A					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	1586	-	-	-	1024	78	-
HCM Lane V/C Ratio	-	-	-	-	0.005	0.001	-
HCM Ctrl Dly (s/v)	0	-	-	0	8.5	7.3	0
HCM Lane LOS	A	-	-	A	A	A	A
HCM 95th %tile Q(veh)	0	-	-	-	0	0	-

Intersection												
Int Delay, s/veh	3.3											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		+			+			+			+	
Traffic Vol, veh/h	1	0	0	8	0	13	1	38	6	25	25	0
Future Vol, veh/h	1	0	0	8	0	13	1	38	6	25	25	0
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	91	92	91	92	91	91	91	91	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	1	0	0	9	0	14	1	42	7	27	27	0

Major/Minor	Minor2		Minor1		Major1			Major2				
Conflicting Flow All	126	133	27	130	130	45	27	0	0	48	0	0
Stage 1	82	82	-	47	47	-	-	-	-	-	-	-
Stage 2	44	51	-	82	82	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	847	758	1048	843	761	1025	1586	-	-	1559	-	-
Stage 1	926	826	-	966	856	-	-	-	-	-	-	-
Stage 2	970	853	-	926	826	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	820	744	1048	827	747	1025	1586	-	-	1559	-	-
Mov Cap-2 Maneuver	820	744	-	827	747	-	-	-	-	-	-	-
Stage 1	909	812	-	966	855	-	-	-	-	-	-	-
Stage 2	956	852	-	909	812	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Ctrl Dly, s/v	9.4		8.93		0.16		3.68	
HCM LOS	A		A					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	39	-	-	820	939	900	-
HCM Lane V/C Ratio	0.001	-	-	0.001	0.025	0.018	-
HCM Ctrl Dly (s/v)	7.3	0	-	9.4	8.9	7.4	0
HCM Lane LOS	A	A	-	A	A	A	A
HCM 95th %tile Q(veh)	0	-	-	0	0.1	0.1	-

Intersection												
Int Delay, s/veh	6.4											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		+			+			+			+	
Traffic Vol, veh/h	0	0	0	26	0	78	0	17	11	27	22	0
Future Vol, veh/h	0	0	0	26	0	78	0	17	11	27	22	0
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	79	92	79	92	79	79	79	79	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	0	0	0	33	0	99	0	22	14	34	28	0

Major/Minor	Minor2		Minor1		Major1			Major2				
Conflicting Flow All	118	132	28	125	125	28	28	0	0	35	0	0
Stage 1	96	96	-	28	28	-	-	-	-	-	-	-
Stage 2	22	35	-	96	96	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	858	759	1047	849	766	1047	1586	-	-	1576	-	-
Stage 1	910	815	-	989	871	-	-	-	-	-	-	-
Stage 2	997	865	-	910	815	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	760	742	1047	831	749	1047	1586	-	-	1576	-	-
Mov Cap-2 Maneuver	760	742	-	831	749	-	-	-	-	-	-	-
Stage 1	890	797	-	989	871	-	-	-	-	-	-	-
Stage 2	903	865	-	890	797	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Ctrl Dly, s/v	0		9.23		0		4.04	
HCM LOS	A		A					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	1586	-	-	-	983	992	-
HCM Lane V/C Ratio	-	-	-	-	0.134	0.022	-
HCM Ctrl Dly (s/v)	0	-	-	0	9.2	7.3	0
HCM Lane LOS	A	-	-	A	A	A	A
HCM 95th %tile Q(veh)	0	-	-	-	0.5	0.1	-

Intersection												
Int Delay, s/veh	5.7											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		+			+			+			+	
Traffic Vol, veh/h	1	0	0	24	0	62	1	38	32	103	25	0
Future Vol, veh/h	1	0	0	24	0	62	1	38	32	103	25	0
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	91	92	91	92	91	91	91	91	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	1	0	0	26	0	68	1	42	35	113	27	0

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	298	333	27	315	315	59	27	0	0	77	0	0
Stage 1	254	254	-	62	62	-	-	-	-	-	-	-
Stage 2	44	79	-	254	254	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	654	587	1048	637	600	1006	1586	-	-	1522	-	-
Stage 1	750	697	-	950	844	-	-	-	-	-	-	-
Stage 2	970	829	-	750	697	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	564	542	1048	589	555	1006	1586	-	-	1522	-	-
Mov Cap-2 Maneuver	564	542	-	589	555	-	-	-	-	-	-	-
Stage 1	694	644	-	949	843	-	-	-	-	-	-	-
Stage 2	904	829	-	694	644	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB			
HCM Ctrl Dly, s/v	11.4		9.83		0.1		6.08			
HCM LOS	B		A							

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	23	-	-	564	840	1448	-	-
HCM Lane V/C Ratio	0.001	-	-	0.002	0.113	0.074	-	-
HCM Ctrl Dly (s/v)	7.3	0	-	11.4	9.8	7.6	0	-
HCM Lane LOS	A	A	-	B	A	A	A	-
HCM 95th %tile Q(veh)	0	-	-	0	0.4	0.2	-	-

Intersection												
Int Delay, s/veh	1.5											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		+			+			+			+	
Traffic Vol, veh/h	0	0	0	1	0	17	0	62	3	9	55	0
Future Vol, veh/h	0	0	0	1	0	17	0	62	3	9	55	0
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	0	0	0	1	0	18	0	67	3	10	60	0

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	147	150	60	148	148	69	60	0	0	71	0	0
Stage 1	79	79	-	69	69	-	-	-	-	-	-	-
Stage 2	67	71	-	79	79	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	822	742	1006	820	743	994	1544	-	-	1530	-	-
Stage 1	929	829	-	941	837	-	-	-	-	-	-	-
Stage 2	943	836	-	929	829	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	801	737	1006	814	738	994	1544	-	-	1530	-	-
Mov Cap-2 Maneuver	801	737	-	814	738	-	-	-	-	-	-	-
Stage 1	923	823	-	941	837	-	-	-	-	-	-	-
Stage 2	925	836	-	923	823	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Ctrl Dly, s/v	0		8.74		0		1.04	
HCM LOS	A		A					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	1544	-	-	-	982	253	-
HCM Lane V/C Ratio	-	-	-	0.02	0.006	-	-
HCM Ctrl Dly (s/v)	0	-	-	0	8.7	7.4	0
HCM Lane LOS	A	-	-	A	A	A	A
HCM 95th %tile Q(veh)	0	-	-	-	0.1	0	-

Intersection												
Int Delay, s/veh	2.8											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		+			+			+			+	
Traffic Vol, veh/h	1	0	0	10	0	36	1	108	7	51	90	0
Future Vol, veh/h	1	0	0	10	0	36	1	108	7	51	90	0
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	1	0	0	11	0	39	1	117	8	55	98	0

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	328	336	98	332	332	121	98	0	0	125	0	0
Stage 1	209	209	-	123	123	-	-	-	-	-	-	-
Stage 2	120	127	-	209	209	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	625	585	958	621	588	930	1495	-	-	1462	-	-
Stage 1	793	729	-	881	794	-	-	-	-	-	-	-
Stage 2	885	791	-	793	729	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	574	561	958	596	564	930	1495	-	-	1462	-	-
Mov Cap-2 Maneuver	574	561	-	596	564	-	-	-	-	-	-	-
Stage 1	762	700	-	880	793	-	-	-	-	-	-	-
Stage 2	847	790	-	762	700	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Ctrl Dly, s/v	11.28		9.62		0.06		2.73	
HCM LOS	B		A					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	15	-	-	574	829	651	-	-
HCM Lane V/C Ratio	0.001	-	-	0.002	0.06	0.038	-	-
HCM Ctrl Dly (s/v)	7.4	0	-	11.3	9.6	7.6	0	-
HCM Lane LOS	A	A	-	B	A	A	A	-
HCM 95th %tile Q(veh)	0	-	-	0	0.2	0.1	-	-

Intersection												
Int Delay, s/veh	4.9											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		+			+			+			+	
Traffic Vol, veh/h	0	0	0	26	0	92	0	62	12	35	55	0
Future Vol, veh/h	0	0	0	26	0	92	0	62	12	35	55	0
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	0	0	0	28	0	100	0	67	13	38	60	0

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	203	216	60	210	210	74	60	0	0	80	0	0
Stage 1	136	136	-	74	74	-	-	-	-	-	-	-
Stage 2	67	80	-	136	136	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	755	682	1006	747	687	988	1544	-	-	1517	-	-
Stage 1	867	784	-	935	833	-	-	-	-	-	-	-
Stage 2	943	828	-	867	784	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	661	664	1006	728	669	988	1544	-	-	1517	-	-
Mov Cap-2 Maneuver	661	664	-	728	669	-	-	-	-	-	-	-
Stage 1	845	764	-	935	833	-	-	-	-	-	-	-
Stage 2	847	828	-	845	764	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Ctrl Dly, s/v	0	9.57	0	2.89
HCM LOS	A	A		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	1544	-	-	-	916	700	-
HCM Lane V/C Ratio	-	-	-	-	0.14	0.025	-
HCM Ctrl Dly (s/v)	0	-	-	0	9.6	7.4	0
HCM Lane LOS	A	-	-	A	A	A	A
HCM 95th %tile Q(veh)	0	-	-	-	0.5	0.1	-

Intersection												
Int Delay, s/veh	4.8											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		+			+			+			+	
Traffic Vol, veh/h	1	0	0	26	0	85	1	108	33	129	90	0
Future Vol, veh/h	1	0	0	26	0	85	1	108	33	129	90	0
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	1	0	0	28	0	92	1	117	36	140	98	0

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	498	534	98	516	516	135	98	0	0	153	0	0
Stage 1	378	378	-	138	138	-	-	-	-	-	-	-
Stage 2	120	155	-	378	378	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	483	452	958	470	463	913	1495	-	-	1427	-	-
Stage 1	643	615	-	866	783	-	-	-	-	-	-	-
Stage 2	885	769	-	643	615	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	389	405	958	421	415	913	1495	-	-	1427	-	-
Mov Cap-2 Maneuver	389	405	-	421	415	-	-	-	-	-	-	-
Stage 1	577	551	-	865	782	-	-	-	-	-	-	-
Stage 2	795	768	-	577	551	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Ctrl Dly, s/v	14.29		11.04		0.05		4.59	
HCM LOS	B		B					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	12	-	-	389	717	1060	-
HCM Lane V/C Ratio	0.001	-	-	0.003	0.168	0.098	-
HCM Ctrl Dly (s/v)	7.4	0	-	14.3	11	7.8	0
HCM Lane LOS	A	A	-	B	B	A	A
HCM 95th %tile Q(veh)	0	-	-	0	0.6	0.3	-

Intersection						
Int Delay, s/veh	5.8					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	↘		↗		↘	↗
Traffic Vol, veh/h	125	25	39	44	9	25
Future Vol, veh/h	125	25	39	44	9	25
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	150	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	136	27	42	48	10	27

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	113	66	0	0	90
Stage 1	66	-	-	-	-
Stage 2	47	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	884	997	-	-	1505
Stage 1	956	-	-	-	-
Stage 2	976	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	878	997	-	-	1505
Mov Cap-2 Maneuver	878	-	-	-	-
Stage 1	956	-	-	-	-
Stage 2	969	-	-	-	-

Approach	WB	NB	SB
HCM Ctrl Dly, s/v	9.91	0	1.96
HCM LOS	A		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	896	1505
HCM Lane V/C Ratio	-	-	0.182	0.007
HCM Ctrl Dly (s/v)	-	-	9.9	7.4
HCM Lane LOS	-	-	A	A
HCM 95th %tile Q(veh)	-	-	0.7	0

Intersection						
Int Delay, s/veh	3.4					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	Y		T		Y	T
Traffic Vol, veh/h	82	16	52	131	26	61
Future Vol, veh/h	82	16	52	131	26	61
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	150	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	89	17	57	142	28	66

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	251	128	0	0	199
Stage 1	128	-	-	-	-
Stage 2	123	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	738	922	-	-	1373
Stage 1	898	-	-	-	-
Stage 2	903	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	723	922	-	-	1373
Mov Cap-2 Maneuver	723	-	-	-	-
Stage 1	898	-	-	-	-
Stage 2	884	-	-	-	-

Approach	WB	NB	SB
HCM Ctrl Dly, s/v	10.6	0	2.29
HCM LOS	B		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	749	1373
HCM Lane V/C Ratio	-	-	0.142	0.021
HCM Ctrl Dly (s/v)	-	-	10.6	7.7
HCM Lane LOS	-	-	B	A
HCM 95th %tile Q(veh)	-	-	0.5	0.1

Intersection												
Int Delay, s/veh	5.3											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		+			+		↑	↑	↑	↑	↑	
Traffic Vol, veh/h	1	0	14	125	0	25	12	72	44	9	56	2
Future Vol, veh/h	1	0	14	125	0	25	12	72	44	9	56	2
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	150	-	150	150	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	1	0	15	136	0	27	13	78	48	10	61	2

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	186	234	62	185	187	78	63	0	0	126	0	0
Stage 1	82	82	-	104	104	-	-	-	-	-	-	-
Stage 2	104	152	-	80	83	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	775	667	1003	776	708	982	1540	-	-	1460	-	-
Stage 1	927	827	-	901	809	-	-	-	-	-	-	-
Stage 2	901	771	-	928	826	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	742	657	1003	753	697	982	1540	-	-	1460	-	-
Mov Cap-2 Maneuver	742	657	-	753	697	-	-	-	-	-	-	-
Stage 1	921	822	-	894	802	-	-	-	-	-	-	-
Stage 2	869	765	-	908	821	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Ctrl Dly, s/v	8.74		10.8		0.69		1.01	
HCM LOS	A		B					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	1540	-	-	980	783	1460	-
HCM Lane V/C Ratio	0.008	-	-	0.017	0.208	0.007	-
HCM Ctrl Dly (s/v)	7.4	-	-	8.7	10.8	7.5	-
HCM Lane LOS	A	-	-	A	B	A	-
HCM 95th %tile Q(veh)	0	-	-	0.1	0.8	0	-

Intersection												
Int Delay, s/veh	2.8											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		+			+		↑	↑	↑	↑	↑	
Traffic Vol, veh/h	2	0	21	82	0	16	12	321	131	26	220	2
Future Vol, veh/h	2	0	21	82	0	16	12	321	131	26	220	2
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	150	-	150	150	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	2	0	23	89	0	17	13	349	142	28	239	2

Major/Minor	Minor2		Minor1		Major1			Major2				
Conflicting Flow All	672	814	240	671	673	349	241	0	0	491	0	0
Stage 1	297	297	-	375	375	-	-	-	-	-	-	-
Stage 2	375	517	-	296	298	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	370	312	799	370	377	694	1325	-	-	1072	-	-
Stage 1	712	668	-	646	617	-	-	-	-	-	-	-
Stage 2	646	533	-	713	667	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	347	301	799	347	363	694	1325	-	-	1072	-	-
Mov Cap-2 Maneuver	347	301	-	347	363	-	-	-	-	-	-	-
Stage 1	693	650	-	640	611	-	-	-	-	-	-	-
Stage 2	624	528	-	674	649	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Ctrl Dly, s/v	10.2		18.24		0.2		0.89	
HCM LOS	B		C					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR
Capacity (veh/h)	1325	-	-	718	378	1072	-
HCM Lane V/C Ratio	0.01	-	-	0.035	0.282	0.026	-
HCM Ctrl Dly (s/v)	7.7	-	-	10.2	18.2	8.4	-
HCM Lane LOS	A	-	-	B	C	A	-
HCM 95th %tile Q(veh)	0	-	-	0.1	1.1	0.1	-

Intersection						
Int Delay, s/veh	4					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		4	4		4	
Traffic Vol, veh/h	17	20	54	0	0	50
Future Vol, veh/h	17	20	54	0	0	50
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	18	22	59	0	0	54

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	59	0	-	0	117 59
Stage 1	-	-	-	-	59 -
Stage 2	-	-	-	-	59 -
Critical Hdwy	4.12	-	-	-	6.42 6.22
Critical Hdwy Stg 1	-	-	-	-	5.42 -
Critical Hdwy Stg 2	-	-	-	-	5.42 -
Follow-up Hdwy	2.218	-	-	-	3.518 3.318
Pot Cap-1 Maneuver	1545	-	-	-	879 1007
Stage 1	-	-	-	-	964 -
Stage 2	-	-	-	-	964 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1545	-	-	-	868 1007
Mov Cap-2 Maneuver	-	-	-	-	868 -
Stage 1	-	-	-	-	952 -
Stage 2	-	-	-	-	964 -

Approach	EB	WB	SB
HCM Ctrl Dly, s/v	3.38	0	8.78
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	827	-	-	-	1007
HCM Lane V/C Ratio	0.012	-	-	-	0.054
HCM Ctrl Dly (s/v)	7.4	0	-	-	8.8
HCM Lane LOS	A	A	-	-	A
HCM 95th %tile Q(veh)	0	-	-	-	0.2

Intersection						
Int Delay, s/veh	3					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		4	4		4	
Traffic Vol, veh/h	52	83	54	0	0	33
Future Vol, veh/h	52	83	54	0	0	33
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	57	90	59	0	0	36

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	59	0	-	0	262 59
Stage 1	-	-	-	-	59 -
Stage 2	-	-	-	-	203 -
Critical Hdwy	4.12	-	-	-	6.42 6.22
Critical Hdwy Stg 1	-	-	-	-	5.42 -
Critical Hdwy Stg 2	-	-	-	-	5.42 -
Follow-up Hdwy	2.218	-	-	-	3.518 3.318
Pot Cap-1 Maneuver	1545	-	-	-	727 1007
Stage 1	-	-	-	-	964 -
Stage 2	-	-	-	-	831 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1545	-	-	-	699 1007
Mov Cap-2 Maneuver	-	-	-	-	699 -
Stage 1	-	-	-	-	927 -
Stage 2	-	-	-	-	831 -

Approach	EB	WB	SB
HCM Ctrl Dly, s/v	2.86	0	8.71
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	693	-	-	-	1007
HCM Lane V/C Ratio	0.037	-	-	-	0.036
HCM Ctrl Dly (s/v)	7.4	0	-	-	8.7
HCM Lane LOS	A	A	-	-	A
HCM 95th %tile Q(veh)	0.1	-	-	-	0.1

Intersection						
Int Delay, s/veh	3.5					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		4	4		4	
Traffic Vol, veh/h	17	29	68	0	0	50
Future Vol, veh/h	17	29	68	0	0	50
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	18	32	74	0	0	54

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	74	0	-	0	142 74
Stage 1	-	-	-	-	74 -
Stage 2	-	-	-	-	68 -
Critical Hdwy	4.12	-	-	-	6.42 6.22
Critical Hdwy Stg 1	-	-	-	-	5.42 -
Critical Hdwy Stg 2	-	-	-	-	5.42 -
Follow-up Hdwy	2.218	-	-	-	3.518 3.318
Pot Cap-1 Maneuver	1526	-	-	-	850 988
Stage 1	-	-	-	-	949 -
Stage 2	-	-	-	-	954 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1526	-	-	-	840 988
Mov Cap-2 Maneuver	-	-	-	-	840 -
Stage 1	-	-	-	-	937 -
Stage 2	-	-	-	-	954 -

Approach	EB	WB	SB
HCM Ctrl Dly, s/v	2.73	0	8.86
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	665	-	-	-	988
HCM Lane V/C Ratio	0.012	-	-	-	0.055
HCM Ctrl Dly (s/v)	7.4	0	-	-	8.9
HCM Lane LOS	A	A	-	-	A
HCM 95th %tile Q(veh)	0	-	-	-	0.2

Intersection						
Int Delay, s/veh	2.5					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		4	4		4	
Traffic Vol, veh/h	52	110	79	0	0	33
Future Vol, veh/h	52	110	79	0	0	33
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	57	120	86	0	0	36

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	86	0	-	0	318 86
Stage 1	-	-	-	-	86 -
Stage 2	-	-	-	-	233 -
Critical Hdwy	4.12	-	-	-	6.42 6.22
Critical Hdwy Stg 1	-	-	-	-	5.42 -
Critical Hdwy Stg 2	-	-	-	-	5.42 -
Follow-up Hdwy	2.218	-	-	-	3.518 3.318
Pot Cap-1 Maneuver	1510	-	-	-	675 973
Stage 1	-	-	-	-	937 -
Stage 2	-	-	-	-	806 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1510	-	-	-	648 973
Mov Cap-2 Maneuver	-	-	-	-	648 -
Stage 1	-	-	-	-	900 -
Stage 2	-	-	-	-	806 -

Approach	EB	WB	SB
HCM Ctrl Dly, s/v	2.4	0	8.84
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	578	-	-	-	973
HCM Lane V/C Ratio	0.037	-	-	-	0.037
HCM Ctrl Dly (s/v)	7.5	0	-	-	8.8
HCM Lane LOS	A	A	-	-	A
HCM 95th %tile Q(veh)	0.1	-	-	-	0.1

Intersection						
Int Delay, s/veh	7.4					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		4	4		4	
Traffic Vol, veh/h	17	3	4	0	0	50
Future Vol, veh/h	17	3	4	0	0	50
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	18	3	4	0	0	54

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	4	0	-	0	45
Stage 1	-	-	-	-	4
Stage 2	-	-	-	-	40
Critical Hdwy	4.12	-	-	-	6.42
Critical Hdwy Stg 1	-	-	-	-	5.42
Critical Hdwy Stg 2	-	-	-	-	5.42
Follow-up Hdwy	2.218	-	-	-	3.518
Pot Cap-1 Maneuver	1617	-	-	-	966
Stage 1	-	-	-	-	1019
Stage 2	-	-	-	-	982
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1617	-	-	-	955
Mov Cap-2 Maneuver	-	-	-	-	955
Stage 1	-	-	-	-	1007
Stage 2	-	-	-	-	982

Approach	EB	WB	SB
HCM Ctrl Dly, s/v	6.16	0	8.51
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1530	-	-	-	1079
HCM Lane V/C Ratio	0.011	-	-	-	0.05
HCM Ctrl Dly (s/v)	7.3	0	-	-	8.5
HCM Lane LOS	A	A	-	-	A
HCM 95th %tile Q(veh)	0	-	-	-	0.2

Intersection						
Int Delay, s/veh	4.8					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		4	4		4	
Traffic Vol, veh/h	52	31	21	0	0	33
Future Vol, veh/h	52	31	21	0	0	33
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	57	34	23	0	0	36

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	23	0	-	0	170 23
Stage 1	-	-	-	-	23 -
Stage 2	-	-	-	-	147 -
Critical Hdwy	4.12	-	-	-	6.42 6.22
Critical Hdwy Stg 1	-	-	-	-	5.42 -
Critical Hdwy Stg 2	-	-	-	-	5.42 -
Follow-up Hdwy	2.218	-	-	-	3.518 3.318
Pot Cap-1 Maneuver	1592	-	-	-	821 1054
Stage 1	-	-	-	-	1000 -
Stage 2	-	-	-	-	881 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1592	-	-	-	791 1054
Mov Cap-2 Maneuver	-	-	-	-	791 -
Stage 1	-	-	-	-	964 -
Stage 2	-	-	-	-	881 -

Approach	EB	WB	SB
HCM Ctrl Dly, s/v	4.6	0	8.54
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1128	-	-	-	1054
HCM Lane V/C Ratio	0.035	-	-	-	0.034
HCM Ctrl Dly (s/v)	7.3	0	-	-	8.5
HCM Lane LOS	A	A	-	-	A
HCM 95th %tile Q(veh)	0.1	-	-	-	0.1

Intersection						
Int Delay, s/veh	5.7					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		4	4		4	
Traffic Vol, veh/h	17	12	18	0	0	50
Future Vol, veh/h	17	12	18	0	0	50
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	18	13	20	0	0	54

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	20	0	-	0	70
Stage 1	-	-	-	-	20
Stage 2	-	-	-	-	50
Critical Hdwy	4.12	-	-	-	6.42
Critical Hdwy Stg 1	-	-	-	-	5.42
Critical Hdwy Stg 2	-	-	-	-	5.42
Follow-up Hdwy	2.218	-	-	-	3.518
Pot Cap-1 Maneuver	1597	-	-	-	935
Stage 1	-	-	-	-	1003
Stage 2	-	-	-	-	972
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1597	-	-	-	924
Mov Cap-2 Maneuver	-	-	-	-	924
Stage 1	-	-	-	-	991
Stage 2	-	-	-	-	972

Approach	EB	WB	SB
HCM Ctrl Dly, s/v	4.27	0	8.59
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1055	-	-	-	1058
HCM Lane V/C Ratio	0.012	-	-	-	0.051
HCM Ctrl Dly (s/v)	7.3	0	-	-	8.6
HCM Lane LOS	A	A	-	-	A
HCM 95th %tile Q(veh)	0	-	-	-	0.2

Intersection						
Int Delay, s/veh	3.5					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		4	4		4	
Traffic Vol, veh/h	52	58	46	0	0	33
Future Vol, veh/h	52	58	46	0	0	33
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	57	63	50	0	0	36

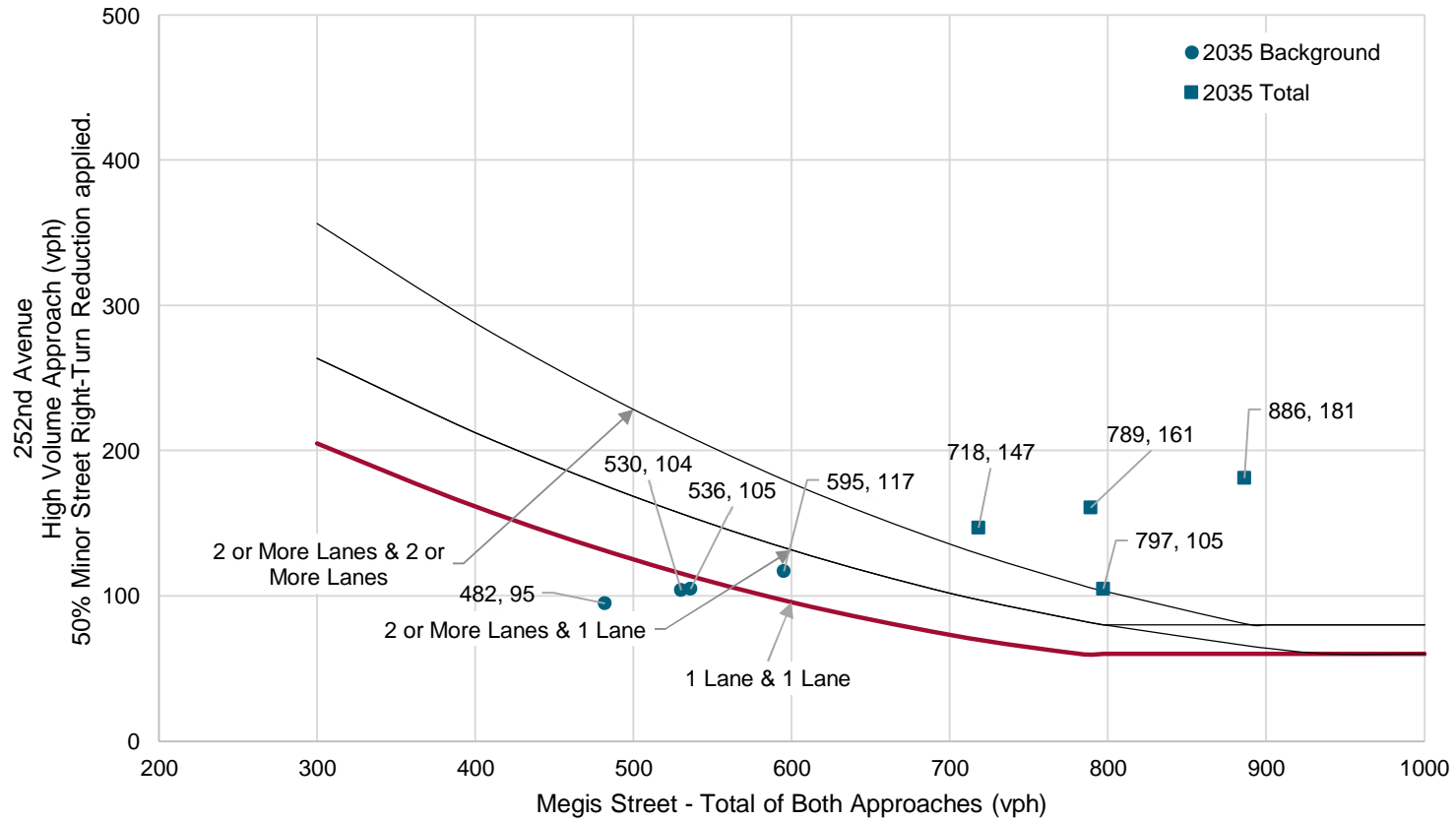
Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	50	0	-	0	226 50
Stage 1	-	-	-	-	50 -
Stage 2	-	-	-	-	176 -
Critical Hdwy	4.12	-	-	-	6.42 6.22
Critical Hdwy Stg 1	-	-	-	-	5.42 -
Critical Hdwy Stg 2	-	-	-	-	5.42 -
Follow-up Hdwy	2.218	-	-	-	3.518 3.318
Pot Cap-1 Maneuver	1557	-	-	-	762 1018
Stage 1	-	-	-	-	972 -
Stage 2	-	-	-	-	854 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1557	-	-	-	733 1018
Mov Cap-2 Maneuver	-	-	-	-	733 -
Stage 1	-	-	-	-	936 -
Stage 2	-	-	-	-	854 -

Approach	EB	WB	SB
HCM Ctrl Dly, s/v	3.5	0	8.66
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	851	-	-	-	1018
HCM Lane V/C Ratio	0.036	-	-	-	0.035
HCM Ctrl Dly (s/v)	7.4	0	-	-	8.7
HCM Lane LOS	A	A	-	-	A
HCM 95th %tile Q(veh)	0.1	-	-	-	0.1

Appendix F: Signal Warrant Worksheet

Warrant 2 - Four-Hour Vehicular Volume (70% Factor)



*Note: 80 vph applies as the lower threshold volume for a minor-street approach with two or more lanes and 60 vph applies as the lower threshold volume for a minor-street approach with one lane.

Appendix G: Queue Analysis Worksheets

Queues
 1: US-275 Southbound Ramps & Meigs St

2050 Total AM
 02/12/2026



Lane Group	EBT	EBR	WBL	WBT	SBT
Lane Group Flow (vph)	465	155	438	632	226
v/c Ratio	0.46	0.17	0.77	0.48	0.77
Control Delay (s/veh)	19.9	5.3	65.4	5.7	53.2
Queue Delay	0.0	0.0	0.0	0.0	0.0
Total Delay (s/veh)	19.9	5.3	65.4	5.7	53.2
Queue Length 50th (ft)	211	11	204	150	137
Queue Length 95th (ft)	377	54	261	192	215
Internal Link Dist (ft)	468			843	827
Turn Bay Length (ft)		150	175		
Base Capacity (vph)	1021	923	716	1313	406
Starvation Cap Reductn	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0
Reduced v/c Ratio	0.46	0.17	0.61	0.48	0.56
Intersection Summary					

Queues
1: US-275 Southbound Ramps & Meigs St

2050 Total PM
02/12/2026



Lane Group	EBT	EBR	WBL	WBT	SBT
Lane Group Flow (vph)	910	368	772	865	374
v/c Ratio	1.06	0.45	1.03	0.65	1.17
Control Delay (s/veh)	79.0	13.8	61.9	25.8	144.9
Queue Delay	0.0	0.0	0.0	1.2	0.0
Total Delay (s/veh)	79.0	13.8	61.9	27.0	144.9
Queue Length 50th (ft)	~785	104	~346	699	~383
Queue Length 95th (ft)	#1037	187	m#389	m744	#601
Internal Link Dist (ft)	468			843	827
Turn Bay Length (ft)		150	175		
Base Capacity (vph)	861	817	750	1329	321
Starvation Cap Reductn	0	0	0	249	0
Spillback Cap Reductn	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0
Reduced v/c Ratio	1.06	0.45	1.03	0.80	1.17

Intersection Summary

- ~ Volume exceeds capacity, queue is theoretically infinite.
Queue shown is maximum after two cycles.
- # 95th percentile volume exceeds capacity, queue may be longer.
Queue shown is maximum after two cycles.
- m Volume for 95th percentile queue is metered by upstream signal.

Queues
1: US-275 Southbound Ramps & Meigs St

2050 Total AM - Improved #1
02/13/2026



Lane Group	EBT	EBR	WBL	WBT	SBL	SBT
Lane Group Flow (vph)	465	155	438	632	163	63
v/c Ratio	0.24	0.16	0.76	0.25	0.69	0.13
Control Delay (s/veh)	15.1	3.1	54.4	4.0	63.2	0.5
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay (s/veh)	15.1	3.1	54.4	4.0	63.2	0.5
Queue Length 50th (ft)	93	0	180	52	129	0
Queue Length 95th (ft)	156	38	247	115	196	0
Internal Link Dist (ft)	468			843		827
Turn Bay Length (ft)		150	175			
Base Capacity (vph)	1966	947	1003	2527	428	623
Starvation Cap Reductn	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0
Reduced v/c Ratio	0.24	0.16	0.44	0.25	0.38	0.10
Intersection Summary						

Queues
1: US-275 Southbound Ramps & Meigs St

2050 Total PM - Improved #1
02/13/2026



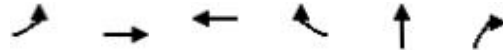
Lane Group	EBT	EBR	WBL	WBT	SBL	SBT
Lane Group Flow (vph)	910	368	772	865	327	47
v/c Ratio	0.67	0.48	0.88	0.36	0.86	0.13
Control Delay (s/veh)	34.8	12.8	44.8	8.4	65.4	10.3
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay (s/veh)	34.8	12.8	44.8	8.4	65.4	10.3
Queue Length 50th (ft)	323	70	306	210	269	1
Queue Length 95th (ft)	421	172	353	180	#387	34
Internal Link Dist (ft)	468			843		827
Turn Bay Length (ft)		150	175			
Base Capacity (vph)	1368	760	949	2395	438	426
Starvation Cap Reductn	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0
Reduced v/c Ratio	0.67	0.48	0.81	0.36	0.75	0.11

Intersection Summary

95th percentile volume exceeds capacity, queue may be longer.
Queue shown is maximum after two cycles.

Queues
2: US-275 Northbound Ramps & Meigs St

2050 Total AM
02/13/2026



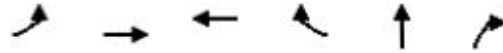
Lane Group	EBL	EBT	WBT	WBR	NBT	NBR
Lane Group Flow (vph)	31	588	891	182	155	443
v/c Ratio	0.07	0.22	0.40	0.18	0.67	0.28
Control Delay (s/veh)	4.4	4.3	7.7	1.5	62.8	0.4
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay (s/veh)	4.4	4.3	7.7	1.5	62.8	0.4
Queue Length 50th (ft)	5	55	158	0	118	0
Queue Length 95th (ft)	m13	75	246	29	181	0
Internal Link Dist (ft)		843	3531		1086	
Turn Bay Length (ft)	175			150		150
Base Capacity (vph)	463	2652	2205	1034	406	1583
Starvation Cap Reductn	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0
Reduced v/c Ratio	0.07	0.22	0.40	0.18	0.38	0.28

Intersection Summary

m Volume for 95th percentile queue is metered by upstream signal.

Queues
2: US-275 Northbound Ramps & Meigs St

2050 Total PM
02/13/2026



Lane Group	EBL	EBT	WBT	WBR	NBT	NBR
Lane Group Flow (vph)	63	1131	1330	347	291	828
v/c Ratio	0.25	0.47	0.61	0.33	0.79	0.52
Control Delay (s/veh)	3.0	5.2	15.9	5.1	60.3	1.2
Queue Delay	0.0	0.0	0.0	0.0	0.1	0.0
Total Delay (s/veh)	3.0	5.2	15.9	5.1	60.4	1.2
Queue Length 50th (ft)	8	153	324	36	219	0
Queue Length 95th (ft)	m13	m227	487	104	298	0
Internal Link Dist (ft)		843	3497		1086	
Turn Bay Length (ft)	175			150		150
Base Capacity (vph)	261	2399	2198	1066	495	1583
Starvation Cap Reductn	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	7	0
Storage Cap Reductn	0	0	0	0	0	0
Reduced v/c Ratio	0.24	0.47	0.61	0.33	0.60	0.52

Intersection Summary

m Volume for 95th percentile queue is metered by upstream signal.

Queues
3: Meigs St & 252nd St

2050 Total AM
02/13/2026



Lane Group	EBT	WBL	WBT	NBL	NBT	NBR	SBL	SBT
Lane Group Flow (vph)	17	160	25	29	113	89	19	199
v/c Ratio	0.14	0.36	0.03	0.04	0.11	0.10	0.02	0.19
Control Delay (s/veh)	24.0	30.7	0.0	9.6	13.5	1.7	9.6	14.0
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay (s/veh)	24.0	30.7	0.0	9.6	13.5	1.7	9.6	14.0
Queue Length 50th (ft)	2	76	0	5	22	0	3	40
Queue Length 95th (ft)	22	122	0	21	72	10	14	118
Internal Link Dist (ft)	391		802		3531			1531
Turn Bay Length (ft)						150	150	
Base Capacity (vph)	343	442	1164	709	1015	919	772	1021
Starvation Cap Reductn	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.05	0.36	0.02	0.04	0.11	0.10	0.02	0.19

Intersection Summary

Queues
3: Meigs St & 252nd St

2050 Total PM
02/13/2026



Lane Group	EBT	WBL	WBT	NBL	NBT	NBR	SBL	SBT
Lane Group Flow (vph)	28	164	48	47	445	177	61	267
v/c Ratio	0.09	0.67	0.15	0.06	0.38	0.17	0.10	0.22
Control Delay (s/veh)	14.4	47.4	10.6	5.0	11.5	2.2	5.1	9.2
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay (s/veh)	14.4	47.4	10.6	5.0	11.5	2.2	5.1	9.2
Queue Length 50th (ft)	3	89	1	7	123	0	8	65
Queue Length 95th (ft)	24	144	28	20	236	31	25	132
Internal Link Dist (ft)	336		753		3497			1566
Turn Bay Length (ft)						150	150	
Base Capacity (vph)	481	390	485	805	1160	1052	630	1206
Starvation Cap Reductn	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.06	0.42	0.10	0.06	0.38	0.17	0.10	0.22
Intersection Summary								



FoleyShald Engineering, LLC
3930 S 147th Street, Suite 200
Omaha, NE 68144
402-804-3993

May 12, 2026

Gregory Perry, PE
City of Valley Engineer
203 Spruce Street,
Valley, NE 68064
402-359-2251

Re: Lago Villaggio - Preliminary Plat Resubmittal
FSE Project No. 104.001

Dear Mr. Perry:

Please accept the resubmitted Preliminary Plat for the project to be known as “Lago Villaggio” located north of King Lake Road and East of N 252nd Street. We are requesting this project be considered at the next available Planning Commission meeting.

This project was originally submitted on 11/14/2025 as “Valley Village”. Following a meeting with City staff on 11/25/2025, the development team initiated a traffic study and changed the proposed project name to “Lago Villaggio”. There are a few other key changes from the original submittal. The phasing has been simplified from 4 to 2 phases. Rather than a mix of residential lots, we have now reconfigured lot widths to a more typical single-family subdivision. Lots 1 and 2 will still be proposed multifamily. However, we are now proposing R2 zoning for the entire development without the cluster overlay.

It should be noted that without the cluster overlay, we are requesting two waivers to zoning requirements.

1. We are requesting that the maximum block length exceed 600'. This is largely due to the centrally located lake and existing riverbed lake on the east side of the property. The proposed layout still maintains adequate traffic circulation and emergency vehicle access.
2. We are requesting a 25' front yard setback. This is a typical front yard setback for other jurisdictions in the area and fits the proposed medium density housing better. Even with the reduced setback, there will be a minimum of 41 feet from back of curb to front of house which provides ample room for driveway parking and sidewalk facilities.

Documents in this resubmittal include:

- Preliminary Plat
- Traffic Study
- Drainage Report
- Source and Use of Funds
- Preliminary Plat Application
- Zoning Application

Please let me know if you should have any questions or require additional information.

Sincerely,

Josh Siel, PE
Project Manager



EAGLE ENGINEERING GROUP

12100 West Center Road, Suite 803 Omaha, Nebraska 68144
(402) 399-0227 | www.eagleengineeringgroup.com

June 10, 2026

Larry Bottger, Chairman
Planning Commission
CITY OF VALLEY
203 North Spruce Street
Valley, Nebraska 68064

Re: Lago Villaggio Preliminary Plat Submittal
252nd Street and King Lake Road
Valley, Nebraska
EEG #24-03-15

Dear Mr. Bottger:

We offer the following review comments related to the Developers' Preliminary Plat submittal dated May 12th for the Lago Villaggio Lake development located on northeast corner near the intersection of N. 252nd Street and King Lake Road:

Preliminary Plat

1. Note: No direct vehicular access to N. 252nd or King Lake Road will be allowed.
2. Street names shall be verified by Douglas County/City of Omaha Planning Department.
3. Lot 293 and 322 do not meet the minimum lot size for the R-2 zoning district (6,000 sq ft).

Rezoning

1. The future land use plan map was amended in 2018 for low density residential for this parcel and the lands east of 252nd Street. The land use would be compatible with no public sanitary and adjacent floodplain of the Elkhorn River. The Catalina development has since extended utilities nearby that would suppose a medium to high density residential (MHDR) land use.
2. The requested zoning of R-2 is contiguous with the Catalina development and adjacent to the R-1 zoning within King Lake.

Utility Plan

1. Lift station location shall be further review with City Engineer for access, elevated above the AO floodplain (1133) and downstream force main conveyance connection.
2. Water main along 252nd Street shall extend south to intersection.

Grading and Drainage

1. Increase of runoff is being conveyed into the proposed lake feature. Overflow outlet release to the adjacent King Lake will need to be further review for location and any downstream impacts.
2. Conveyance of Impact Point 1 needs to be provided. Drainage from the north boundary may need to be considered to flow easterly.
3. Major storm conveyance from 252nd Street on the west to the east towards the Elkhorn River need to be provided.
4. Provide lake slope of stability for dredging operations.

Paving

1. Typical Section: sidewalk shall be located 1' from property line.
2. Street F and Street G shall extend to the north boundary.



Centerline Profiles

1. Provide vertical curves for all vertical point of intersection (VPI) with an algebraic difference above 1%.
2. Provide drainage away from 252nd Street on Street 'B' (STA. 100+00).
3. Provide drainage away from King Lake Road on Street 'F' (STA. 500+00) and Street 'G' (STA. 600+00)

Source and Use of Funds

1. The development will be financed using a Sanitary and Improvement District (SID).
2. Capital Facility Fees for sanitary sewer and water main shall be specially assessed on the 50% portion paid at the time of platting.

Traffic Impact Study

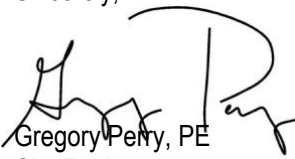
1. The analysis needs to provide a Phase 1 study year (i.e. 2028) build out and Phase 2 study year (i.e. 2035) buildout.
2. The analysis needs to include the Catalina development that is currently under construction for the 2035 short term and 2050 long term study years (Section 3.4).
3. 4.1 Trip Generation: Separate the traffic generation by development phase.
4. Appendix A: Update the conceptual site plan with the proposed preliminary plat exhibit.

General Items

1. Waivers: 1) 600' block Length – Provide the 10' pedestrian easements (Section 5.22)
2. Waivers: 2) 25' front yard setback: City Engineer does not support the waiver of the front yard setback as it does not demonstrate a hardship or meet the subdivision regulations.
3. Existing 252nd Street or King Lake Road improvements shall be coordinated with the City and Douglas County. The Developer will be required to construct all off-site improvements as part of Phase 1.
4. The property is located within the Zone 'AO' special flood hazard area (SFHA) as delineated by FEMA and a floodplain development permit will be required.
5. The Development shall confirm with Douglas County Emergency Management Agency (DC EMA) for adequate outdoor warning siren coverage (#102).

I will be available to answer any questions that you may have prior to the Planning Commission meeting on June 16, 2026.

Sincerely,



Gregory Perry, PE
City Engineer

ec: Mike Gorman, City Administrator
Gabe Evans, Building Inspector/ Zoning Administrator
Josh Seil, PE FoleyShald



DOUGLAS COUNTY ENGINEER

15505 West Maple Road
Omaha, Nebraska 68116-5173
402-444-6372
Fax: 402-444-6244
engineer@douglascounty-ne.gov

June 15th, 2026

TO: Ms. Christie Donnermeyer City Clerk
City of Valley

FROM: Brian J. Goodbarn, P.E. Civil Engineer
Douglas County Engineer's Office

RE: **REVIEW COMMENTS FOR June 16th, 2026
PLANNING COMMISSION MEETING**

The Douglas County Engineer's Office submits the following review comments regarding the agenda items scheduled for discussion before the Valley Planning Commission on June 16th, 2026.

VALLEY COMMENTS – LAGO VILLAGGIO

1. CAD files for the preliminary plat and final plat shall be submitted to the Douglas County Engineer's Office for review.
2. The proposed driveway connections from the development to King Lake Road will require permits from this office and must meet proper sight distance criteria and incorporate intersection improvements as required by current City/County Access Policy. The cost of the roadway improvements for the driveway connections, if required, will be the responsibility of the developer. The sight distance criteria to be used in the evaluation, of the driveway connections, will be AASHTO Stopping Sight Distance for 50 MPH design (425').
3. Storm sewer design computations and final plans must be submitted to this office for review that relate to the handling of roadway and site drainage along the development boundary adjacent to King Lake Road and the county portion of 252nd street.
4. Any grading, sewer or utility work, sidewalk, landscaping, ADA curb ramps or any other improvements carried out in conjunction with this development, in King Lake Road and the county portion of 252nd street right-of-way, will require a permit(s) from this office.
5. 40' ROW should be dedicated along the north boundary from 252nd Street to Street F for the future Ida Street.
6. Temporary turn-arounds should be provided at the terminus of all temporarily dead-ended streets
7. It should be noted on the plat that direct access to King Lake Road and the county portion of 252nd street, from the adjacent lots or outlots, will not be permitted.
8. It may be necessary to incorporate traffic calming measures in the design of some of the interior streets.
9. If Lago Villaggio Addition will not be located in a Sanitary Improvement District, a maintenance agreement will be required before Douglas County will provide routine maintenance of public streets or request street lights on behalf of the Developer. If not located in a Sanitary Improvement District, major maintenance activities are the responsibility of the adjacent property owner.

10. An Interlocal Agreement with Douglas County will be needed for the improvement of any County Roads, prior to the signing of the final plat.

If you have any questions concerning the above review comments, feel free to call me at (402) 444-6474.